



**City of Kingston  
Report to Planning Committee  
Report Number PC-25-008**

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**To:** Chair and Members of the Planning Committee  
**From:** Paige Agnew, Commissioner, Growth & Development  
**Resource Staff:** Tim Park, Director, Planning Services  
**Date of Meeting:** March 6, 2025  
**Subject:** Recommendation Report  
**File Number:** D35-001-2025  
**Address:** North King's Town Specific Policy Area (location in DASH shown as 216 Ontario Street)  
**Application Type:** Official Plan & Zoning By-Law Amendment  
**Applicant:** City of Kingston

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**Council Strategic Plan Alignment:**

Theme: 1. Support Housing Affordability

Goal: 1.1 Promote increased supply and affordability of housing.

Theme: 3. Build an Active and Connected Community

Goal: 3.3 Improve public transit and active transportation options.

Theme: 5. Drive Inclusive Economic Growth

Goal: 5.3 Diversify Kingston's economic base.

**Executive Summary:**

The following is a report recommending approval to the Planning Committee regarding applications for Official Plan and Zoning By-Law amendments submitted by the City of Kingston, to implement the recommendations of the North King's Town (NKT) project, including the

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findings of the technical studies completed as part of this work. The proposed amendments include Official Plan policies and zoning by-law regulations to guide infill and intensification in strategic locations within NKT in proximity to express transit and active transportation routes, identification of mixed-use areas and a new main street commercial area, and the implementation of the planning-related transportation and cultural heritage recommendations through text and mapping changes in the Official Plan and/or the zoning by-laws, as applicable.

The proposed amendments relate to lands generally located within the area described as John Counter Boulevard to the north, the Great Cataraqui River to the east, Colbourne Street and Bay Street to the south and Division Street to the west. The proposed amendments were presented at a Community Meeting held on December 5, 2024 ([Report Number PC-25-001](#)).

Intensification areas, generally representing vacant, underutilized or brownfield properties, have been identified and would be subject to a new Specific Policy Area section of the Official Plan. These intensification areas are primarily concentrated around the Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street and Railway Street intersections, with a limited number of infill areas identified within existing residential neighbourhoods. The intensification areas are contemplated for mixed-use development with maximum building heights generally ranging between four and 15 storeys, with the potential to increase building heights up to 20 storeys through the minor variance process, in appropriate locations. A total of approximately 7,380 residential units have been contemplated for the identified intensification areas, current development applications and areas subject to existing site-specific Official Plan policies.

New commercial uses would be supported through a new Mixed-Use designation that would enable a broad range of residential and commercial uses, together with limited light industrial uses where land use compatibility can be achieved. Ground floor commercial uses will be required within identified areas. Commercial uses would be further supported by a new Main Street Commercial designation on Montreal Street, generally between James Street and Raglan Road. This area was identified as previously containing a broader range of commercial uses that functioned to support the surrounding residential neighbourhoods. These properties would retain residential uses, while having the flexibility to establish commercial uses on the ground floors. Conversion of existing buildings to commercial uses would not be required.

Approximately 18 hectares of land are proposed to be converted from an industrial land use designation to facilitate mixed-use development within NKT. These lands generally represent vacant, underutilized and brownfield properties, or land uses that no longer qualify as "Employment Areas" under the Provincial Planning Statement, 2024. The remaining employment lands within the study area would be protected from incompatible development through new zoning regulations requiring separation distances between industrial and sensitive uses.

There are no amendments proposed to the Environmental Protection Area designation, whereas the Open Space designation would expand to recognize existing uses at Veteran's Field and along the Waterfront Pathway.

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The Official Plan amendments are proposed to be implemented through zoning by-law amendments. The Kingston Zoning By-Law would be amended to include three new Mixed-Use (MU) zones, a new Urban Multi-Residential 11 (URM11), various new exceptions to establish site-specific regulations, and amendments to the Parking Area Schedule, Required Ground Floor Commercial Schedule, Road Classification Schedule, Zone Exception Overlay Schedule, Holding Overlay Schedule, and Express Transit Overlay Schedule. Any intensification areas currently regulated through former City of Kingston Zoning By-Law Number 8499 would be removed from this by-law and brought into the Kingston Zoning By-Law.

The proposed Official Plan and Zoning By-Law amendments are supported by the following technical studies:

- **Mobility Plan**

The Mobility Plan, prepared by ARUP Canada Inc. with support from Dillon Consulting, provides a series of recommendations for the pedestrian, cycling, transit and road networks to be able to adequately accommodate the identified potential growth within NKT. The recommendations were designed to encourage a shift towards active transportation and transit options, reducing dependence on automobile travel. These improvements are intended to be phased over time in conjunction with development as the investment of capital to complete the infrastructure and resourcing to support service levels increases will be significant.

Detailed transportation modelling completed in 2019 confirmed the southern portion (south of Rideau Street/ Railway Street) of the Wellington Street Extension (WSE) was not required, as there was adequate capacity within the existing road network to accommodate the planned growth. Going forward, the Mobility Plan recommends that this road allowance be protected for a future active transportation corridor, directly connecting an area of future intensification to the downtown area. The modelling conducted as part of the Mobility Plan indicated that the northern portion, now referred to as New Road #1 as it would no longer connect to Wellington Street, providing both benefits and trade-offs to the road network. The Mobility Plan recommends that New Road #1 be carried forward for further consideration as part of city-wide transportation planning (i.e. the city-wide Integrated Mobility Plan) in the context of new growth projections prior to making a determination on the future road.

- **Servicing Study**

The Servicing Study, prepared by J.L. Richards & Associates with support from Aquafor Beech, examines the existing servicing conditions, assesses servicing requirements associated with identified intensification areas, and proposes strategies to satisfy those future servicing demands. The Servicing Study confirms that upgrades to the potable water and wastewater networks will be required to support full build-out of NKT intensification areas. There were no specific upgrades identified for the electrical or natural gas networks at this time, however Utilities Kingston will monitor service levels in

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relation to future demand. The infrastructure upgrades are anticipated to be phased over time in conjunction with development pressure.

- **Cultural Heritage Study**

The Cultural Heritage Study (CHS), prepared by Bray Heritage, provides a cultural heritage analysis and recommendations for the NKT project and includes analysis of NKT's existing and potential tangible and intangible cultural heritage resources. The recommendations from the CHS are proposed to be incorporated into the Official Plan in various ways, including: policies to require heritage impact assessments and urban design studies; a detailed site-specific approach to guide future development of the Outer Station land; modifications to protected views; and an expansion to the St. Lawrence Ward Heritage Character Area, which has been identified as a future Heritage Conservation District study area.

- **Financial and Implementation Strategy**

The proposed amendments are further supported by the Financial and Implementation Strategy (FIS), prepared by Hemson Consulting, which focused on identifying the most effective financial incentive programs to stimulate private investment within NKT. The FIS provides various recommendations for the City's Brownfield Community Improvement Program, including that the program should be extended for at least another 10 years to allow sufficient time to encourage and support rehabilitation and redevelopment of the NKT intensification areas. The FIS contains high-level recommendations on prioritizing future capital projects within NKT to balance infrastructure projects with more 'visible' public realm projects that enhance the amenity within NKT and foster momentum for additional private sector investment and market interest in the area.

The technical studies are available on the Development and Services Hub (DASH) as supporting information for record number [D35-001-2025](#) under the 'Record Info' drop down menu.

The proposed amendments have regard for matters of provincial interest set out in the *Planning Act*, are consistent with the Provincial Planning Statement, 2024, conform with and implement the City of Kingston Official Plan and represent good land use planning. The proposed amendments have been widely informed by comments received through several public engagement sessions and meetings with the Community Working Group held as part of the NKT project.

**Recommendation:**

**That** the Planning Committee recommends to Council:

**That** the North King's Town Mobility Plan, dated November 8, 2024, the North King's Town Servicing Study, dated August 30, 2024, the North King's Town Cultural Heritage Study,

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dated July 2024, and the Financial and Implementation Strategy, dated January 31, 2025 be received by Council; and

**That** the applications for Official Plan and zoning by-law amendments (File Number D35-001-2025) submitted by the City of Kingston, be approved; and

**That** the City of Kingston Official Plan, as amended, be further amended by amendment number 99, as per Exhibit A, (Draft By-Law and Schedules 'A' to 'I' to Amend the Official Plan) to Report Number PC-25-008; and

**That** Kingston Zoning By-Law Number 2022-62, as amended, be further amended, as per Exhibit B (Draft By-Law and Schedules 'A' to 'G' to Amend Zoning By-Law Number 2022-62) to Report Number PC-25-008; and

**That** former City of Kingston Zoning By-Law Number 8499, as amended, be further amended, as per Exhibit C, (Draft By-Law and Schedule A to amend Zoning By-Law Number 8499) to Report Number PC-25-008; and

**That** Council determines that in accordance with Section 34(17) of the *Planning Act*, no further notice is required prior to the passage of the by-law and Official Plan amendment; and

**That** the amending by-laws be presented to Council for all three readings.

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**Authorizing Signatures:**

ORIGINAL SIGNED BY COMMISSIONER

**Paige Agnew, Commissioner,  
Growth & Development Services**

ORIGINAL SIGNED BY CHIEF ADMINISTRATIVE OFFICER

**Lanie Hurdle, Chief  
Administrative Officer**

**Consultation with the following Members of the Corporate Management Team:**

- |   |                                     |
|---|-------------------------------------|
| Jennifer Campbell, Commissioner, Community Services                       | <input checked="" type="checkbox"/> |
| Neil Carbone, Commissioner, Corporate Services & Emergency Services       | Not required                        |
| David Fell, President & CEO, Utilities Kingston                           | <input checked="" type="checkbox"/> |
| Desirée Kennedy, Chief Financial Officer & City Treasurer                 | <input checked="" type="checkbox"/> |
| Ian Semple, Acting Commissioner, Transportation & Infrastructure Services | <input checked="" type="checkbox"/> |

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**Options/Discussion:****Background**

The North King's Town (NKT) project was initiated in 2016 as a result of significant public concern around the Wellington Street Extension and an expressed desire to develop a long-term vision for the Inner Harbour and Old Industrial Areas that will support redevelopment in these areas in a manner that promotes a sustainable, healthy, vibrant, and liveable community. The NKT project included two phases and culminated with the preparation of the following technical studies:

- Land Use (proposed Official Plan policies and zoning regulations to guide development within infill and intensification areas);
- Mobility Plan;
- Servicing Study;
- Cultural Heritage Study; and
- Financial and Implementation Strategy.

A Community Meeting was held at Planning Committee on December 5, 2024 to present the findings of the technical studies (except the Financial and Implementation Strategy) and the proposed Official Plan and Zoning By-Law amendments to implement the planning related aspects. [Report Number PC-25-001](#) provides a more description of the background and phases of the NKT project. A summary of the feedback received at the Community Meeting is provided in the Public Comments section of this report.

**Study Area**

As shown in Exhibit D, the North King's Town (NKT) study area is comprised of the Inner Harbour and Old Industrial Areas, generally bordered by John Counter Boulevard and the Canadian National (CN) rail line to the north, the Great Cataraqui River to the east, Colborne Street and Bay Street to the south and Division Street to the west. As shown in Exhibits E and F, the study area contains a wide range of land uses, generally transitioning from residential uses in the southern portion to industrial uses in the northern portion, with commercial uses interspersed along Montreal Street and Division Street. NKT is home to approximately 4,300 residential homes and approximately 7,500 residents. The study area contains several valued open spaces, including McBurney (Skeleton) Park, Doug Fluhrer Park, Emma Martin Park and portions of the Kingston & Pembroke (K&P) and Waterfront Trails. The shoreline of the Great Cataraqui River contains woodlands, riparian corridors and provincially significant wetlands, which provide important habitat for a range of flora and fauna. The study area has started to transition away from industrial uses in previous years, leaving behind several large, vacant or underutilized brownfield properties that hold significant redevelopment potential within the community.

Belle Park, Belle Island and the Great Cataraqui River hold significance for Indigenous community'. In recognition of the Belle Island Accord, Belle Island is excluded from the study

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area and is not contemplated for development. In addition, Belle Park is subject to the Belle Park Master Plan and no changes have been identified for the park through the NKT project.

A key map showing the lands within the NKT study area that are subject to the proposed amendments is included in Exhibit E.

### **Statutory Public Meeting**

This recommendation report forms the basis of a statutory public meeting at Planning Committee. Anyone who attends the statutory public meeting may present an oral submission, and/or provide a written submission on the proposed application. Also, any person may make written submissions at any time before City Council makes a decision on the application.

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of Kingston to the Ontario Land Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of Kingston before the by-law is passed, the person or public body is not entitled to appeal the decision. If a person or public body does not make oral submissions at a public meeting, or make written submissions to the City of Kingston before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

Planning Committee will consider the recommendations in this report and make its recommendation to City Council at this meeting.

Anyone wishing to be notified of Council's decision on the subject application must submit a written request to:

Niall Oddie, Senior Planner  
The Corporation of the City of Kingston  
Planning Services  
216 Ontario Street  
Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)

### **Overview of Application**

The City of Kingston has submitted applications to amend the Official Plan, Kingston Zoning By-Law Number 2022-62 and former City of Kingston Zoning By-Law Number 8499 in order to implement the final policy recommendations for the NKT project. The proposed amendments include Official Plan policies and zoning regulations to guide infill and intensification in strategic locations within NKT in proximity to express transit and active transportation routes, identification of mixed-use areas and a new main street commercial area, and the implementation of the planning-related transportation and cultural heritage recommendations through text and mapping changes in the Official Plan and/or the zoning by-laws, as applicable.



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The proposed Official Plan and Zoning By-Law amendments are supported by a number of technical studies including a Mobility Plan, Servicing Study, Cultural Heritage Study and a Financial and Implementation Strategy.

All supporting information, including the technical studies and tracked changes versions of the proposed amendments to the Official Plan, Kingston Zoning By-Law Number 2022-62 and former City of Kingston Zoning By-Law Number 8499, can be accessed from the following Development and Services Hub (DASH) record: [D35-001-2025](#). Within this record window on DASH, choose “Supporting Information” from the “Record Info” drop-down menu to access the application materials.

A more detailed summary of the proposed amendments to the Official Plan, Kingston Zoning By-Law Number 2022-62 and former City of Kingston Zoning By-Law Number 8499 are provided below.

### **Proposed Official Plan Amendment**

The proposed Official Plan amendment includes modifications to existing policies and map schedules, in conjunction with the addition of new policies and map schedules to guide redevelopment within NKT. Exhibit A contains the draft By-Law to implement the proposed Official Plan amendment, whereas Exhibit H presents the proposed amendment within the context of the existing Official Plan including tracked changes text. The existing Official Plan designations for the lands subject to the amendments are provided within Exhibit F. The proposed modifications are summarized as follows:

- **New Mixed-Use Designation**

NKT has historically contained a wide range of land uses, often within proximity to each other – such as residential uses in proximity to commercial or light industrial uses. The proposed land use changes build upon this characteristic by proposing a new “Mixed-Use” designation for the majority of the intensification areas as well as some adjacent areas. Some of these areas are currently located within an industrial land use designation as further discussed below. The proposed designation would permit a range of residential uses, supporting commercial uses and the potential for compatible light industrial uses. This designation would provide greater flexibility to property owners and better enable future development applications to respond to market demands and fulfill community needs. Ground floor commercial uses will be required in certain locations. A new Section 3.4.H is proposed to be added to the Official Plan with a set of policies applicable to the Mixed-Use designation.

- **Environmental Protection Areas and Open Space**

The NKT project proposes to retain all existing areas designated Environmental Protection Area and expand the Open Space designation to capture existing components of the Waterfront Trail and Veteran’s Field. It is important to note that the majority of the

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shoreline along the Great Cataraqui River is held in public ownership and protected from private development.

- **Main Street Commercial Designation**

Lands along Montreal Street between James Street and Raglan Road are proposed to be redesignated as “Main Street Commercial” to permit small scale commercial uses, while retaining the existing low-rise residential uses. This change is intended to provide property owners the flexibility to convert the ground floor of existing buildings into small scale commercial uses. Ground floor commercial uses would not be required. The proposed boundary of the Main Street Commercial designation is influenced from the Cultural Heritage Study, which indicated this area formerly contained a higher concentration of commercial uses and functioned as a supporting main street for the surrounding residential neighbourhoods.

- **Industrial**

Approximately 18 hectares of land are proposed to be redesignated from the Business Park Industrial and General Industrial designations to the new “Mixed-Use” designation. These lands generally represent vacant or underutilized lands that were formerly developed for industrial uses that have now ceased to exist. The redesignation of these lands is intended to better utilize these centrally located lands for mixed-use intensification. It is recognized that industrial uses are vital to a diverse economy and that the existing Old Industrial Area contains a breadth of long-standing, successful industrial uses that are intended to remain. Land use compatibility between industrial uses and adjacent sensitive uses will be implemented through separation requirements in the zoning by-law and further examined through technical studies associated with future *Planning Act* applications. Additional discussion on the conversion of employment lands is contained within Exhibit I.

- **Existing Site-Specific Policies**

Modifications are proposed to Schedule 3-D to remove lands from existing Site-Specific Policy Areas 6 and 8 to ensure these areas benefit from the proposed policies within Section 10H. Text amendments are proposed to ensure the policies reflect the lands remaining within each Site-Specific Policy Area. Modifications are also proposed to Section 3.17.8 to remove the reference to Section 37 of the *Planning Act* related to density bonusing as due to legislative changes to Section 37 of the *Planning Act*, the density bonusing policies are no longer applicable. A minor amendment to Site-Specific Policy Area 17 is proposed to replace the reference to Wellington Street Extension with a reference to a future active transportation corridor.

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- **North King's Town Specific Policy Area**

The amendment proposes to create Schedule NKT-1, which identifies lands intended for residential and mixed-use intensification. The lands shown on Schedule NKT-1 would be subject to the policies of new Section 10H – North King's Town Specific Policy Area.

- **Transportation Amendments**

The amendment proposes to remove the southern portion of the Wellington Street Extension (WSE) (south of Rideau Street and Railway Street) from the list of major road projects and Schedule 4 of the Official Plan. A text amendment has been included to clarify that these lands have been re-envisioned as an active transportation corridor, which is already shown on Schedule 5 of the Official Plan. The northern portion of the WSE between Railway Street and John Counter Boulevard, now referred to as New Road 1 as it no longer connects to Wellington Street, would be carried forward for further consideration as part of city-wide transportation planning (i.e. the city-wide Integrated Mobility Plan) in the context of new growth projections prior to making a determination on the future road. Schedule NKT-1 identifies various "Active Transportation Desire Lines" where the City intends to secure an active transportation connection through private lands as part of future development applications, which will help expand the active transportation network. Further, various text amendments will encourage development applications to utilize transit service to reduce automobile usage, which would be further encouraged by a reduction in minimum parking rates for residential uses.

- **Cultural Heritage Amendments**

The amendment proposes to establish a policy framework to guide the redevelopment of the Outer Station lands, which highlights that additional information is required prior to confirming the amount or location of development within the Outer Station lands. The location of protected views on Schedule 9 are proposed to be amended by shifting the protected view along Raglan Road one block north, as the view of the Great Cataraqui River along Raglan Road is already impacted. An expansion to the existing St. Lawrence Ward Heritage Character Area is proposed, which would include additional properties with similar characteristics. The accompanying policies would be updated to discourage demolition, encourage adaptive reuse of buildings, highlight that *Planning Act* applications may require a heritage impact assessment or urban design study to assess potential impacts on the character of the area. The expanded St. Lawrence Ward Heritage Character Area would be used as the basis for a future heritage conservation district study area.

## **Intensification Areas**

Development of intensification areas will be subject to the policies of the applicable land use designation (i.e. Mixed-Use or Residential) and the policies of new Section 10H – North King's

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Town Specific Policy Area. The intensification area locations and applicable draft policies are contained within Exhibits A and H.

The intensification areas were identified based on various criteria, including:

- **Size:** Smaller parcel sizes can hinder redevelopment as additional land may be required to accommodate density, parking and amenity areas. Therefore, intensification areas are generally composed of larger parcels to provide greater flexibility for redevelopment.
- **Utilization:** Properties that are vacant or underutilized have greater development potential than properties that are fully developed with active commercial or residential uses.
- **Location:** The early phases of the project identified the intersections of Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street and Railway Street as appropriate nodes of redevelopment. Properties located near these intersections were generally prioritized for redevelopment regardless of their size or utilization. Limited intensification areas have been identified within low-rise residential neighbourhoods where the parcel size represents a substantial opportunity for infill development.
- **Development Interest:** Through the consultation processes, staff spoke with a number of property owners who were interested in the redevelopment of the lands. These properties were considered for intensification areas.

As outlined in Exhibit N, public comments have generally been supportive of the identified intensification areas.

These intensification areas have the potential to provide a significant amount of growth in the urban area of the City. Table 1 below provides an estimate of the amount of growth that could be accommodated within the broader NKT area, which includes the identified intensification areas and other areas subject to current development applications or existing Official Plan policies that would permit development. The growth potential identified within the below table was included within modelling for the Mobility Plan and Servicing Study.

**Table 1 - Summary of Potential Growth within North King's Town**

Use	Existing (2023)	Future (2036+)	Change
Residential (Units)	4,308	11,688	7,380
Residential (Population) (1.74 people per unit)	7,496	20,336	12,841

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Use	Existing (2023)	Future (2036+)	Change
Commercial (square metres)	67,325	103,156	35,832
Commercial (Jobs) (1 job per 35 square metres)	1,924	2,947	1,024
Industrial (square metres)	136,564	172,272	35,708
Industrial (Jobs) (1 job per 75 square metres)	1,821	2,297	476

The residential units associated with the intensification areas were determined based on the assumed built form for each intensification area and then multiplying by an estimated density for that built form. Table 2 identifies the densities that were applied to each built form and were based on a review of recent development applications across the municipality. As shown in Exhibits B and J, the zoning by-law would establish maximum residential density for each intensification area.

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**Table 2 - Residential Density by Built Form**

<b>Built Form</b>	<b>Density (dwelling units per net hectare)</b>
Townhouse	60
Stacked Townhouses	90
Low-Rise Apartments	120
Mid-Rise Apartments	175
High-Rise Apartments – Large Floorplate	200
High-Rise Apartments - Tower	300

As per the Population, Housing and Employment Growth Analysis Study completed by Watson & Associates Economists Limited ([Report Number PC-24-051](#)), Central Kingston is anticipated to accommodate an additional 10,100 permanent housing units, 20,500 new residents, and 11,850 jobs between 2021 and 2051. In addition to this permanent population and housing growth, the post-secondary student population and housing forecast of 6,400 people and 2,300 housing units is also anticipated to be accommodated within Central Kingston. The potential growth within the NKT area will contribute toward this overall growth. It is, however, recognized that the growth numbers presented in Table 1 above represent full build-out and that not all property owners will take advantage of the increased land use permissions. As such the population, housing and job growth within the NKT area will likely be less than what has been included in Table 1.

### **Building Heights**

The NKT project has acknowledged that increased height and density may be required to help offset the remediation costs anticipated for many of the intensification areas. Most intensification areas are contemplated for mid-rise buildings between four and six storeys in height, with additional height focused within centralized nodes of redevelopment, as shown in the conceptual massing models in Exhibit K. The Kingston Zoning By-Law will regulate maximum building height in accordance with the Official Plan guidance.

Building heights within “infill sites” identified on Schedule NKT-1 are intended to be generally four storeys in height, which is generally one storey greater in height than currently permitted

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within the surrounding low-rise residential zones. This approach would enable these infill locations to be redeveloped at densities greater than the existing residential development, while maintaining a compatible built form that considers the surrounding neighbourhood and heritage attributes of the area. The implementing zoning by-law would establish setbacks and performance standards similar to those found in surrounding residential zones.

The intersection of Montreal Street and John Counter Boulevard contains existing residential apartment buildings ranging between 9 and 11 storeys in height. The NKT project contemplates maximum building heights that are consistent and compatible with the existing building heights within this area. The building height permitted at this intersection will encourage high-rise development to locate within a mixed-use cluster. Staff have consulted with Parks Canada, who has indicated that building heights of 12 storeys could be supported at this intersection, whereas greater building heights would start to negatively impact the cultural heritage landscape of the Rideau Canal.

The majority of intensification areas around the intersection of Montreal Street, Rideau Street and Railway Street are contemplated for mid-rise development, with maximum building heights generally up to six storeys. Conflicting public comments were received through the various engagement events regarding the appropriateness of increased building heights within this node, with some respondents preferring building heights greater than 20 storeys while others sought a maximum of six storeys to facilitate human-scale development. Staff are recommending that building heights be permitted to transition from mid-rise up to generally 15 storeys in height in proximity to the Montreal Street, Rideau Street and Railway Street intersection, where adequate separation from low-rise residential and cultural heritage resources can be achieved and where parcel sizes permit. Minor variance applications could be used to increase maximum building height within this area to generally 20 storeys, subject to criteria outlined in proposed Section 10H, including: no negative impacts on the Rideau Canal; no significant shadowing on surrounding residential properties; inclusion of an appropriate transition to adjacent low-rise residential uses; any additional units can be supported by transportation and servicing infrastructure; and the inclusion of a signature architectural design that includes a variety of building heights that contribute to an interesting and varied skyline. The locations identified for high-rise buildings are appropriate given the transition to existing low-rise residential, the anticipated need for additional density to offset remediation costs, proximity to express transit and active transportation infrastructure, and proximity to recreation and open space for future residents.

Concerns relating to building height in this area are summarized below, together with a brief response:

- **Overlook of Residential Properties:** High-rise buildings have been situated to avoid overlooking the rear yard of existing low-rise residential buildings beyond intensification areas. The zoning by-law would further require minimum setbacks to be achieved.
- **Overlook of Belle Park:** Concerns were raised that high-rise buildings in proximity to Belle Park would impact park users' ability to enjoy the park setting. Staff respectfully disagree that this is a substantial planning concern.

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- **Visibility of High-Rise Buildings:** Concerns were raised that high-rise buildings in this location would be visible along the length of Montreal Street and surrounding area. Staff agree that high-rise buildings will be visible from surrounding lands but disagree that this is not an appropriate location for high-rise buildings. The built form policies and zoning requirements would seek to ensure these buildings are appropriately designed.
- **Bird Flight Patterns:** Concerns were raised that high-rise buildings in proximity to the natural habitats along the Great Cataraqui River and associated migratory bird flight paths would increase bird strikes. The recommended locations for high-rise buildings are not situated directly along the shoreline or natural areas. Mitigation measures can be factored into the design of buildings to reduce collisions.
- **Wind Impacts:** Concerns were raised that high-rise buildings contribute to wind tunnels for pedestrians. Development applications involving high-rise buildings are typically required to submit a wind study to demonstrate resulting conditions do not generate adverse effects. The setbacks and tower separation distances regulated through the zoning by-law assist in mitigating wind impacts.
- **Shadowing Impacts:** Concerns were raised that high-rise buildings contribute to increase shadowing for pedestrians and surrounding property owners. Development applications involving high-rise buildings are typically required to submit a shadow study to demonstrate the extent of impacts on surrounding properties, including the public realm. Buildings can be designed and shifted within the property to mitigate impacts.

## Built Form

The built form between the intensification areas is intended to vary to reflect the existing and planned characteristics of the surrounding lands, as explained below:

- **Montreal Street and John Counter Boulevard Intensification Area**

The built form within this node is intended to be high-rise buildings with larger format floorplates, similar to the existing high-rise buildings within the area. Development is intended to enhance the pedestrian realm by locating buildings close to the front lot line, requiring commercial uses along the ground floor and requiring the building to setback above the fourth storey to reduce the visual mass. Buildings would be encouraged to provide vertical and horizontal articulation to promote visual interest and provide substantial amount of glazing along the ground floor commercial areas to integrate the public and private realms and contribute to functional commercial spaces. To minimize the effects of parking on the pedestrian realm, parking would be permitted within the rear and interior yards only. Exhibit K displays conceptual massing models of these buildings.

- **Montreal Street, Rideau Street and Railway Street Intensification Area**

The high-rise buildings within these intensification areas are intended to be in a tower/podium format to assist in reducing the visual mass of the buildings. Streetwall heights will be a maximum of four storeys, with the majority of built form anticipated to be mid-rise, generally being a maximum of six storeys. Where additional height is permitted,



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upper storeys will be required to stepback above the fourth storey to ensure the pedestrian realm maintains a human-scale. Towers will be required to provide further setbacks above the podium bases. Buildings will be situated close to the front lot line to assist in framing the street, while maintaining a sufficient setback to accommodate outdoor seating areas, patios, commercial signage, bike racks and other components associated with the ground floor commercial uses. Sidewalks through this section are intended to be wider to accommodate street furniture, landscaping and street trees, while maintaining sufficient space for pedestrian movements. To accommodate the needs of the mixed-use area on Montreal Street that serves areas of future intensification, the planned road widening width for Montreal Street in Table 1 of the Official Plan is proposed to be increased from 26.2 metres to 30.5 metres from Railway Street to Russell Street.

Large areas of redevelopment will be encouraged to provide mid-block connections to break up the visual mass and enhance pedestrian connectivity through the redevelopment. Exhibit K shows conceptual massing models of these buildings.

- **Infill Areas**

The intensification areas that are located within existing residential neighbourhoods are intended to be developed as infill developments, drawing design inspiration from the surrounding buildings. While building heights and form may vary from the surrounding, the developments are intended to maintain similar setbacks and will be encouraged to incorporate similar design elements, such as materials and colours, into their designs. The City may require an urban design study to be submitted with these applications to demonstrate the site has been appropriately designed.

### **Transition to Adjacent Lands**

The zoning by-law will establish setbacks that are appropriate for the use and massing of buildings. Planting strips will be required between proposed development and various zones to provide additional screening. Development applications will be encouraged to locate amenity area and landscape open space in yards abutting low-rise residential uses to help buffer the transition between existing and proposed uses.

### **Compatibility**

Section 2.7 of the Official Plan identifies various land use compatibility principles, ranging from shadowing and loss of privacy to increased wind speed and noise. The Official Plan defines 'Compatible' as:

“The ability of various land uses, buildings, sites, or urban design treatments to co-exist with one another in a manner that will not have an undue physical or functional adverse effect on, existing or proposed development in the area, or pose an unacceptable risk to environmental or human health.”

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The preceding sections of this report have discussed the mitigation measures that can be used to achieve compatibility between built forms. With respect to compatibility of land uses, the Province directs residential and industrial uses to be separated to mitigate noise, dust, odour, vibration, and other similar conflicts. The minimum separation distances required by the Province are determined by the intensity and type of industrial use, which are referenced in Section 2.7.5 of the Official Plan. Several intensification areas within NKT are in proximity to existing industrial uses or lands designated for industrial uses. Where minimum separation distances cannot be achieved, development applications may be required to implement specialized mitigation measures into building and site design to reduce separation distances, where appropriate. Development of residential uses may be constrained for certain intensification areas if land use conflicts cannot be mitigated through site and building design. The zoning by-law will implement separation distances between industrial uses and sensitive uses as per the Province's D-Series Land Use Compatibility Guidelines.

### **Proposed Zoning By-Law Amendment (Kingston Zoning By-Law Number 2022-62)**

The existing zoning for the proposed intensification areas and areas proposed to be redesignated to either a Mixed-Use, Main Street Commercial, or an Open Space designation is shown in Exhibit G. An amendment to the zoning by-law is proposed to implement the proposed Official Plan amendment related to the intensification areas and the Main Street Commercial areas. Proposals for new mixed-use development on sites located within the Mixed-Use designation not identified for intensification would need to proceed via site-specific zoning by-law amendments. The proposed zoning by-law amendment is outlined in Exhibits B and J and summarized below:

- **New Mixed-Use Zones**

Three new Mixed-Use (MU) zones are proposed to implement the permitted uses and built form contemplated for the Mixed-Use designation. These zones would all permit the same range of residential, commercial and appropriate light industrial uses; however the performance standards and maximum building heights would vary between the zones.

- **Mixed Zone 1 (MU1)**

This zone would permit mixed-use development up to six storeys in height and would be applied to nodes where redevelopment is contemplated within a broader area. This zone would permit a maximum residential density of 175 dwelling units per net hectare and setbacks that would encourage a compact urban built form. Stepbacks would be required above the fourth storey along the front and exterior setbacks to promote human-scale development. Planting strips would be required along rear property lines that abut an Urban Residential, Commercial or Employment zone.

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- **Mixed Zone 2 (MU2)**

This zone would permit mixed-use development up to 15 storeys in height in a tower/podium-built form. This zone would be applied to the limited areas contemplated for high-rise buildings within the Montreal Street, Rideau Street and Railway Street node and would permit a maximum residential density of 300 dwelling units per net hectare. The podium would be required to provide a stepback above the fourth storey along front and exterior setbacks, with an additional stepback required between the tower and podium at the seventh storey. Planting strips would be required along rear property lines that abut an Urban Residential, Commercial or Employment zone.

- **Mixed Zone 3 (MU3)**

This zone would permit mixed-use development up to four storeys in height and would be applied in areas where redevelopment is expected to transition more gradually to existing properties. Maximum permitted residential density would be 90 dwelling units per net hectare. This zone would require greater landscaped open space and permit less lot coverage than the other proposed Mixed Zones. Setbacks and other performance standards would be similar to surrounding development. Planting strips would be required along rear property lines that abut an Urban Residential, Commercial or Employment zone.

- **New Urban Multi-Residential Zone**

A new Urban Multi-Residential 11 (URM11) zone is proposed to apply to most of the infill redevelopment areas within existing residential neighbourhoods. This zone would permit apartments, dwelling units in a mixed-use building, stacked townhouse and townhouses. Neighbourhood commercial uses would be permitted on the ground floor. This zone is similar to the MU3 zone but permits fewer commercial uses and does not permit any light industrial uses. Maximum residential density would be 90 dwelling units per net hectare.

- **Existing Zones**

The zoning by-law amendment proposes to utilize the following existing zones already found within the Kingston Zoning By-Law:

- **Urban Multi-Residential Zone 8 (URM8)**

The permitted uses and performance standards associated with this existing zone were deemed appropriate for an intensification site on Bagot Street. This zone permits six storeys, requires an increased rear setback, stepback above the fourth storey along the front and exterior setback and a planting strip along the rear lot line.

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- **Main Street Commercial Zone (CN)**

The CN zone is proposed along Montreal Street between James Street and Raglan Road to correspond with the proposed Main Street Commercial designation in the Official Plan amendment. An exception is proposed to ensure the continuation of the existing Urban Residential 5 (UR5) zone, which permits low-rise residential uses and a maximum of four residential units subject to a Holding Overlay.

- **Development Reserve Zone (DR)**

The Development Reserve zone is proposed for the portion of the Outer Station property that are contemplated for intensification. As detailed within Exhibits A and H, additional studies will be required to determine the appropriate amount, height and location of development on the subject lands. The DR zone supports this approach as only existing uses would be permitted without a zoning by-law amendment.

- **New Exceptions**

A series of new exceptions are proposed to vary zone requirements, such as increased density, increased height, larger floorplate, or increased setbacks. This approach helps ensure the proposed zoning implements the intended vision of the NKT project.

- **Legacy Exceptions**

A limited number of properties have existing Legacy Exceptions within the Kingston Zoning By-Law, which means that the properties were previously subject to a zoning by-law amendment. These Legacy Exceptions are proposed to either be replaced with a new Exception that carries forward the relevant provisions, or the boundary of the Legacy Exception is proposed to be amended to exclude an intensification area.

- **Separation Distance**

A new section is proposed within the Kingston Zoning By-Law that would specify minimum separation distances between sensitive uses and Class 1, Class 2 or Class 3 industrial uses, in accordance with Provincial guidelines. This proposed section would apply throughout the municipality and will help ensure land use compatibility between sensitive uses and industrial uses. These separation distances may constrain redevelopment of certain intensification areas until the land use compatibility conflict can be resolved or appropriately mitigated. The Province defines Class 1, Class 2 and Class 3 industrial uses as follows:

- Class 1 industrial uses are generally small-scale, self-contained plants or buildings which produce or store a product which is contained within a package and has low

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proximity of fugitive emissions. Examples include beverage bottling, laundry and linen supply, or electronics manufacturing or repair.

- Class 2 industrial uses are generally medium-scale processing and manufacturing with outdoor storage of wastes or materials and there are periodic outputs of minor annoyance. Examples include manufacturing of dairy products, dry cleaning services, or paint spray booths.
- Class 3 industrial uses generally large-scale manufacturing or processing, characterized by large size, outdoor storage of raw or finished products and frequent outputs of major annoyance with high probability of fugitive emissions. Examples include manufacturing of paint or varnish, major breweries, large-scale metal manufacturing.

- **Parking Area Schedule**

An amendment is proposed to the Parking Area Overlay which would reduce the residential parking requirements for the intensification areas by placing them within Parking Area 2 (PA2). Currently, the intensification areas are located within PA3 and PA4. PA2 permits a minimum of 0.4 parking spaces per dwelling unit, whereas PA3 permits a minimum of 0.6 and PA4 permits a minimum of 0.8. Further, PA2 requires 0.03 visitor spaces per dwelling unit, whereas PA3 and PA4 require 0.06 visitor spaces per dwelling unit.

- **Ground Floor Commercial Schedule**

Amendments are proposed to the Ground Floor Commercial Schedule to identify the intensification areas where ground floor commercial uses would be required.

- **Holding Overlay**

Amendments are proposed to add the intensification sites to one of two new holding overlays in Schedule F and Section 22 of the zoning by-law. The intent of these new holding overlays is to ensure future development of the intensification areas does not proceed without confirmation of adequate servicing, the submission of a transportation impact assessment, or, if applicable, confirmation that a Record of Site Condition has been obtained.

- **Express Transit Overlay**

The Express Transit Overlay is proposed to be amended to remove the intensification areas with frontage along Montreal Street and 541 Division Street. This modification is proposed to provide greater clarity on the setbacks, stepbacks and other zoning regulations, rather than using the Express Transit Overlay framework included in the zoning by-law.

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- **Removal of some intensification areas from Employment Zones**

Amendments are proposed to remove lands from the Business Park (M1) and General Industrial (GI) zones from some of the proposed intensification areas to correspond to the lands removed from the Business Park Industrial and General Industrial designations in the Official Plan.

### **Proposed Zoning By-Law Amendment (Former City of Kingston Zoning By-Law 8499)**

The proposed amendment seeks to remove lands intended for intensification from former City of Kingston Zoning By-Law Number 8499 in favour of regulating those lands through the new Kingston Zoning By-Law Number 2022-62. This modification is necessary to ensure these lands benefit from the more permissive uses and associated performance standards contained within the new Kingston Zoning By-Law Number 2022-62. The amendments to former City of Kingston Zoning By-Law Number 8499 are outlined in Exhibit C, and include the removal of intensification areas from Schedule A of Zoning By-Law Number 8499 and the removal of any zones from Table 1 within the by-law where that zone would no longer apply to lands regulated by Zoning By-Law Number 8499.

### **Provincial Planning Statement**

The Provincial Planning Statement (PPS) (2024) provides policy direction on matters of provincial interest related to land use planning and development, which are intended to be complemented by local policies addressing local interests. The *Planning Act* requires all municipal planning decisions to be consistent with the PPS (2024).

A detailed discussion of how the proposed amendments are consistent with the applicable PPS (2024) policies is attached in Exhibit L, with a high-level summary provided below. The proposed amendments:

- Support intensification and redevelopment within settlement areas;
- Provide an appropriate range and mix of housing options and densities;
- Contribute to complete communities by permitting an appropriate range and mix of uses;
- Create multi-modal transportation networks;
- Protect employment areas from incompatible development;
- Promote healthy, active and inclusive communities;
- Conserve natural heritage features;
- Protect water resources; and
- Conserve cultural heritage and archaeological resources.

### **Official Plan Considerations**

A detailed discussion of how the proposed amendments conform with the applicable policies of the Official Plan is attached in Exhibit M, with a high-level summary provided below:

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- A range of appropriate housing options have been identified within appropriate locations;
- Land use compatibility has been considered when identifying new land use designation, zone boundaries and zoning regulations, including the use of increased setbacks, maximum building heights and separation distances between industrial uses and sensitive land uses;
- Climate change has been considered in a variety of ways, including promoting intensification within the urban area, encouraging energy efficient building designs, promoting active transportation and transit opportunities and reducing greenhouse gas emissions through reduced minimum parking regulations;
- The NKT Servicing Study has appropriately considered servicing infrastructure, including identifying areas where improvements will be necessary to support the full build-out of NKT;
- The NKT Mobility Plan has appropriately considered transportation infrastructure, including identifying recommended improvements to the pedestrian, cycling, transit and road networks to support full build-out of NKT with a more sustainable mode-share target;
- The NKT Cultural Heritage Study has appropriately considered tangible and intangible cultural heritage resources, including protections for built heritage resources and proposed expansion to the St. Lawrence Ward Heritage Character Area;
- The proposed policies associated with Section 10H to guide development of the NKT intensification areas build upon and support the urban design policies contained within Section 8;
- Future development applications will be required to submit supporting studies in accordance with the implementation policies contained within Section 9.

### Supporting Technical Studies

The formal *Planning Act* applications noted above form the Land Use component of the NKT project, which was supported by several technical studies. A summary of each technical study is provided below, with more in-depth discussion provided within [Report Number PC-25-001](#). Full implementation of the recommendations of these technical studies, as appropriate, would occur through mechanisms other than the current *Planning Act* applications, such as the Integrated Mobility Plan, Water and Wastewater Master Plan, departmental workplans, and future capital budgets and financial planning exercises.

- **Mobility Plan**

To ensure the transportation networks can accommodate the potential growth within NKT, the Mobility Plan, prepared by ARUP Canada Inc. with support from Dillon Consulting, recommends a future mode share target of 44% automobile trips, 37% active transportation trips, and 19% transit trips. The Mobility Plan provides a series of recommendations for the pedestrian, cycling, transit and road networks to help achieve these mode shares. The recommendations are intended to be phased over time in conjunction with development as the investment of capital to complete the infrastructure and resourcing to support service levels increases will be significant.

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One of the key objectives of the Mobility Plan was to determine whether the construction of the Wellington Street Extension (WSE) was required to support future growth within NKT. Detailed transportation modelling completed in 2019 confirmed the southern portion of the WSE (south of Rideau Street/Railway Street) was not required. Council endorsed these findings through Report Number 19-143 and directed staff to remove the southern portion of the WSE from further consideration within NKT. Additional modelling completed in 2023 determined the northern portion of the WSE (between Railway Street and John Counter Boulevard), now referred to as New Road 1 as it no longer would connect to Wellington Street, provided some relief to Division Street and Montreal Street, but also increased congestion along John Counter Boulevard. New Road 1 would also provide additional access to future employment land redevelopment in NKT which may increase their development potential. The Mobility Plan recommends that New Road 1 be carried forward for further consideration as part of city-wide transportation planning (i.e. the city-wide Integrated Mobility Plan) in the context of new growth projections prior to making a determination on the future road, while the southern portion of the WSE is to be protected for a future active transportation corridor, directly connecting an area of future intensification to the downtown area.

The overall network recommendations are captured in the following high-level summaries:

- **Pedestrian Network**

- Enhance the pedestrian network by constructing new pedestrian pathways, multi-use pathways, and sidewalks to fill gaps within existing routes and establish new connections to transit, planned development and intensification areas, and community destinations.
- Improve pedestrian safety by undertaking intersection improvements, identifying candidate pedestrian crossings at strategic locations and lowering vehicle speeds within residential areas.
- Identifying pedestrian priority corridors with wider sidewalks in areas with planned commercial uses and connecting to transit nodes to create space for pedestrians, transit shelters, benches and other street furniture.

- **Cycling Network**

- Enhance the cycling network by constructing new multi-use pathways, on-street bike lanes, advisory bike lanes and neighbourhood bikeways to fill gaps within existing routes and establish new comfortable routes.
- Improve cycling comfort throughout the study area by focusing on cycling facilities that are separated from vehicle lanes and improving road safety in shared spaces along low-volume roads through measures that prioritize bicycle travel, including reduced speed limits, improved signage, sharrows, traffic reduction measures, speed management measures, and intersection treatments.



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- Reviewing on-street parking policies to create space for on-street cycling facilities and improve cycling safety.
- Transition the southern portion of the former Wellington Street Extension (south of Rideau Street and Railway Street) into an active transportation corridor.
- **Transit**
  - Build upon the success of the Express Transit routes by expanding frequent transit service levels to achieve consistent 15-minute-or-less headways along Division Street and Montreal Street.
  - Build upon the new east/west routes along John Counter Boulevard and Stephen Street to intersect and complement the existing north/south Express Routes along Division Street and Montreal Street.
  - Review opportunities to prioritize transit vehicles along routes, including consideration for the re-prioritization of the existing road platform for queue jumps at intersections, signal priority, and vehicle movement restrictions.
- **Road**
  - Utilize future development applications to establish additional east/west connections by extending Russell Street and River Street to intersect with Rideau Street.
  - Increase road safety by reducing speed limits to 40 km/h within residential areas in the southern portion of the study area.
  - Utilize applicable Planning Act applications to secure road widenings of sufficient width to accommodate all modes of transportation, with an emphasis on active transportation and transit.
  - Reclassify Rideau Street from an Arterial Road (from Montreal Street to River Street) and a Local Road (from River Street to Barrack Street) to a Collector Road.
  - Increase the planned road widening width for Montreal Street in Table 1 of the Official Plan from 26.2 metres to 30.5 metres from Railway Street to Russell Street.
- **Servicing Study**

The Servicing Study, prepared by J.L. Richards & Associates with support from Aquafor Beech, and reviewed by Utilities Kingston, examines the existing servicing conditions, assesses servicing requirements associated with identified intensification areas, and proposes strategies to satisfy those future servicing demands. The Servicing Study confirms that upgrades to the potable water and wastewater networks will be required to support full build-out of NKT intensification areas. There were no specific upgrades identified for the electrical or natural gas networks at this time, however Utilities Kingston will monitor service levels in relation to future demand. The infrastructure upgrades are

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anticipated to be phased over time in conjunction with development pressure. The key findings of the study are summarized below:

- **Potable Water**

Several upgrades to the potable water system would be required to accommodate full build-out of the modelled growth within NKT. It is important to note that these upgrades could be phased over time to reflect demand.

- **Wastewater**

Wastewater from NKT and much of the downtown core of the city is directed to the River Street Pumping Station for treatment at the Ravensview Wastewater Treatment Facility. There are several areas within this catchment where wastewater and stormwater sewers are combined, which can challenge the pumping station during storm events. Several sections of combined wastewater and stormwater sewers would need to be separated to create sufficient wastewater capacity to accommodate full build-out of the modelled growth within NKT. Similar to the potable water upgrades, these separation activities could be phased over time to reflect growth demands. It is important to note that these separation projects were previously identified within the 2017 Water and Wastewater Master Plan updates and already form part of the City's 20-year combined sewer separation plan.

- **Electrical**

It is anticipated that significant upgrades to the electrical infrastructure within NKT and surrounding network will be required to accommodate the full build-out of NKT. Detailed modelling has not been undertaken to identify specific upgrades that may be required given the broader distribution characteristics of the electrical network and the unknowns associated with timing or phasing of development within NKT. In the interim, various mitigation strategies that can be implemented to accommodate growth within NKT, such as shifting development between the 44 kilovolt and 5 kilovolt distribution works, conservation and demand management strategies and the inclusion of sub-stations within development applications. The City and UK will need to monitor electrical capacity within the broader network and phase upgrades accordingly.

- **Natural Gas**

UK does not anticipate any significant constraints associated with the natural gas distribution system due to development of the identified intensification areas within NKT. Detailed modelling was not undertaken as more detailed information relating to phasing and timing of development is required for accurate results. Further, the growing trend in electrification is anticipated to reduce demand for natural gas in the future which would generate capacity for future development.

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- **Stormwater**

Stormwater sewer upgrades are not required to accommodate new development, as development applications are required to match post-development run-off rates to pre-development run-off rates. The Servicing Report provides a series of recommendations to improve the quality of stormwater runoff and promote greater infiltration within the intensification areas. These recommendations will be implemented by the City when reviewing future development applications.

- **Cultural Heritage Study**

The Cultural Heritage Study (CHS), prepared by Bray Heritage, provides a cultural heritage analysis and recommendations for the NKT project and includes analysis of NKT's existing and potential tangible and intangible cultural heritage resources. The report discusses the evolution of the study area from Indigenous occupation, early European and military settlement, construction of Canada's first railway through to residential and industrial expansions in the 20<sup>th</sup> century. The CHS divided the study area into eight sub-areas that conceptually followed neighbourhood boundaries and areas of similar characteristics. Each sub-area was further reviewed to identify the characteristics, document known heritage resources, identify potential heritage resources and consider areas that may be impacted by future development activities. The CHS found that each sub-area contains unprotected heritage resources and provides a series of recommendations for the City to undertake additional analysis to determine if formal protection under the *Ontario Heritage Act* is warranted.

The proposed Official Plan policies contained within Exhibits A and H have incorporated several recommendations from the CHS, including:

- **Heritage Impact Assessments and Urban Design Studies**

In recognition of the concentration of heritage resources in proximity to the NKT intensification areas, applicable development applications will be required to undertake heritage impact assessments to demonstrate no negative impacts on adjacent resources and may be required to submit urban design studies to demonstrate how development proposals have incorporated characteristics from the surrounding neighbourhood, including appropriate massing and setbacks.

- **Redevelopment of the Outer Station**

NKT contains the surviving elements of the former Grand Trunk Railway rail depot (810 Montreal Street), commonly referred to as the "Outer Station", which was the original railway station for the City. The Outer Station greatly contributed to the expansion of the local economy and shaped the physical expansion of the City, with spur lines constructed south along the Inner Harbour and Lake Ontario. The property was designated under the federal *Heritage Railway Stations Protection Act* and is currently designated under Part IV of the *Ontario Heritage Act*, and as

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such demolition of the existing structures is not permitted without approval from the relevant approval bodies. The CHS and heritage staff appreciate the significant historic, contextual, and community value of the Outer Station, and also appreciate the severely dilapidated state of the site. It is understood that the site has a high potential for future development. As such, the CHS considered how appropriate future use might occur, including the potential for adaptive reuse or moving structures within the site.

- **Heritage Conservation District Study**

The CHS found that portions of the Division Street Corridor, Montreal Street Corridor, Bagot Street Neighbourhood, McBurney Park Neighbourhood and Patrick Street Neighbourhood sub-areas likely contain concentrations of heritage resources and recommends that the City undertake additional study to determine if the area should be designated as a Heritage Conservation District under Section 41, Part V of the *Ontario Heritage Act*. An amendment to Schedule 9 of the Official Plan to expand to the existing St. Lawrence Ward Heritage Character Area is proposed to correspond to the proposed future study area boundary. Updated policies are also proposed for the St Lawrence Ward Heritage Character Area.

- **Protected Views**

Schedule 9 of the Official Plan identifies protected views of the Great Cataraqui River along several streets. The CHS recommended that the protected view currently shown along Raglan Road be shifted one block north to be along Corrigan Street, given the view along Raglan Road has already been impacted by development.

- **Financial and Implementation Strategy**

The Financial and Implementation Strategy (FIS), prepared by Hemson Consulting, focused on identifying the most effective financial incentive programs to stimulate private investment within NKT. The key findings from the FIS are summarized below:

- **Brownfield Community Improvement Plan**

The FIS acknowledges the success of the City's Brownfield Community Improvement Program and recommends that the existing suite of financial incentives provided through the program be extended for at least another 10 years to allow sufficient time to encourage and support the rehabilitation and redevelopment of the NKT intensification areas. Additional recommendations are provided to prioritize Brownfield Community Improvement Plan applications, if resources require, to the projects that could function as a catalyst for redevelopment of adjacent lands.

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- **Incentive Programs**

The FIS recommends that the City's existing heritage incentive programs be continued, given the amount of (potential) build heritage resources within NKT. The City currently offers the Heritage Property Grant Program and the Heritage Property Tax Refund Program, which can provide financial assistance to owners of eligible properties for eligible projects. The FIS also recommends that the City consider establishing a façade grant program to provide funding for business and commercial property owners to improve the exterior facades, which may assist in encouraging additional commercial uses within the Main Street Commercial area on Montreal Street between James Street and Raglan Road.

- **Prioritization of Capital Projects**

The FIS provides high-level recommendations on prioritizing future capital projects within NKT to help ensure City investments will be maximized and facilitate redevelopment. The prioritization of future capital projects will need to balance infrastructure projects that are crucial to servicing, with more 'visible' public realm projects (such as Belle Park revitalization and additional multi-use pathways) that enhance the amenity within NKT as these projects will assist in creating momentum for additional private sector investment and market interest in the area.

## **Public Engagement**

Community input into planning processes is important as residents and property owners have detailed knowledge of the area and contribute knowledge from diverse backgrounds. Comments received throughout the NKT project were summarized by event and by theme within Exhibits J and K, respectively, within [Report Number PC-25-001](#). Engagement sessions refined the direction of the land use, cultural heritage and transportation components of the project. Engagement opportunities included:

- **Open Houses**

A total of six Open Houses were held between 2018 and 2024, including in-person and virtual events. The Open Houses presented the available draft materials and collected initial feedback for refinement.

- **Workshops**

A total of four Workshops were held between 2018 and 2023, including in-person and virtual events where participants engaged in small groups facilitated by staff or consultants to discuss specific questions.

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- **Interviews**

Between June and July 2023, staff interviewed property owners of intensification areas and members of the development community who own land or have development interests within the study area.

- **Surveys**

In May and June of 2023 an online survey was conducted on Get Involved Kingston to understand what had changed within NKT, what residents were excited about and the areas of improvement.

- **Indigenous Engagement**

A Talking Circle was held with the local Indigenous community on October 3, 2018, and an additional engagement session was held on April 30, 2024 to collect Indigenous feedback on the project.

- **Skeleton Park Arts Festival**

A booth was setup at the Skeleton Park Art Festival on June 23, 2018, to discuss the project with community members and collect feedback on the approach and draft materials.

- **Drop In Sessions**

Drop-in sessions were held at City Hall and Artillery Park in March 2018 to collect initial community comments on land use, transportation and cultural heritage within NKT.

- **Draft Material Review**

Various draft materials were posted on Get Involved Kingston between March 2018 and April 2024 for public review and comment, including the Cultural Heritage Study, transportation modelling results and recommended networks, mapping and draft policy direction for land use, built form and building heights.

- **Email Correspondence**

A project email ([nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca)) was established to collect community feedback on the project.

- **Direct Mailings**

Staff sent notices to property owners within identified intensification areas advising of open houses and workshops held in 2023 and 2024. The notices offered opportunities for individual meetings with staff to discuss the project.

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- **Community Meeting at Planning Committee**

A Community Meeting was held at Planning Committee on December 5, 2024, to present and seek feedback on the draft Official Plan and zoning by-law amendments.

In addition, 11 Community Working Group meetings have been held since December 2017. During these meetings, the project team presented draft materials and sought community input on various directions.

### Public Comments

The following is a summary of the feedback received at the Community Meeting on December 5, 2024 and additional public input received thereafter. All original written public comments received after the Community Meeting are available in Exhibit N of this report.

- **Climate change:** How does the NKT Plan address climate pressures and meet the City's sustainability goals?

Response: The North King's Town project aims to address climate change in several ways, for example:

- Identifying opportunities for a significant amount of infill, intensification and redevelopment within the existing urban area of the City. This approach reduces pressure on natural heritage systems, conserves resources, contributes to more efficient infrastructure and service delivery, and reduces greenhouse gas emissions by reducing distances residents would need to travel to access places of employment, commerce and recreation.
- Creating a new Mixed-Use designation and enabling mixed use development within all identified intensification areas, which can promote active transportation and reduce greenhouse gas emissions by locating residential uses in close proximity to supporting commercial uses.
- Establishing densities to facilitate mid-rise and high-rise development, which contributes to a more efficient use of land and resources.
- Promoting active transportation and transit use by locating intensification areas along (or within walking distance to) express transit routes and reducing minimum parking requirements for residential developments (while preserving a maximum parking requirement within zoning), which all serve to reduce greenhouse gas emissions.
- Encouraging developments to plant "little forests" within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and contribute to greater biodiversity, which can assist in mitigating urban heat island effects by increasing tree canopy, habitat loss and promote infiltration.

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- Encouraging new development to incorporate community gardens that provide a shared space to grow and harvest fruits and vegetables, herbs, flowers or native plants.
  - Encouraging development applications to include fruit trees, nut trees and other edible plantings within their planting plans to promote food security and local food options, which can reduce greenhouse gas emissions associated with food transportation, while also serving to establish tree canopy within the urban area.
  - Establishing minimum landscaped open space requirements for the intensification sites.
- **Impacts of the Canada Post Strike:** Why was the Community Meeting held while Canada Post was on strike? Residents did not receive notice of the meeting until after the meeting had been held and were not able to participate in the public process.

Response: Staff made their best efforts to mail out the notices prior to the Canada Post strike, however notices could not be delivered in time for the meeting due to the strike. Alternative methods for notifying the public were utilized. Notice of the meeting was also distributed by email to the project contact list, circulated twice in the Kingston Whig-Standard, posted on the [NKT Get Involved Page](#) and on the City's website. Residents who contacted Staff after the meeting were directed to the recording of the presentation and staff report on the City's website, provided instructions on how to access the draft materials on DASH and provided an opportunity to meet with Staff to discuss the project. Residents were informed that another Public Meeting would be held where they could also provide comments on the materials, and that notice of the Public Meeting would be distributed through another direct mailout and published in the Kingston Whig-Standard.

- **Location of Intensification Areas:** The intensification areas are focused at the Montreal Street and John Counter Boulevard and Montreal Street, Railway Street and Rideau Street intersections. Are there opportunities for intensification in other areas of NKT?

Response: While areas around the Montreal Street and John Counter Boulevard and Montreal Street, Railway Street and Rideau Street intersections are intended to be the main nodes of redevelopment, other intensification areas have also been identified based on their size, location, utilization and development interest. The intensification areas are shown on the proposed Schedule NKT-1 within the proposed Official Plan amendment. While these areas have been identified for intensification through the NKT project, this is not to say that there are not other properties that may similarly be able to support infill development through private development applications. Private development applications will be required to demonstrate compliance with applicable policies and will be reviewed in the context of the PPS, Official Plan policies and Zoning By-Law regulations in effect, while also satisfying any separate study requirements (e.g., servicing, transportation, etc.) that may be identified through the planning process to support the application.

- **Over Regulation of Development:** Why have Staff included maximum building heights within both the Official Plan and Zoning By-Law amendments, and included maximum



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densities within the proposed zoning by-law? These regulations will be constraining to development and either result in unnecessary *Planning Act* applications to adjust the regulations or prohibit development altogether.

Response: Staff have modified the proposed Official Plan amendment to remove specific height maximums, in favour of more general guiding language around maximum building heights. Staff have retained maximum building heights and density within the proposed Zoning By-law amendments as these regulations provide clarity for property owners and residents on the expected built form within these areas. Further, the maximum density regulations ensure the City's Planning, Transportation and Servicing plans are in alignment. Relief from both density and building height requirements may be requested through a minor variance application, where supported by technical studies and appropriate technical review. The City commonly includes both maximum building heights and maximum density regulations within zones throughout the municipality.

- **Urban Design:** Questions and comments around proposed design or mid-rise and high-rise buildings shown in the massing models and concern with compatibility with surrounding area.

Response: The massing models that were prepared are conceptual and intended to illustrate policy concepts, such as the use of podium bases, tower floorplates, setbacks and stepbacks. The conceptual modelling may not reflect the ultimate build out of the area. The built form contemplated within the intensification areas has considered the existing and planned characteristics of the surrounding lands. Mid-block connections will be utilized within larger developments to increase pedestrian circulation and connectivity and alignment of mid-block connections between abutting developments will be encouraged to improve pedestrian movement. For example, the Montreal Street, Rideau Street and Railway Street Intensification is intended to be in a tower/podium format to assist in reducing the visual mass of the buildings. Streetwall heights will be a maximum of four storeys, with the majority of built form constructed generally to a maximum of six storeys. Where additional height is permitted, upper storeys will be required to stepback above the fourth storey to ensure the pedestrian realm maintains a human-scale. Towers will be required to provide further stepbacks above the podium bases. Buildings will be situated close to the front lot line to assist in framing the street, while maintaining a sufficient setback to accommodate outdoor seating areas, patios, commercial signage, bike racks and other components associated with the ground floor commercial uses. Rear setbacks will be increased to create separation between new development and existing low-rise residential uses. Sidewalks through this section are intended to be wider to accommodate street furniture, landscaping and street trees, while maintaining sufficient space for pedestrian movements. To accommodate the needs of the mixed-use area on Montreal Street that serves areas of future intensification, the existing road widening in Table 1 of the Official Plan is proposed to be increased from 26.2 metres to 30.5 metres from Railway Street to Russell Street. The location of tree planting has not been identified as part of this work as grading, utility locations and transportation entrances need to be

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considered. However, the increased road allowance width and front yard setbacks creates space for pedestrian realm improvements throughout this area.

The design concepts around podiums and towers, and location of mid-rise and high-rise buildings, are further discussed in the Design Guidelines for Residential Lots (2015) and Density by Design – Issues and Options Report. As identified in the Design Guidelines for Residential Lots, high-rise, multi-unit buildings, including mixed-use buildings, are best developed at key locations (e.g. major intersections, adjacent to Arterial Roads, within new and existing centres and corridors). In addition, as identified in Density by Design – Issues and Options Report, one of the most common ways to de-emphasize the height and mass of mid-rise and high-rise is to step back upper floors. This makes the upper floors less obvious and influential on the perception of height and mass of the building for pedestrians on the street. These modifications also enable greater penetration of sunlight and improve sky views and can improve compatibility with and transition to adjacent lower-rise buildings, including built heritage resources. The zoning by-law regulations that would be applied within these intensification areas contain appropriate setbacks to help ensure compatibility between the intensification areas and adjacent development.

- **Height:** Various questions relating to building heights, including whether regulations to limit the number of towers on a property, whether zoning by-law amendments to increase building height could be prohibited, and whether Parks Canada was supportive of high-rise buildings in proximity to the Rideau Canal.

Response: Phase 1 of the NKT project identified the need to include additional building height and density to offset remediation costs anticipated for many of the intensification areas. Through Phase 2 of the project, increased building heights have been proposed along the Montreal Street corridor, especially around the nodes at Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street and Railway Street. Most intensification areas are contemplated for mid-rise buildings between four and six storeys in height, with additional height focused within centralized nodes of redevelopment and is generally consistent with public comments that have been received.

Building heights greater than six storeys have been clustered around the Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street and Railway Street intersections to form nodes of compact mixed-use development helping to locate future populations along Express Transit Corridors and proximity to supporting commercial amenities, open space and active transportation networks. Additional building height around the Montreal Street, Rideau Street and Railway Street intersection could be considered in appropriate locations through a minor variance application. The proposed zoning currently does not establish a maximum number of towers that could be constructed within the zones that permit high-rise development, but does limit development through other zoning regulations, such as density, setbacks, tower separation distances, lot coverage, and setbacks.

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The *Planning Act* establishes processes for property owners to submit applications to amend the Official Plan and Zoning By-laws and requires municipalities to consider and issue a decision on these applications within a timely manner. The City cannot prohibit property owners from submitting applications under the *Planning Act*.

Parks Canada was consulted on building heights within NKT. Heights up to 12 storeys could be supported near Montreal Street and John Counter Boulevard. There were fewer concerns with high-rise buildings near the Montreal Street, Rideau Street and Railway Street intersection given the increased separation to the Rideau Canal.

- **Natural Heritage:** Comments that NKT should consider daylighting to expose buried or partially buried streams in the area and questions on how they can be considered in the development of NKT policy. Additional questions relating to the inclusion of Little Forests and community gardens were also received.

Response: The NKT project did not consider the daylighting of buried watercourses as this was beyond the scope of the servicing study. There may be future opportunities through different City projects where daylighting of streams is possible. However, it will be important to consult with Cataraqui Region Conservation Authority (CRCA) to ensure that daylighting those streams would not create floodplain hazards. The stormwater management chapter of the Servicing Study prepared by J.L. Richards provides a series of lot-level recommendations to reduce run-off volume and improve the quality of run-off leaving development sites, which will help to improve the quality of water entering the Great Cataraqui River. It is important to note that the daylighting of any watercourse would not necessarily transition that space into a public green space – if this were to occur on privately owned lands, it may remain as private property.

The proposed Official Plan amendment would include policies within Section 10H that would encourage development applications to include 'Little Forests' within their planting plans, especially those adjacent to existing treed areas. The proposed policies would also encourage planting plans to consider fruit trees, nut trees and other edible plantings to facilitate opportunities for foraging and help promote food security. A new policy has been included encouraging development applications to include community gardens that provide a shared space to grow and harvest produce, flowers and native plants.

- **Commercial Uses and Tourism Opportunities:** How was tourism, hotels and unique commercial opportunities considered, as these will help support the surrounding community as well?

Response: The proposed mixed-use zoning for the area includes various types of permitted uses that can be contemplated for future redevelopment of the lands in the area, including hotels. The inclusion of required ground floor commercial ensures new commercial opportunities will be provided within NKT to support new residential units and existing surrounding residential uses and accessible through various modes of transportation, including active transportation. The proposed Main Street Commercial

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area on Montreal Street between James Street and Raglan Road is intended to create flexibility to establish smaller-scale commercial uses to support the surrounding community.

- **Ground Floor Commercial:** The amendment should not include requirements for mandatory ground floor commercial uses and instead should permit commercial uses while requiring increased floor to ceiling heights. The amount of ground floor commercial uses cannot be supported.

Response: The proposed ground floor commercial areas within the proposed Official Plan and Zoning By-law amendments have been retained. The intensification areas will permit the opportunity for a significant number of new residential units to be added to these areas and the ground floor commercial requirements are intended to ensure that these future residents have access to commercial supports and amenities within walking distance. Further, the existing residential units within these areas would benefit from new commercial spaces being added to the area as it will reduce the distance, they need to travel to access commercial amenities. The proposed ground floor commercial requirements would not require every linear metre of these areas to be constructed with commercial uses – there will be gaps within buildings, there may be parkland dedications, there may be new internal roads and mid-block connections established, development will need to comply with maximum lot coverages. Within the MU1 and MU2 zones, the minimum floor to floor height of the first storey is proposed to be 4.5 metres to help ensure functional commercial spaces.

- **Brownfield Redevelopment:** What is the extent of environmental contamination of these properties and what will the cost of remediation be?

Response: The environmental condition of the various intensification areas is unknown at this time; however, it is anticipated that most redevelopment will require environmental remediation in accordance with provincial requirements. The intensification areas are located within the existing Brownfield Community Improvement Plan. Anticipated remediation costs would not be known until detailed assessments are completed for each development application.

- **Tannery:** Do any of the NKT amendments relate to the Davis Tannery development application that was appealed?

Response: The Official Plan and Zoning By-Law amendments associated with the NKT project do not apply to the Davis Tannery lands.

- **Traffic and Parking:** Will the development of intensification areas near the intersection of Montreal Street and John Counter Boulevard exacerbate existing traffic congestion?

Response: The NKT Mobility Plan considers the transportation networks in relation to future growth and provides various recommendations for the pedestrian, cycling, transit

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and road networks to support a more integrated and multimodal transportation system. These recommendations are intended to better align land use and mobility by improving access to active and transit-oriented transportation options and reducing reliance on private automobiles. Recent improvements were made in 2024 to the intersection of Montreal Street and John Counter Boulevard to improve traffic flow, with additional improvements planned as part of remaining work associated with the Waaban Crossing including enhancements to active transportation and transit connectivity.

- **New Road 1** (formerly known as the northerly portion of the Wellington Street Extension): The construction of New Road 1 would come with significant costs and the modelling analysis demonstrates the road would provide limited time savings, while increasing congestion in other areas. Why has this project not been cancelled, similar to the southern portion of the Wellington Street Extension?

Response: The proposed amendments retain New Road 1 within the Official Plan as a major road project, subject to additional city-wide analysis. The analysis of the modelling completed within NKT was generally limited to the lands contained within NKT. The additional city-wide considerations, including updated growth projections, will provide more information on the potential benefits or impacts of New Road 1 on the broader transportation network, beyond NKT. Further, the amendments note that a partial local road extending Hagerman Avenue to the north may be considered by the City as part of a future plan of subdivision or other development application, which could improve access and development potential for industrial lands that currently have limited road frontage.

- **Southern Portion of Wellington Street Extension:** Clarification was requested on what the active transportation corridor contemplated within the southern portion of the Wellington Street Extension may look like, and what forms of transportation would be able to use this corridor.

Response: Active transportation in this context refers to bicycles, e-bikes, assistive devices, walking, rollerblading, etc. Public transit is not considered active transportation and city buses would not be permitted to utilize any future active transportation corridor through these lands. A concept has been included in the Mobility Plan that includes dedicated bi-directional cycle lanes, a wide pedestrian pathway, open green space, lighting, seating and bicycle parking. Public engagement would form part of any future design process.

- **Active Transportation Desire Lines:** The active transportation desire lines shown on Schedule NKT-1 should be removed as there are already policies relating to mid-block connections. Showing these lines on Schedule NKT-1 will constrain development of these intensification areas.

Response: The Active Transportation Desire Lines shown on Schedule NKT-1 are important future connections for the active transportation networks identified within the NKT Mobility Plan. Including the mapped desire lines is important for providing a clear

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visual reference and reflects the input Staff received in building out more of a grid network within the northern portion of the study area, providing viable access to frequent transit along Division and Montreal Streets. Amendments have been incorporated into proposed policy 10H.2.36 that clarify that the City will collaborate with the property owner to integrate these features into the development proposal, to ensure the necessary connections are secured while not unnecessarily constraining development. Staff will work with the development community when locating these future connections, however it is important to show them on the schedule for transparency purposes.

- **Affordability:** How will NKT help address housing affordability?

Response: NKT seeks to address housing affordability by identifying opportunities for a significant number of new residential units, which would help to increase housing supply and reduce demand for existing units. These City-initiated amendments serve to establish the principle of development for these lands, which means that development applications would only need to complete their detailed design studies as part of hold removal and site plan control applications (provided no Official Plan or zoning by-law amendments are sought). This process will help to create certainty for developers (which reduces risk) and greatly accelerate the development approvals process (which reduces cost). It is hoped that these initiatives will help incentivize development applications and the cost-savings associated with the more efficient process would translate to lower housing costs.

The NKT project aligns with the Council Strategic Plan objective to “Support Housing Affordability” and more specifically, the goal to “Promote increased supply and affordability of housing”. Supporting the affordability of housing is a broader objective to reduce the cost of all forms of housing and is different than Affordable housing – which generally relates to units where accommodation costs do not exceed 30% of annual household income for low- and moderate-income households.

The proposed NKT Official Plan policies do not directly address affordable housing targets as affordable housing is a City-wide concern, rather than focused within a small geographic area, such as NKT. Sections 3.2.18 and 3.2.19 of the Official Plan provides City-wide affordable housing policies and objectives. The City’s new Official Plan project is investigating new tools and processes to help secure affordable housing units within the municipality.

- **St. Lawrence Ward Heritage Character Area:** What does the expanded St. Lawrence Ward Heritage Character Area mean for my property?

Response: These proposed policies would discourage the demolition of buildings within this area and encourage adaptive reuse of buildings to help conserve the character of the area. Further, development applications requiring *Planning Act* approval may be required to submit heritage impact assessments or urban design studies to mitigate the impacts of development on the character of the area. The proposed policies also identify the intention for a future heritage conservation district study to determine if the area satisfies

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the criteria to be designated under the *Ontario Heritage Act*. The modification relating to the St. Lawrence Ward Heritage Character Area is not a formal designation under the *Ontario Heritage Act*.

- **Outer Station:** Have the proposed NKT amendments done enough to conserve the Outer Station, considering the policies may permit the relocation of the Outer Station buildings?

Response: The proposed amendments indicate that additional cultural heritage investigation is required before confirming the amount of redevelopment appropriate for the Outer Station lands. The proposed Official Plan amendment would create policies specific to the Outer Station and describe how the conservation of the built heritage resources should lead any redevelopment proposal for the lands. Similarly, the proposed Zoning By-law amendment places the lands within a Development Reserve zone, which ensures a future zoning by-law amendment will be required prior to any development of the lands. With respect to the potential relocation of the Outer Station buildings, the proposed policies identify this as a last resort and would only permit the buildings to be relocated within the Outer Station lands themselves and to a location that improves public access. The proposed policies are clear that the City will not support off-site relocation.

- **Servicing Capacity:** Why were the natural gas and electricity networks not considered within the Servicing Study?

Response: The Servicing Study was completed by J.L. Richards and relevant chapters reviewed by Utilities Kingston. Each of the utilities (wastewater, potable water, stormwater, electrical and natural gas) within NKT were analyzed to assess the impacts of the potential growth within NKT and provide recommendations for necessary modifications. Modelling was completed by Utilities Kingston for the wastewater and potable water networks and determined where specific upgrades within NKT would be required to support a full-build out scenario. Utilities Kingston did not complete the same modelling for the natural gas or electrical utilities as these networks function as a scale beyond the NKT study area and require more specific phasing information to determine where upgrades will be required. Utilities Kingston will continue to monitor development applications in relation to servicing capacity.

- **Stormwater Management:** Why have the NKT amendments not included the specific stormwater management targets identified within the Servicing Study? How will stormwater management be implemented within low-laying areas of NKT to avoid flooding?

Response: The City does not typically include technical specifications, such as stormwater management targets, within Official Plan policies as applications that cannot meet these specifications for technical reasons would be required to undertake an Official Plan amendment. Instead, the City identifies the requirement for development applications to undertake these studies within the Official Plan and then provides the

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detailed specifications through the pre-application process. This process ensures the specifications are appropriate for the proposed development and remain consistent with best management practices. The intent is to use the NKT Servicing Study as a guide when reviewing development applications.

NKT has not identified any development applications within the floodplain of the Great Cataraqi River. Development applications are required to demonstrate how stormwater will be accommodated on the subject lands, including the 1:100 year storm event. Development applications will not be approved if technical studies do not support the proposed development.

- **Parkland:** Why have the NKT amendments not identified any new parks to support existing and future residents within NKT, especially in proximity to the Montreal Street and John Counter Boulevard intersection?

Response: New development proposals will be required to provide parkland in accordance with the City's Parkland Dedication By-law and *Planning Act* requirements. Staff have not identified the locations of the parklands within the intensification areas at this time as the location and orientation of the parkland needs to be considered in relation to the development application, servicing requirements, grade changes, etc. which are determined through detailed design of the development applications. The City further utilizes the [Parks and Recreation Master Plan](#) to determine if additional parkland, outside of the development process, is necessary to support neighbourhoods and includes consideration of population density within the surrounding areas.

The Montreal Street and John Counter Boulevard intersection is in proximity to Belle Park, which has an approved master plan to guide the redevelopment and programming within the park. Further, the City has been working to secure additional segments of the Waterfront Trail along the Great Cataraqi River south of the Waaban Crossing, which will contribute to recreation opportunities within this area. The lands on the immediate north and south sides of the Waaban Crossing are City-owned lands and will be designated Open Space through the proposed Official Plan amendments, however detailed plans for these lands are not yet available. Residents are also able to use the Newmarket Lane Parkette along the shoreline of the Great Cataraqi River.

- **Industrial Separation Distances:** The proposed minimum separation between industrial uses and sensitive uses should be removed from the Zoning By-Law amendment as these can be implemented through other means, in accordance with the Province's D-series guidelines.

Response: The proposed Zoning By-Law amendment includes "this separation distance must be measured as per the province's D-series Environmental Land Use Compatibility Guidelines". Staff feel this language would enable Staff to consider the same nuances for determining and identifying separation distances. For example, Section 4.4.4 of D-6 enables parking lots to be included within the separation distance when a sensitive use is



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being added in proximity to an industrial use – the current wording would allow staff to measure from the proposed building to the industrial use, allowing the parking lot to be located within the required separation distance. Further, including the setbacks within the zoning by-law under Section 4.32 enables those distances to be reduced through a minor variance, where appropriate.

- **Expropriation:** Has the City considered expropriation of lands to facilitate intensification within these areas?

Response: Staff have not considered the use of expropriation to facilitate intensification. The proposed Official Plan and zoning by-law amendments will be focused on privately owned lands. Typically, a municipality only expropriates lands if it is for an identifiable municipal purpose, for example, for the purposes of a road.

### Effect of Public Comments

In response to the feedback received both in writing and at the Community Meeting on December 5, 2024, a number of changes have been made to the proposed Official Plan and Zoning By-law. These modifications are reflected within Exhibits A, B, H or J, as appropriate, and summarized below:

- **Building Heights**

Staff received comments expressing concern that maximum building heights would be specified within both the Official Plan and Zoning By-Law amendments. There were concerns that including maximum building heights within the Official Plan would be overly restrictive as minor and appropriate increases in building height could not be implemented through a minor variance application and would instead require an Official Plan amendment, which is a more complicated and costly application process.

Staff agree with this comment and have amended the approach to regulating maximum building heights within NKT. The previously proposed Schedule NKT-2, which identified maximum building heights, has been removed in favour of amended policies that speak to the general location mid-rise and high-rise buildings within the intensification areas shown on Schedule NKT-1. Maximum building heights would continue to be regulated by the Kingston Zoning By-Law, guided by the policies and intent described within the Official Plan. This approach would enable minor variances to consider minor and appropriate increases in height, where applications can satisfy the 'four tests' of a minor variance.

- **541 Division Street and 157 Joseph Street**

This property was identified as an intensification area on Schedule NKT-1 and proposed zoning of URM11 with a site-specific exception. The owner of these lands has requested that the same development permissions be applied to the abutting parcel at 157 Joseph Street. These two parcels are considered 'one lot for zoning purposes' which means they

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would be redeveloped together in a cohesive manner. A number of site-specific exceptions have been included to reflect a development concept for these lands.

- **5-7 Cataraqui Street**

The property owner of 5-7 Cataraqui Street has requested that their lands also be removed from Site Specific Policy (SSP) area #8 to remove the density maximums contained within that policy area. These lands have not been included as an intensification area on Schedule NKT-1 or identified as lands to be redesignated as Mixed Use. The zoning for these lands would remain unchanged.

- **Active Transportation Desire Lines**

Proposed policy 10H.2.36 relating to Active Transportation Desire Lines have been modified to highlight that the City will work collaboratively with the property owner to integrate these features into the development proposal, to ensure the necessary connections are secured while not unnecessarily constraining development.

- **Community Gardens**

A new policy has been added that encourages development applications to establish community gardens, which would provide a shared space to grow and harvest produce, flowers or native plants.

## **Conclusion**

In summary, the proposed amendments to the Official Plan, Kingston Zoning By-Law Number 2022-62 and former City of Kingston Zoning By-Law Number 8499 represent policy changes intended to promote residential and mixed-use intensification within a centrally located neighbourhood with excellent transit and active transportation opportunities, while considering important cultural heritage resources. The proposed amendments have regard for matters of provincial interest set out in the *Planning Act*, are consistent with the Provincial Planning Statement, 2024, conform with an implement the City of Kingston Official Plan and represent good land use planning.

## **Existing Policy/By-Law:**

The proposed amendment was reviewed against the policies of the Province of Ontario and City of Kingston to ensure that the changes would be consistent with the Province's and the City's vision of development. The following documents were assessed:

### **Provincial**

*Planning Act*

Provincial Planning Statement, 2024

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## **Municipal**

City of Kingston Official Plan

Kingston Zoning By-Law Number 2022-62

Former City of Kingston Zoning By-Law Number 8499

### **Notice Provisions:**

Pursuant to the requirements of the *Planning Act*, notice of the statutory public meeting was provided by advertisement in The Kingston Whig-Standard 20 days in advance of the public meeting. As shown in Exhibit O, notices were also sent by mail to all property owners (according to the latest Assessment Rolls) subject to the proposed Official Plan and Zoning By-law amendments and to all property owners within 120 metres of these properties. Additionally, a notice was sent to all individuals on the project mailing list for the NKT project and posted on the NKT Get Involved Kingston page. Finally, a courtesy notice was placed in The Kingston Whig-Standard on February 28, 2025. Further, notice was emailed to all residents who had requested notification or participated in the NKT project.

If the application is approved, a Notice of Adoption and a Notice of Passing will be circulated in accordance with the provisions of the *Planning Act*.

At the time of writing of this report, 30 pieces of written public correspondence have been received and all planning related matters have been addressed within the body of this report. Any public correspondence received after the publishing of this report will be included as an addendum to the Planning Committee agenda.

### **Accessibility Considerations:**

None

### **Financial Considerations:**

As noted in the Mobility Plan and Servicing Study sections of this report, there are major financial implications for future improvements that will be required to support the proposed land use changes. The City will need to monitor development trends and advance corresponding capital budget requests to implement the recommendations for transportation networks and servicing infrastructure improvements on an as-needed basis and in tandem with other planned capital works.

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Joel Konrad, Manager, Heritage Planning

Matt Kussin, Manager, Transportation Policy and Programs

**Exhibits Attached:**

- Exhibit A Draft By-Law and Schedules 'A' to 'I' to Amend the Official Plan
- Exhibit B Draft By-Law and Schedules 'A' to 'G' to Amend Zoning By-Law Number 2022-62
- Exhibit C Draft By-Law and Schedule 'A' to Amend Zoning By-Law Number 8499
- Exhibit D Study Area
- Exhibit E Key Map
- Exhibit F Existing Official Plan – Land Use
- Exhibit G Existing Zoning
- Exhibit H Proposed Official Plan Modifications – tracked changes version
- Exhibit I Employment Land Conversion Rationale
- Exhibit J Proposed Zoning By-law Modifications – tracked changes version
- Exhibit K Conceptual Massing Models
- Exhibit L Consistency with the Provincial Planning Statement, 2024
- Exhibit M Conformity with the Official Plan
- Exhibit N Public Comments Received Since Community Meeting
- Exhibit O Public Notice Notification Map

**By-Law Number 2025-XXX**

**A By-Law To Amend The City Of Kingston Official Plan (Amendment Number 99,  
North King's Town)**

**Passed:** [Meeting Date]

**Whereas** a Public Meeting was held regarding this amendment on March 6, 2025;

**Now Therefore** the Council of The Corporation of the City of Kingston, in accordance with the provisions of Section 17 of the *Planning Act*, R.S.O. 1990, c.P13, hereby enacts as follows:

1. The City of Kingston Official Plan is hereby amended by the following changes which shall constitute Amendment Number 99 to the Official Plan for the City of Kingston:
  - a. **Amend** Section 2.3.3 by deleting clause b. in its entirety and renumbering the subsequent clauses accordingly.
  - b. **Amend** Section 3.2.29. by deleting clause g. in its entirety and renumbering the subsequent clause accordingly.
  - c. **Amend** Section 3.4.C by adding the following policies at the end of the section:

**"Montreal Street Main Street**

- 3.4.C.14.** The Montreal Street Main Street, generally extending from James Street to Ragland Road, has traditionally contained a mixture of residential and local small-scale commercial uses, functioning as a main street to support the surrounding neighbourhoods.
- 3.4.C.15.** This area is not intended for significant *infill* or *intensification* that would substantially alter the character of the existing low-rise buildings that line the street and create a sense of enclosure. New *development* is intended to integrate into the existing built form with minimal impacts.
- 3.4.C.16.** The ground floor of buildings within the Montreal Street Main Street may be used for commercial or residential uses.
- 3.4.C.17.** New automotive sales and uses, gas stations, gas bars and drive-through facilities are prohibited.

- 3.4.C.18.** Parking will be permitted in rear or interior yards only. New *development* will not be permitted to establish front yard parking.
- 3.4.C.19.** Infill and redevelopment of existing properties will:
  - a.** respect the massing, configuration and setbacks of existing surrounding properties to establish a visually continuous pedestrian scale street wall of up to 3 storeys with facades located close to the sidewalk.
  - b.** maintain the appearance of narrow frontages and fine grain parcel fabric and contribute to a consistent façade rhythm along the streetscape.”

d. **Amend** Section 3 by adding a new Section 3.4.H Mixed Use as follows:

**“3.4.H           Mixed Use**

The Mixed Use designation offers *development* opportunities that combine residential, commercial or compatible light industrial uses on the same property. *Development* in these areas is intended to enhance the pedestrian streetscape, promote *active transportation* and contribute to complete communities by clustering a mix of uses in proximity to each other.

**Goal:**

To provide opportunities for a broad range of *compatible* uses in the same building or separate buildings on a site in a compact built form.

**Policies:**

**Permitted Uses**

- 3.4.H.1.** The Mixed Use designation will permit a broad range of residential, commercial and light industrial uses while maintaining land use compatibility. Some uses within the Mixed Use designation may not be appropriate for all locations and as such, the zoning by-law may limit uses in certain areas to maintain land use *compatibility*.
- 3.4.H.2.** A full range of residential housing options will be permitted, however more intensive forms of housing are encouraged, preferably in the form of mid- rise or high-rise buildings, subject to the policies of Sections 2.6 and 2.7 of this Plan.

- 3.4.H.3.** Home occupations are permitted in accordance with Section 3.1.7 of this Plan.
  - 3.4.H.4.** A variety of commercial uses are contemplated, such as retail stores, restaurants, personal services, professional offices, creativity centres, recreational, entertainment, and other similar uses intended to provide support and services to the community. Automobile related uses, such as automobile sales, service or repair shops will be directed to alternate land use designations to foster the evolution of the pedestrian realm within the Mixed Use designation.
  - 3.4.H.5.** Small-scale, light industrial uses with low risk of noise impacts, heavy truck traffic and fugitive emissions, such as workshops, warehousing, and other similar uses that can operate entirely within an enclosed building, may be permitted by the zoning by-law, subject to the policies of Section 2.7. Outdoor storage of goods and materials is not permitted.
  - 3.4.H.6.** The zoning by-law may require commercial uses on the ground floor.
  - 3.4.H.7.** Where ground floor commercial uses are required, the zoning by-law may establish a minimum floor-to-ceiling height on the ground floor.”
- e. **Delete** Section 3.9.A.8 in its entirety.
  - f. **Amend** the sub-heading of Section 3.17.6 by deleting the words “102 Fraser Street and”.
  - g. **Amend** Section 3.17.6 by deleting existing text and replacing it with: “The facility located at the corner of Fraser Street and Patrick Street, known municipally as 342 Patrick Street, is recognized as a community facility use. It is the intent of this Plan that should this use be discontinued or relocated, the respective property should be converted to a residential use such that it can be integrated into the surrounding residential area. The Plan also intends that further analysis be undertaken to determine an appropriate density level for such conversion to ensure *compatibility* with the adjacent structure of densities in the area.”
  - h. **Amend** Section 3.17.8 by deleting existing text and replacing it with:

“This Plan permits the use and *development* of lands shown on Schedule 3-D as Area 8 in accordance with the Residential policies of this Plan and the following specific policies:

- The maximum permitted density is fifty (50) *residential units* per net hectare. Any application for the *development* for residential purposes must be supported by a study designed in accordance with the Provincial regulations and standards to assess on-site soil contamination, and identify remedial works and clean-up procedures required to eliminate health risks to the occupants of any residential *development*.
  - Mandatory filing of a Record of Site Condition is required if the change in use of land is from industrial or commercial to a residential land use.”
- i. **Amend** Section 3.17.17.b, second last paragraph, by deleting the words “pedestrian systems built along Wellington Street” and replacing them with “future *active transportation* corridor built to the west of this site.”
- j. **Amend** Section 3.17.28 by deleting the words “without an amendment to this Plan, but” and “Inner Harbour Area”.
- k. **Amend** Section 3.17.44 by deleting all references to “Residential designation” and replacing with “Residential and Mixed Use designations.”
- l. **Amend** Section 4.6.9 by adding the **following** sub-sections:
- 4.6.9.1** As shown on Schedule 5, the lands formerly contemplated as an extension of Wellington Street from Bay Street to Montreal Street have been re-envisioned to serve as a critical *active transportation* corridor to support sustainable and efficient modes of travel.
- 4.6.9.2** In limited cases, for future *development* proposed on existing lots of record that directly abut the planned *active transportation* corridor between Bay Street and Montreal Street, the use of municipal lands for resolving access constraints may be considered at the sole discretion of the City. *Development* applications will be required to demonstrate that all reasonable steps to resolve access constraints have been taken on the site. The extent of this consideration by the City will generally be limited to an allowance of the minimum drive aisle width required to accommodate vehicular access and/or viable pedestrian access to the property.”



- m. **Amend** Table 1 in Section 4 by replacing “Railway St” with “Russell St” within the row that indicates a 30.5 metre right of way width along Montreal Street south of Highway 401.
- n. **Amend** Table 1 in Section 4 by replacing “Railway St” with “Russell St” within the row that indicates a 26.2 metre right of way width along Montreal Street north of Stephen Street.
- o. **Amend** Section 4.6.35 by deleting clause e. in its entirety and renumbering the subsequent clauses accordingly.
- p. **Amend** Section 4.6.35.1 by deleting the existing text in its entirety and replacing it with the following: “The mid-block two lane road from Montreal Street to John Counter Boulevard listed in Section 4.6.35.f. will be examined in the context of city-wide transportation impacts through a future update to the Kingston Transportation Master Plan. In the interim, for enhanced access to lands located in the Old Industrial Area, a partial local road extending Hagerman Avenue to the north may be considered by the City as part of a future plan of subdivision or other *development* application.”
- q. **Amend** Section 7.3.D.5 by deleting the existing text in its entirety and replacing it with the following:
  - “**7.3.D.5.** The St. Lawrence Ward Heritage Character Area, as shown on Schedule 9, is one of the oldest areas of the City with an urban style that has survived since the 1800’s. It is recognized that the heritage character of the area was created through the combination of buildings, street pattern, varying street widths and public spaces. It is the intent of this Plan to maintain the heritage integrity of the area with the application of the following policies:
    - a. The City will undertake further investigations that will define appropriate boundaries of a future heritage conservation district study area;
    - b. *Development* involving the demolition of existing buildings will be discouraged. The City may require a heritage impact assessment prior to permitting demolition, where applicable.
    - c. *Development* will be encouraged to adaptively re-use buildings in a manner that conserves the attributes that contribute to the heritage character of the area; and
    - d. Applications requiring *Planning Act* approval may be required to submit studies, such as urban design or heritage impact assessment, to demonstrate how the proposed *development* is compatible with the characteristics of the St. Lawrence

Ward Heritage Character Area and any adjacent cultural heritage resources. The North King's Town Cultural Heritage Study should be consulted for characteristics and context."

- r. **Amend** Section 10A.3.2 by adding "and," at the end of clause a., deleting "; and," at the end of clause b. and replacing it with a period, and deleting clause c. in its entirety.
  - s. **Amend** the text of the City of Kingston Official Plan by adding a new section 10 H. North King's Town Specific Policy Area as shown in Schedule 'A' to this By-Law;
2. The City of Kingston Official Plan, as amended, is further amended by the following map changes:
- a. **Amend** Schedule 2 'City Structure' by changing the lands identified as 'Business District' to 'Housing District' as shown on Schedule 'B' to this By-Law;
  - b. **Amend** Schedule 3-A 'Land Use – Central' by designating lands Mixed Use, Main Street Commercial, and Open Space, as shown on Schedules 'C1', 'C2', 'C3', 'C4', and 'C5' to this By-Law;
  - c. **Amend** Schedule 3-D 'Site Specific Policies' by amending Site Specific Policy Areas 6 and 8, as shown on Schedule 'D' to this By-Law;
  - d. **Amend** Schedule 4 'Transportation' by deleting the southern portion of the Wellington Street Extension, identifying the entire length of Rideau Street as a Collector Road, and identifying the Waaban Crossing as an Arterial Road, as shown on Schedule 'E' to this By-Law;
  - e. **Amend** Schedule 5 'Major Pathways' by adding a new 'Proposed Pathway or Trail' extending from Stephen Street to River Street, as shown on Schedule 'F' to this By-Law;
  - f. **Amend** Schedule 9 'Heritage Areas, Features and Protected Views' by expanding the boundary of the St Lawrence Ward Heritage Character Area, deleting the protected view along Ragland Road and adding a protected view along Corrigan Street, as shown on Schedule 'G' to this By-Law;
  - g. **Amend** Schedule 13 'Detailed Planning Areas' by identifying the North King's Town Secondary Plan as being complete, as shown on Schedule 'H' to this By-Law;
  - h. **Add** new Schedule NKT-1 'North King's Town Specific Policy Area', as shown on Schedule 'I' to this By-Law;

3. This by-law shall come into force and take effect on the day that is the day after the last day for filing an appeal pursuant to the *Planning Act*, provided that no Notice of Appeal is filed to this by-law in accordance with the provisions of Section 17, Subsection 24 of the *Planning Act*, as amended; and where one or more appeals have been filed within the time period specified, at the conclusion of which, the By-Law shall be deemed to have come into force and take effect on the day the appeals are withdrawn or dismissed, as the case may be.

Given all Three Readings and Passed: [Meeting date]

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**Janet Jaynes**  
**City Clerk**

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**Bryan Paterson**  
**Mayor**

## Schedule 'A' to By-Law Number 2025-XX

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### 10H. North King's Town Specific Policy Area

The following policies apply to the North King's Town Specific Policy Area, shown on Schedule NKT-1. The North King's Town lands are generally bounded by John Counter Boulevard to the north, Division Street to the west, the Great Cataraqui River to the east, and an irregular boundary along Colborne and Bay Streets to the south. The overall intent of the North King's Town Specific Policy Area is to facilitate intensification in appropriate locations in a manner that fulfills the following vision statement:

North King's Town is at the heart of Kingston's 21st century community, building on a legacy of providing great places for people to live, work, and play, and fostering innovative growth that continues to diversify the city's economy and enhance its quality of life. It is a place for the arts and industry; a hub for recreation and community services, supporting active and accessible daily life; and home to walkable neighbourhoods, with strong connections to jobs, amenities, open spaces, the waterfront, and neighbouring communities so residents from a variety of backgrounds and income levels can grow, thrive, and age in place. North King's Town is a resilient and sustainable community that values and protects the urban wilderness adjacent to the Great Cataraqui River, and honours its rich and diverse cultural heritage, including the spiritual connections that Indigenous Peoples have with the area.

The policies in this Specific Policy Area are to be implemented in accordance with the following supporting studies, which should be referred to when assessing policy intent:

- Phase 1 – Visioning Report and Preliminary Market Analysis;
- Cultural Heritage Study;
- Mobility Plan;
- Servicing Master Plan; and
- Financial and Implementation Plan.

#### 10H.1 Principles

The following principles are intended to help implement the vision statement for North King's Town:

- a. To create a welcoming and inclusive setting for people to gather, recreate, work, and live;
- b. To enhance options for movement within North King's Town, to the waterfront, and to surrounding neighbourhoods, with an emphasis on active transportation and transit;

- c. To cluster new *development* to create hubs of activity and investment, and a compact, walkable, built form;
- d. To diversify the economic and employment base and enhance customer access to businesses;
- e. To conserve natural and cultural heritage resources and enhance public access to open spaces and the waterfront;
- f. To respect Indigenous traditions and use of the land, and honour the Belle Island Accord;
- g. To support arts and cultural uses and activities;
- h. To implement sustainable and resilient plans, technologies, and design approaches;
- i. To provide a wide variety of housing options; and
- j. To identify opportunities for residential intensification, primarily through the redevelopment of larger, vacant or underutilized parcels of land.

## 10H.2 General Policies

### Intensification Areas

- 10H.2.1.** The intensification areas shown on Schedule NKT-1 generally represent larger parcels of land that are primarily vacant or underutilized and contribute to establishing a node or a corridor or otherwise represent an appropriate *infill* opportunity.

### Permitted Uses

- 10H.2.2.** The permitted uses are established by the applicable land use designation as shown on Schedule 3 of this Plan. Ground floor commercial uses are required for intensification areas on Montreal Street as shown on Schedule NKT-1.

### Maximum Building Heights and Transition to Adjacent Uses

- 10H.2.3.** Maximum building heights will be regulated by the zoning by-law. Minor variance applications to increase the height of mid-rise and high-rise buildings may be considered subject to conformity with the land use compatibility policies of Section 2.7 and other built form and urban design principles of this Plan.

- 10H.2.4.** The zoning by-law will include performance standards to create appropriate separation of residential uses from industrial uses and to facilitate a suitable transition between new mid-rise and high-rise buildings and existing low-rise residential uses.
- 10H.2.5.** *Development* applications will be encouraged to locate amenity area and landscape open space in yards adjacent to low-rise residential uses to provide for appropriate transition between the existing and proposed uses.

### Land Use Compatibility

- 10H.2.6.** The intensification areas shown on Schedule NKT-1 contain or are adjacent to a variety of land uses, including residential uses and existing or planned industrial uses. Future *development* of the intensification areas is subject to the land use compatibility policies of Section 2.7 of this Plan and the following:
- a. Where minimum separation distances between sensitive uses and Class I, II, or III industrial uses recommended by the Province's D-6 Guidelines, or similar provincial guidance, cannot be achieved, reduced separation distances may be considered in the zoning by-law subject to appropriate and effective mitigation; and
  - b. *Development* of certain uses may be constrained due to land use conflicts that cannot be mitigated through site and building design.

### Urban and Sustainable Design

- 10H.2.7.** *Intensification* and *infill development* must be appropriately designed and be sensitive to the physical character of adjacent neighbourhoods, including existing cultural heritage features, where applicable. This may be achieved through the use of stepbacks, setbacks from property lines, architectural styles, building materials, patterns of fenestration, preservation of mature vegetation, and maintaining the appearance of consistent lot frontages and streetscape rhythm.
- 10H.2.8.** The primary façade of new buildings should be oriented towards the street. Where buildings are situated on a corner of two streets, the building should be designed to address both frontages. Where a *development* involves both public and private streets, priority should be given to orienting the building toward the public street.
- 10H.2.9.** *Development* situated on a corner of two streets will be encouraged to provide outdoor amenity area adjacent to the intersection to facilitate space for tree planting and gathering spaces.
- 10H.2.10.** To improve pedestrian circulation and to increase pedestrian permeability and connectivity, mid-block connections will be utilized to limit maximum

building width and length. These mid-block connections should be arranged to align with other pathways and connections to facilitate pedestrian movement and are encouraged to incorporate landscaping, amenity areas and parkettes. Generally, mid-block connections should be wide enough to provide landscaping, seating areas, street furniture, lighting, bicycle parking and other placemaking opportunities and further support active transportation. Buildings flanking mid-block connections should be designed to address the connection, including windows and entrances to promote use and surveillance of the connection.

- 10H.2.11.** *Developments* are encouraged to incorporate architectural elements, such as canopies, awnings, recessed entrances, covered walkways, trees and other similar elements to provide weather protection and optimize pedestrian comfort.
- 10H.2.12.** New *development* is encouraged to establish community gardens that provide a shared space to grow and harvest produce, flowers or native plants.
- 10H.2.13.** New *development* is encouraged to preserve existing trees by incorporating them into amenity and landscaped open space areas, wherever possible. However, it is recognized that tree removal may be required to facilitate *development*, especially where remediation is required by provincial legislation to establish sensitive land uses.
- 10H.2.14.** New *development* is encouraged to incorporate fruit trees, nut trees and other edible plantings to promote food security and facilitate opportunities for foraging. The trees should be situated appropriately to avoid creating hazards associated with fallen fruit or nuts.
- 10H.2.15.** New *development* is encouraged to establish 'Little Forests' within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and provide greater biodiversity.
- 10H.2.16.** Where *development* is proposed adjacent to a public park or open space, including the K&P and Waterfront Trails, the public spaces should be integrated into the design, including pedestrian connections, façade designs and landscape connections. Loading, parking and servicing areas should be screened from view with fencing and landscaping.
- 10H.2.17.** *Developments* are encouraged to incorporate sustainable features, such as solar panels or green roof components into the roof design of buildings. Hard and soft landscaping components may also be included to create high quality amenity areas that reduce the urban heat island effect and mitigate stormwater runoff.

- 10H.2.18.** Buildings and windows should be oriented and designed such that natural means of heating, cooling, ventilating, lighting interior spaces and avoiding intrusive overlook are maximized.
- 10H.2.19.** Where *development* is intended to occur in phases, the first phase should include buildings fronting onto a public street.
- 10H.2.20.** New *development* proposals may be required to submit an urban design study at the City's discretion in accordance with Section **Error! Reference source not found.** to demonstrate that the urban design policies of this Plan have been appropriately considered, especially, but not limited to, where the proposed *development* is:
- a. adjacent to existing low-rise residential uses;
  - b. adjacent to properties that are listed or designated under the *Ontario Heritage Act*;
  - c. located within St Lawrence Ward Heritage Character Area; or
  - d. 4 or more storeys in height.
- 10H.2.21.** Building massing will be articulated or broken up through a continuous rhythm of building fronts achieved through a pattern of projections and recessions, entrances, display spaces, signage, and glazed areas. The intent is to create the sense of having multiple buildings along the width of the building.
- 10H.2.22.** Buildings are encouraged to provide vertical and horizontal articulation and a variety of materials to create interesting facades and forms.
- 10H.2.23.** High-rise buildings are encouraged to employ high-quality architectural design.
- 10H.2.24.** Mid-rise and high-rise buildings will be required to stepback above the fourth storey along each streetline to reduce the visual mass of the building and enhance the pedestrian realm. The zoning by-law will regulate the depth of the stepback.
- 10H.2.25.** Balconies are encouraged in new *development*, and may project from the building face above the second storey. Balconies of new *development* must not encroach into the public road allowance.
- 10H.2.26.** Where individual unit entrances are provided on the ground floor for residential uses, each unit must have an independent pedestrian access. Some entrances may be raised above the sidewalk level to provide transition from the public realm to the private realm and/or to provide private amenity space or landscaping to buffer the residential units from the public realm.



- 10H.2.27.** Building entrances should be easily accessible for pedestrians with a direct path of travel from public sidewalks and pathways.
- 10H2.28.** Parking lay-by areas that front onto the street and cross the pedestrian realm will be discouraged.
- 10H2.29.** The exterior design of the ground floor should reflect the intended unit sizes, including entrances, signage and glazing to provide a strong integration between the public and private realms.
- 10H2.30.** Blank side wall conditions may be acceptable up to a height of two storeys if designed with a material finish that complements the architectural character of the main building façade. Blank walls are not permitted facing a street or public open space and are only appropriate where they exist in proximity to an existing building.
- 10H2.31.** Commercial units should be accessed directly from individual entrances on the street side of the building. Where parking has been provided in the rear or side of a building, a secondary access to the commercial unit may be provided from the parking area provided the principal entrance remains along the street frontage.
- 10H2.32.** Commercial units on a street corner should be designed to wrap around the corner and address both street frontages by incorporating signage, glazing, entrances, landscaping, patio spaces and other similar detail details
- 10H2.33.** Advertising and associated signs related to non-residential uses must be designed and situated so as to be *compatible* with adjoining residential uses.

## Transportation

- 10H.2.34.** As outlined in the North King's Town Mobility Plan, it is intended for the transportation mode share to evolve over time to support greater active transportation and transit ridership and fewer vehicle trips.
- 10H.2.35.** The Intensification Areas identified on Schedule NKT-1 are located within *walking distance* of an express transit route. *Development* of Intensification Areas are encouraged to utilize the express transit service to reduce automobile usage.
- 10H.2.36.** 'Active Transportation Desire Lines' are shown on Schedule NKT-1 and represent the approximate locations where the City intends to secure a multi-use pathway through future development applications within these intensification areas. These locations were identified within the North King's Town Mobility Plan as being important connections to the existing and planned active transportation network. The City will work collaboratively with the property owner to ensure the future multi-use pathway is integrated into the *development* proposal and provides

necessary connections to the existing and planned active transportation network, while not unnecessarily constraining redevelopment.

**10H.2.37.** The City will secure road widenings as outlined in Section 4.6 of this Plan to accommodate the future transportation demands associated with redevelopment of the intensification areas.

**10H.2.38.** The zoning by-law will prescribe appropriate parking rates to assist in transitioning to a mode share that relies less on private automobiles and more on public transit and active transportation.

**10H.2.39.** Parking areas will be developed according to the following policies:

- a. Underground vehicular parking will be encouraged wherever feasible.
- b. Above-grade parking structures integrated into the podium of buildings are permitted provided they are located to the rear and visually screened from the pedestrian realm.
- c. Where surface parking is provided, it must be located at the rear of buildings or within an interior side yard. Where no other arrangement is feasible, surface parking may be located within front or exterior yards for accessible spaces only.
- d. Planting strips, landscaped traffic islands, and/or paving articulation should be used to define vehicle routes and smaller parking courts that provide pedestrian walkways, improve edge conditions, and minimize the negative visual impact of surface parking.
- e. Preferential parking for bicycles, energy efficient vehicles and car-share services is encouraged.

**10H.2.40.** Vehicle access points and loading and servicing areas must be appropriately located, and developed in accordance with the following policies:

- a. Wherever possible, vehicular access to on-site parking, loading, and servicing facilities are expected to be provided from side streets and rear lanes.
- b. Loading and service areas must be screened from prominent public areas and adjacent residential areas.
- c. Service and drop-off area circulation must not interfere with accessible pedestrian circulation.

- d. Garbage, loading, servicing, and utility functions shall be integrated within the interior of a building or located within the rear yard or interior side yard.

### **Parks and Open Space**

- 10H.2.41. Where *development* is proposed adjacent to a public park or open space, including the K&P and Waterfront Trails, the public spaces should be integrated into the design through pedestrian connections, façade design, tree planting and landscaping.
- 10H.2.42. As *intensification* occurs, future *developments* are encouraged to include parkettes in accordance with the following:
  - a. Parkettes are intended to be small in size, to accommodate all-season uses, and to contain hardscape surfaces and elements, such as sitting areas and public art, along with adequate soft landscaping.
  - b. Where publicly accessible open space is required as part of the *development* of private property, this open space will be secured through parkland dedication, donation, acquisition, or a combination of these methods.
  - c. Parkette features should reinforce the street edge and the parkette should be configured to allow for the functional design and placement of public amenities, such as street trees or benches.
  - d. The final decision on the design of a parkette, and the facilities or amenities to be included in a parkette, will be at the discretion of the City.
- 10H.2.43. The City will evaluate parkland dedication options through future *development* applications at 158 Patrick Street to preserve public access to the tobogganing hill in the southwestern portion of the site, adjacent to the Catherine Street road allowance.

### **Cultural Heritage**

- 10H.2.44. New *development* in the intensification areas that contain or are adjacent to built heritage resources may be required to submit a Cultural Heritage Evaluation Report to evaluate the significance of existing buildings or a Heritage Impact Statement to assess potential impacts to protected built heritage resources, as appropriate.
- 10H.2.45. *Development* applications may be required to reduce building heights and increase setbacks in order to mitigate impacts on adjacent built heritage resources.

- 10H.2.46.** Redevelopment of existing buildings through adaptive reuse is encouraged.

### **Servicing and Stormwater Management**

- 10H.2.47.** *Development* applications will be required to submit stormwater management plans to control the quality and quantity of stormwater to the satisfaction of the City. *Development* applications will be encouraged to incorporate innovative approaches to improve water quality, reduce runoff and promote infiltration.

### **Holding Overlay**

- 10H.2.48.** The intensification areas will be subject to a holding overlay:
- a. to ensure adequate servicing capacity related to water, wastewater, gas and electricity.
  - b. for the purposes of assessing and mitigating transportation impacts.

## **10H.3 Area-specific Policies**

The following area-specific policies apply to the *intensification* areas as shown on Schedule NKT-1.

### **Montreal Street and John Counter Boulevard**

- 10H.3.1.** The following additional policies apply to new *intensification* in the Montreal Street / John Counter Boulevard Area:
- a. The intensification areas located at this intersection are intended to form a gateway into North King's Town with mid-rise and high-rise mixed use *development* framing Montreal Street. Future *developments* will be subject to the policies of Section 8.11 of this Plan and will provide enhanced public realm facilities, including wide sidewalks, street trees, street furniture and landscaping.
  - b. In recognition of the existing built form near this intersection, the zoning by-law will enable maximum floorplate sizes and maximum building heights to be consistent with the existing high-rise apartment buildings in this area.

### **Outer Station Area**

- 10H.3.2.** The following additional policies apply to new *development* in the Outer Station Area.

- a. The Outer Station Intensification Area is the site of Kingston's first railway station. While the conservation of this *cultural heritage resource* is of utmost importance, this Plan recognizes its redevelopment potential and that conservation efforts may depend on redevelopment. New *development* within this site will be guided by the following:
- Any redevelopment of the original railway station building should maintain its historic form based on archival record. Alternative materials may be proposed subject to review by the City, however, if the structure is determined to be structurally compromised, as a last resort, other construction and design options that maintain the historic form may be considered.
  - The City will encourage adaptive re-use of the existing *built heritage resources* and *cultural heritage landscape* attributes in their original locations on site. In the absence of any feasible alternative to redevelopment, and instead of demolition, the City may consider relocation of *built heritage resources* or *cultural heritage landscape* attributes to locations within the site provided it has been demonstrated to the satisfaction of the City that the relationship of the station building to Montreal Street is maintained and that relocation would enhance public access to the *cultural heritage resources*. The City will not support off-site relocation.
  - The City will collaborate with the property owner to explore creative options and strategies that ensure the greatest degree of conservation of *built heritage resources* and *cultural heritage landscape* attributes.
  - Schedule NKT-1 identifies the City's desire for an active transportation link through the lands to provide an east/west connection from Montreal Street to Hagerman Street and the K&P Trail.
  - The maximum building height, setbacks and other performance standards, as appropriate, will be determined through a site-specific zoning by-law amendment, which must be supported by a Conservation Plan, Heritage Impact Assessment and urban design study. The maximum building height of these lands should not detract from the intersections of Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street, and Railway Street being the main nodes of redevelopment within North King's Town.

- b. Studies and assessments required in support of future *development* applications on the site include, but are not limited to, the following:
- Stage 1 and 2 archaeological assessments, at a minimum, to ensure that significant *archaeological resources* have been appropriately *conserved*. Although some archaeological assessment may have been completed on the property, historical mapping shows that there were many other structures on the property, including a large engine house, and any archaeological evidence of these structures should be inventoried and evaluated for potential conservation and interpretation. The findings of the archaeological assessment should inform the Heritage Impact Assessment and its strategies for heritage conservation.
  - A Conservation Plan that includes consideration of the following factors:
    - Description of the *built heritage resource* and *cultural heritage landscape* features that exist on the property and assessment of their significance and current condition;
    - Discussion of available repair and conservation methods and an analysis to identify a proposed repair and conservation approach, including long-term conservation, monitoring and maintenance measures, as appropriate.
    - Description of built form, massing, building heights, locations, setbacks, stepbacks and materiality that future development should utilize/consider to appropriately conserve the built heritage resource and maintain visibility from the public realm.
    - Identification of view corridors to ensure the *built heritage resources* remain visible to the public from Montreal Street.
    - A Temporary Protection Plan for the conservation of *built heritage resources* and *cultural heritage landscape* features during construction.
    - Methods of incorporating remaining elements of the former station buildings within/around new *development* while distinguishing new versus old.
  - A Heritage Impact Assessment, prepared to the satisfaction of the City and consistent with municipal and Provincial policies

and guidelines, and based on the municipal designation by-law for the Outer Station property.

### Montreal Street, Rideau Street and Railway Street

**10H.3.3.** The following additional policies apply to new *infill* and *intensification* in the Montreal Street, Railway Street and Rideau Street intersection, which is intended to transition over time into the primary hub of redevelopment within North King's Town:

- a. Increased building heights and densities have been contemplated within this area to assist in redeveloping *brownfield* properties.
- b. Building heights throughout this hub of redevelopment are intended to be primarily mid-rise, generally up to six storeys in height. Building heights may transition to high-rise, generally 15 storeys in height, in proximity to the Montreal Street, Rideau Street and Railway Street intersection, where adequate separation from low-rise residential and cultural heritage resources can be achieved and where parcel sizes permit. The zoning by-law may limit building heights to low-rise, generally up to four storeys in height, along the periphery of the intensification areas to facilitate a transition to the existing low-rise residential neighbourhoods abutting the North King's Town Specific Policy Area.
- c. Where high-rise buildings are permitted by the zoning by-law, a minor variance application may be utilized to increase the building height generally to a maximum of 20 storeys, provided the *development*:
  - would not have a negative impact on the *cultural heritage landscape* and *built heritage resources* associated with the Rideau Canal;
  - would not generate significant additional shadowing on any surrounding residential properties;
  - Incorporates appropriate transition to any adjacent low-rise residential uses through such methods as increased yard setbacks, increased building stepbacks, introduction of intervening ground-oriented dwellings or units, or other approaches supported by an urban design study;
  - can be supported by available transportation and servicing networks;

- integrates a variety of building heights within the broader proposal to contribute to an interesting, varied and focal point skyline in the area; and
  - demonstrates a signature architectural design respecting the prominent and visible location along Montreal Street.
- d.** High-rise buildings will consist of smaller floor plate towers above larger podium bases to mitigate shadow and wind impacts. The zoning by-law will establish maximum floor plate sizes and setbacks to facilitate this built form.
- e.** The implementing zoning by-law may limit the number of high-rise buildings on individual sites within this intensification area to reinforce mid-rise pedestrian scale *development* as the predominant built form.
- f.** Adequate separation distances will be required to be established between sensitive uses and adjacent industrial uses.






# Schedule 'B' to By-Law Number

File Number: D35-001-2025

## Official Plan Schedule 2 City Structure

 Lands Subject to change  
from Business District to  
Housing District

Planning  
Services

### Certificate of Authentication

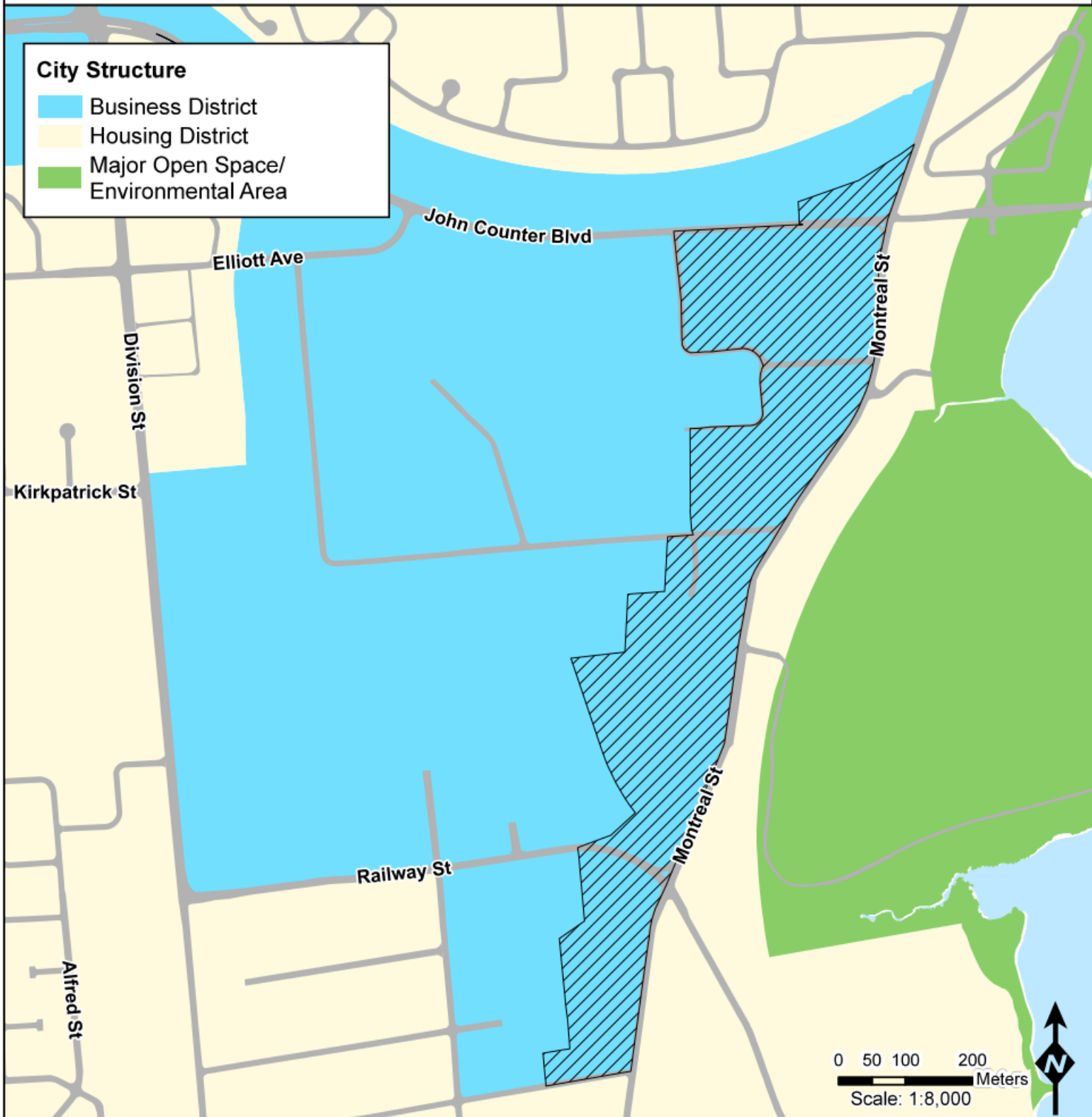
This is Schedule 'B' to By-Law Number \_\_\_\_, passed this \_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk

#### City Structure

-  Business District
-  Housing District
-  Major Open Space/  
Environmental Area








# Schedule 'C1' to By-Law Number

File Number: D35-001-2025

## Official Plan Schedule 3A Land Use

Report Number PC-25-008  
Lands to be redesignated from:

-  Arterial Commercial to Mixed Use
-  General Industrial to Mixed Use
-  Residential to Mixed Use

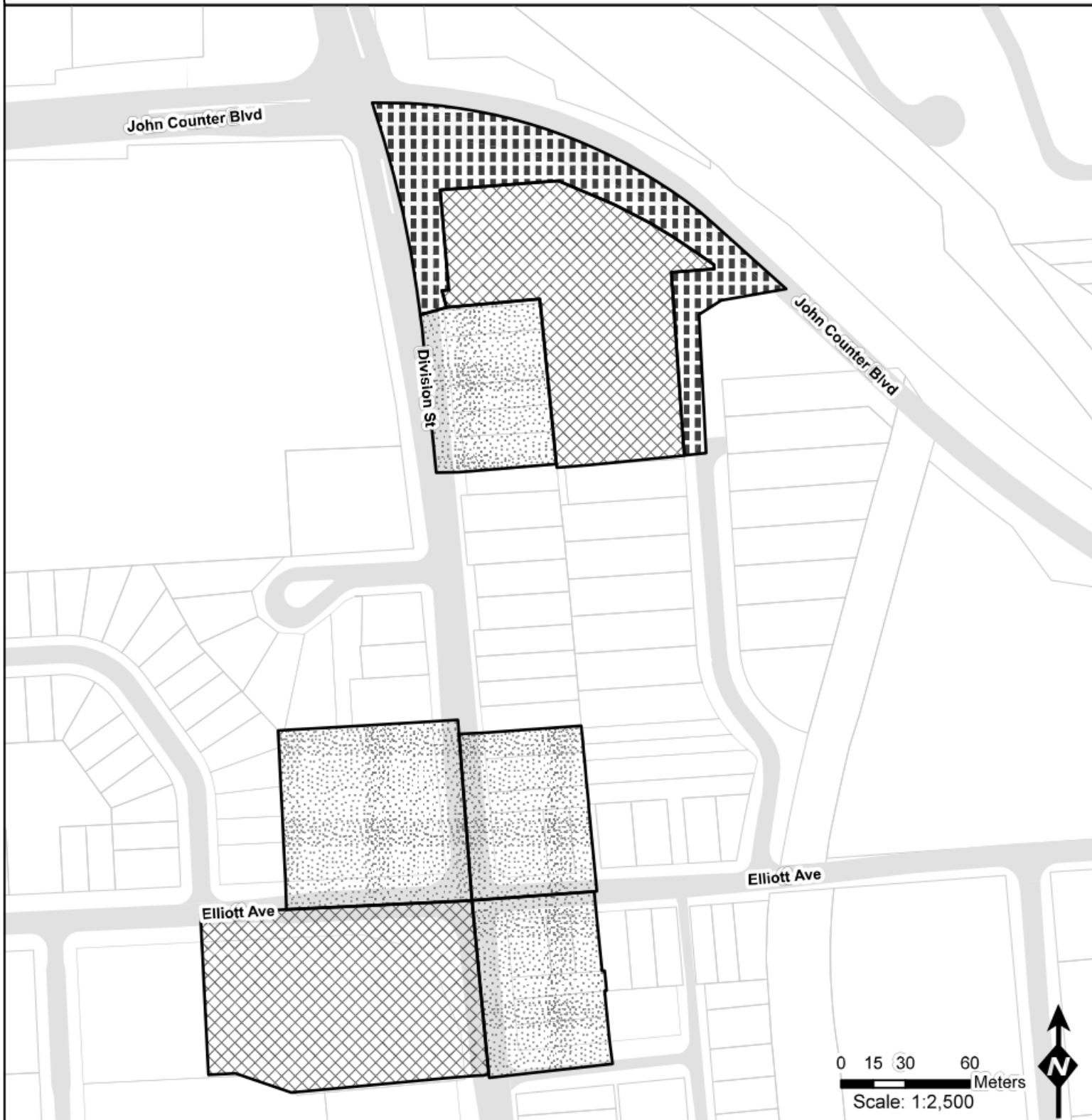
Planning  
Services

### Certificate of Authentication

This is Schedule 'C1' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



Lands to be redesignated from:



# Schedule 'C2' to By-Law Number

File Number: D35-001-2025

# Official Plan Schedule 3A Land Use






Planning  
Services

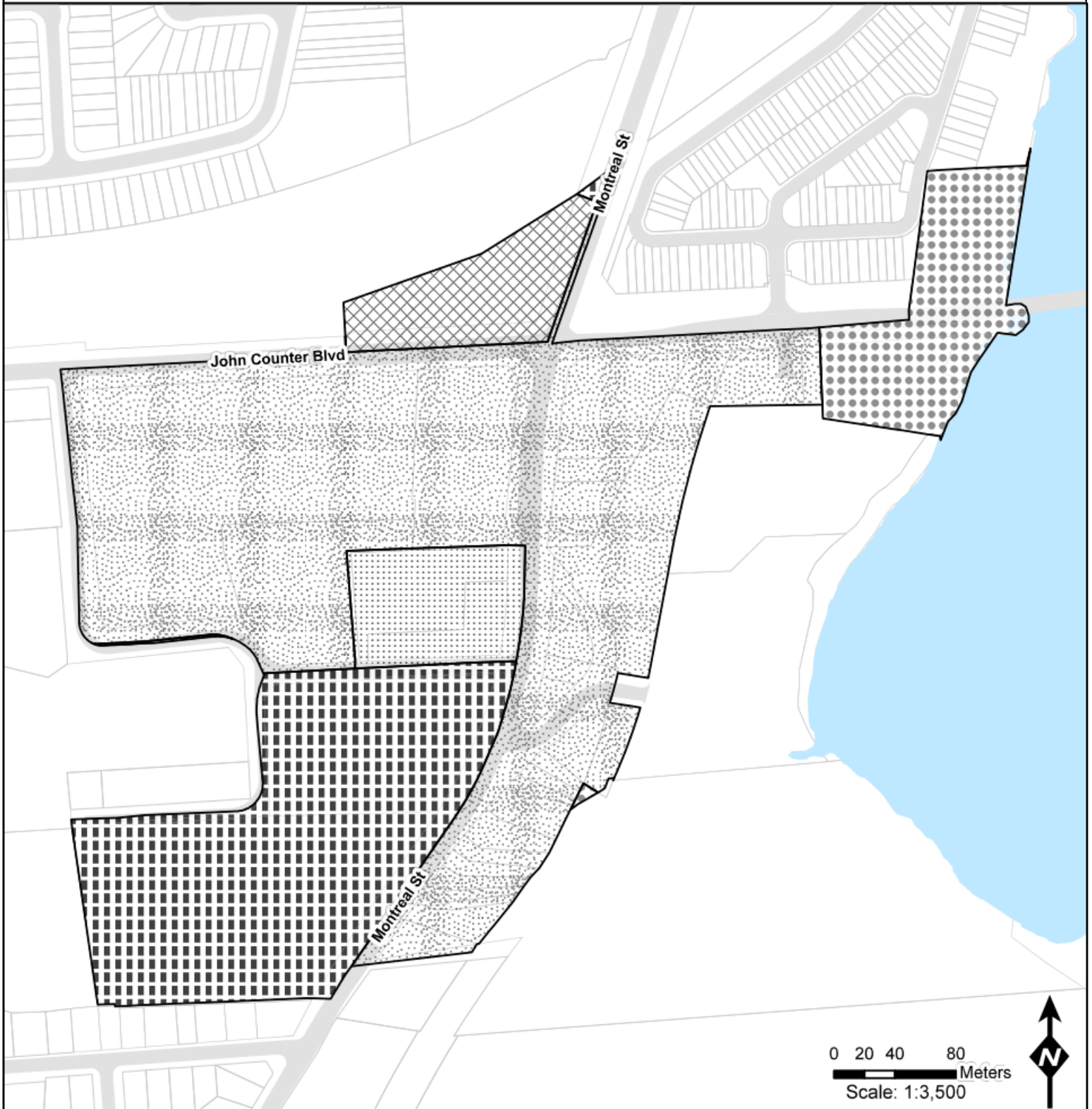
## Certificate of Authentication

This is Schedule 'C2' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk

-  Arterial Commercial to Mixed Use
-  General Industrial to Mixed Use
-  Institutional to Mixed Use
-  Residential to Mixed Use
-  Residential to Open Space









# Schedule 'C3' to By-Law Number

File Number: D35-001-2025

## Official Plan Schedule 3A Land Use

Lands to be redesignated from:

-  Business Park Industrial to Mixed Use
-  General Industrial to Mixed Use
-  General Industrial to Open Space
-  Residential to Mixed Use

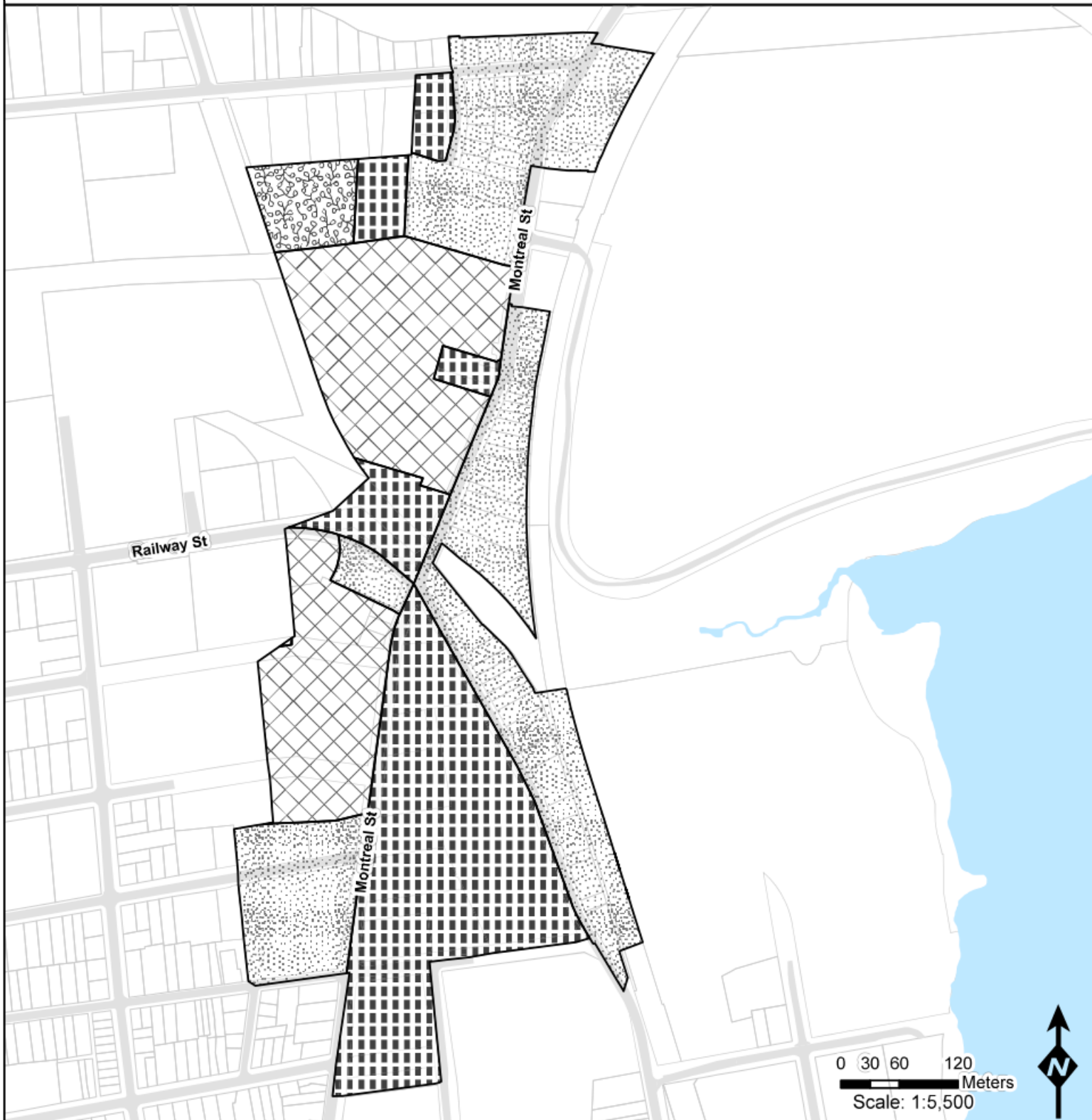
Planning  
Services

### Certificate of Authentication

This is Schedule 'C3' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





# Schedule 'C4' to By-Law Number

File Number: D35-001-2025

Official Plan  
Schedule 3A  
Land Use

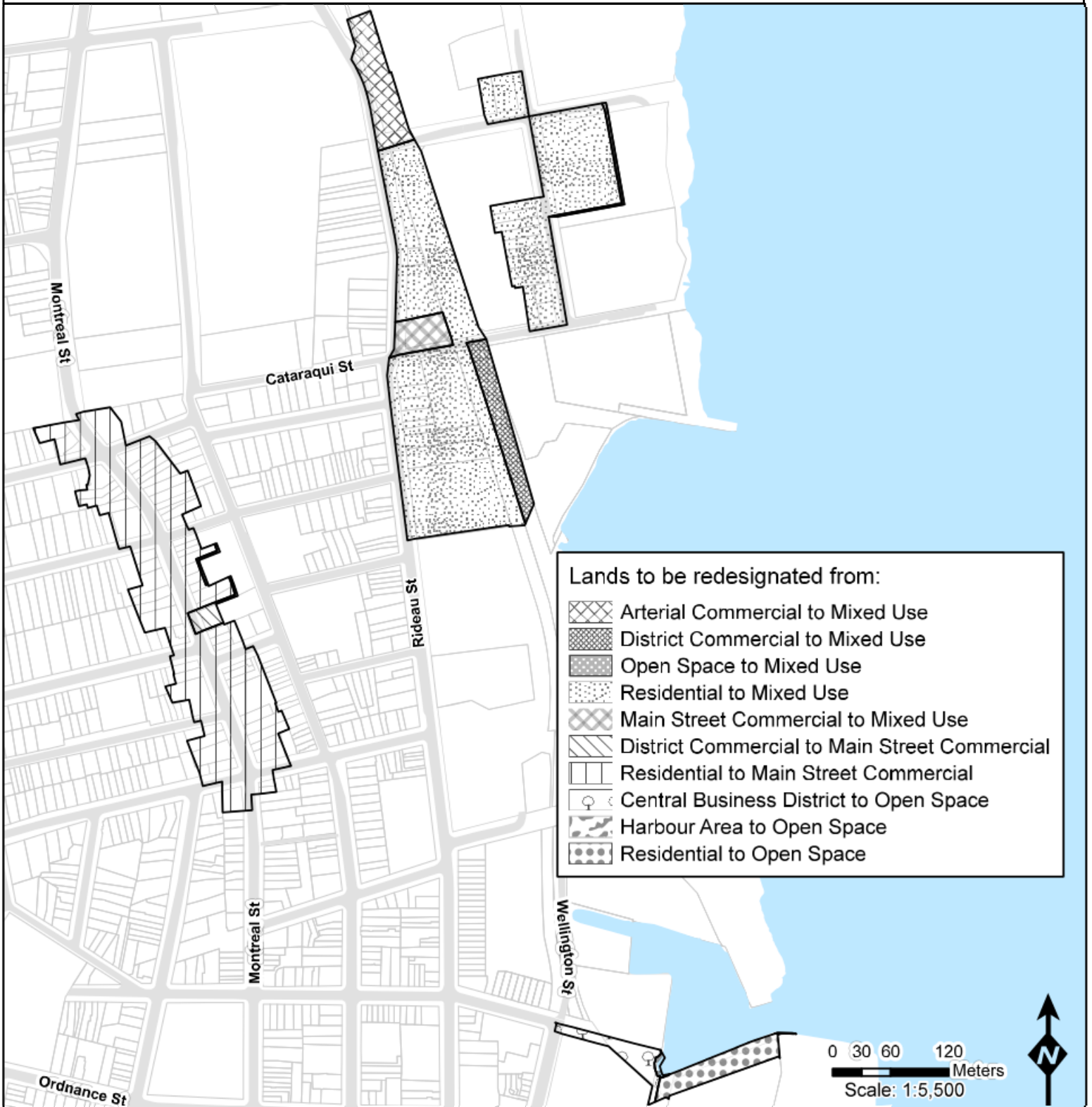
Planning  
Services

## Certificate of Authentication

This is Schedule 'C4' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk







# Schedule 'C5' to By-Law Number

File Number: D35-001-2025

# Official Plan Schedule 3A Land Use

Report Number PC-25-008

Lands to be redesignated from:

-  Arterial Commercial to Mixed Use
-  Residential to Mixed Use

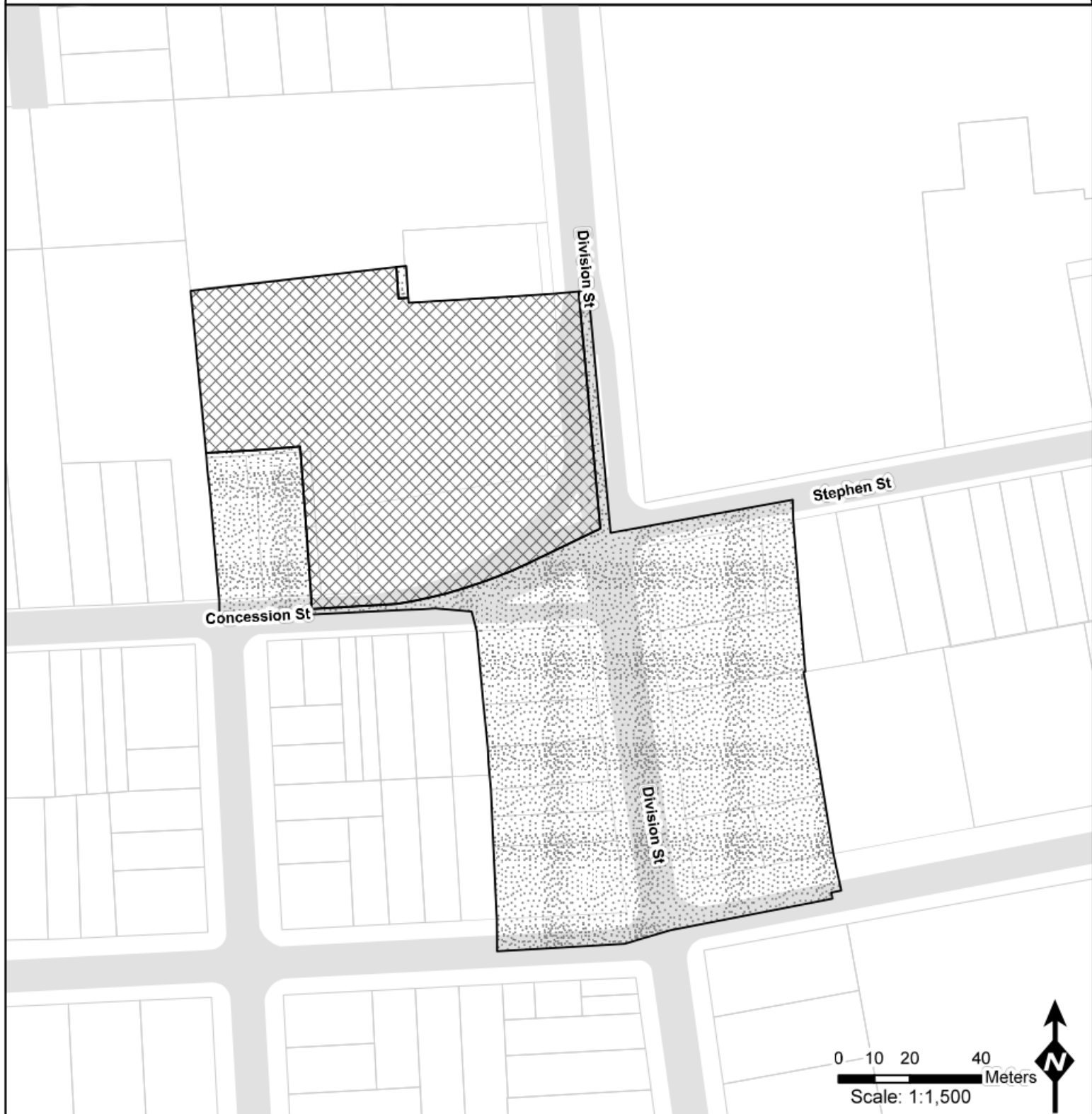
Planning  
Services

## Certificate of Authentication

This is Schedule 'C5' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk








Planning  
Services

**Schedule 'D'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 3D  
Site Specific Policy Areas**

-  Lands to be removed from Site Specific Policy Area
-  New Boundary of Site Specific Policy Area 6
-  New Boundary of Site Specific Policy Area 8

**Certificate of Authentication**

This is Schedule 'D' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk









# Schedule 'F' to By-Law Number

File Number: D35-001-2025

## Official Plan Schedule 5 Pathways

Change from Future Major Road to Major Road

Proposed Pathway or Trail

Planning  
Services

### Certificate of Authentication

This is Schedule 'F' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
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# Schedule 'G' to By-Law Number

File Number: D35-001-2025

# Official Plan Schedule 9 Heritage and Protected Views

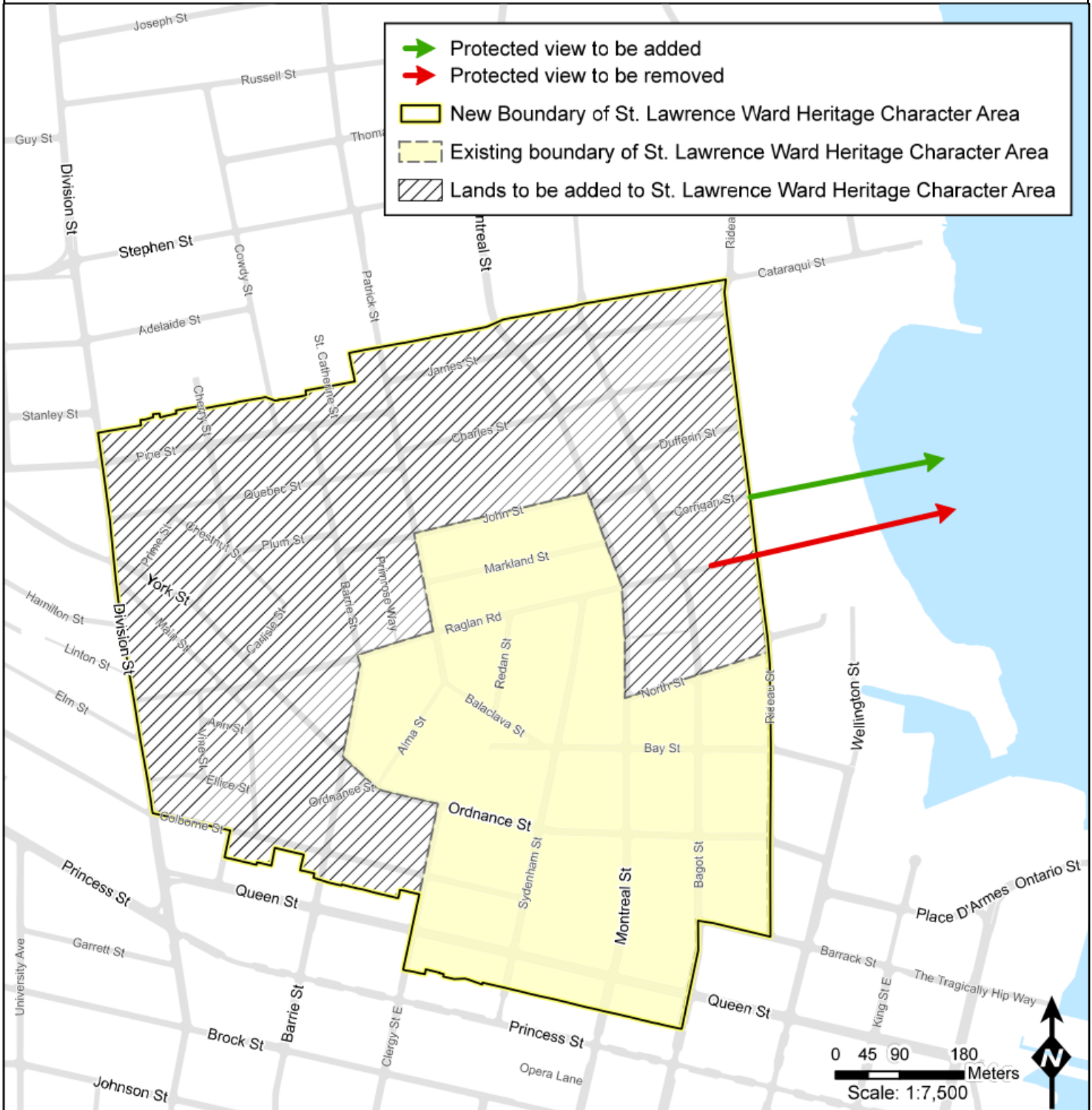
Planning  
Services

## Certificate of Authentication

This is Schedule 'G' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





# Schedule 'H' to By-Law Number

## Official Plan

Report Number PC-25-008

## Schedule 13, Detailed Planning Areas

Amend by deleting Future Secondary Plan Study Area 5 (North King's Town) and replacing it with a new Specific Policy Area 5:

 North King's Town Specific Policy Area

Planning Services

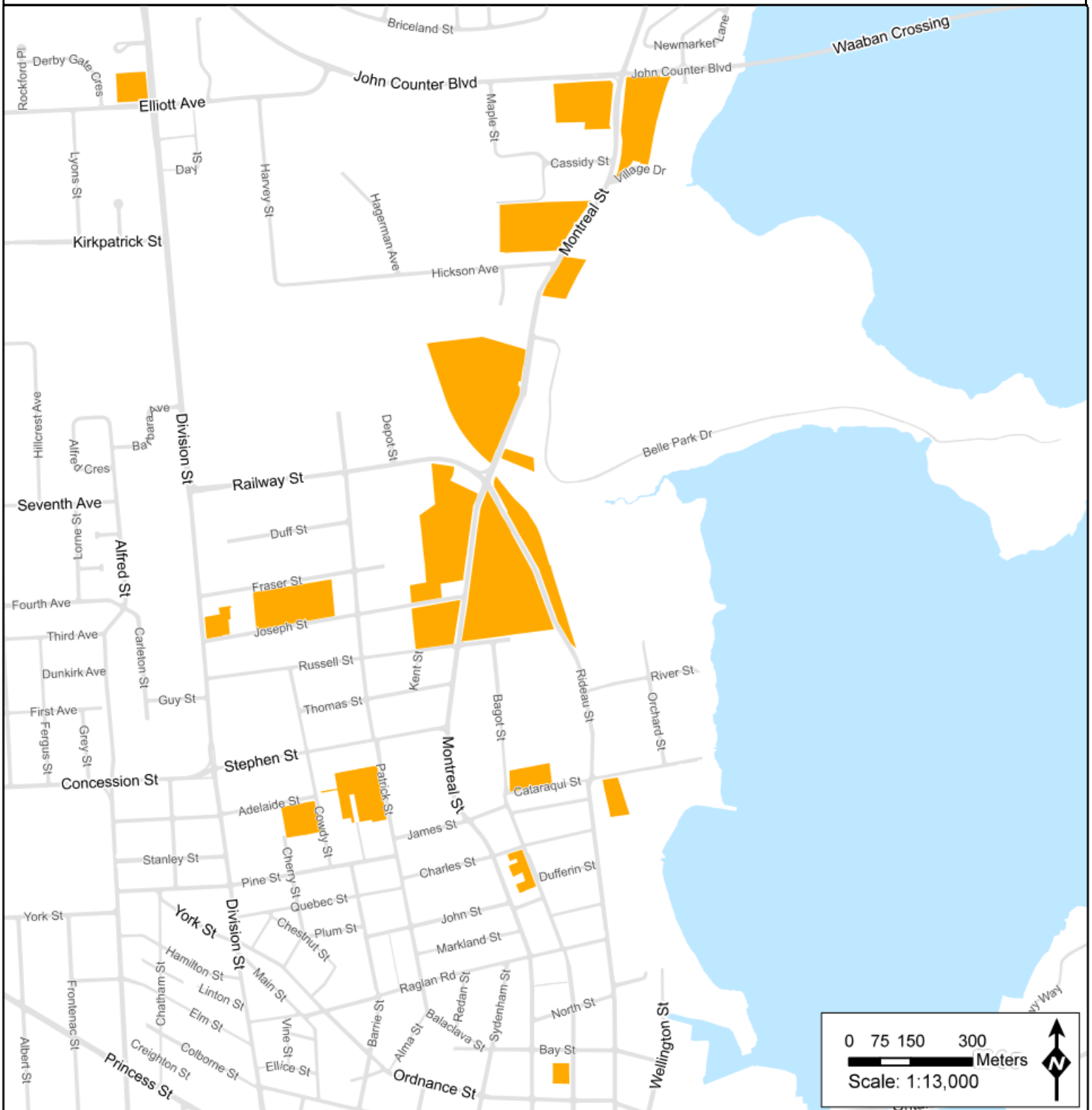
File Number: D35-001-2025

### Certificate of Authentication

This is Schedule 'H' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
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# Schedule 'I' to By-Law Number

File Number: D35-001-2025

## Official Plan Schedule NKT-1 North King's Town Specific Policy Area

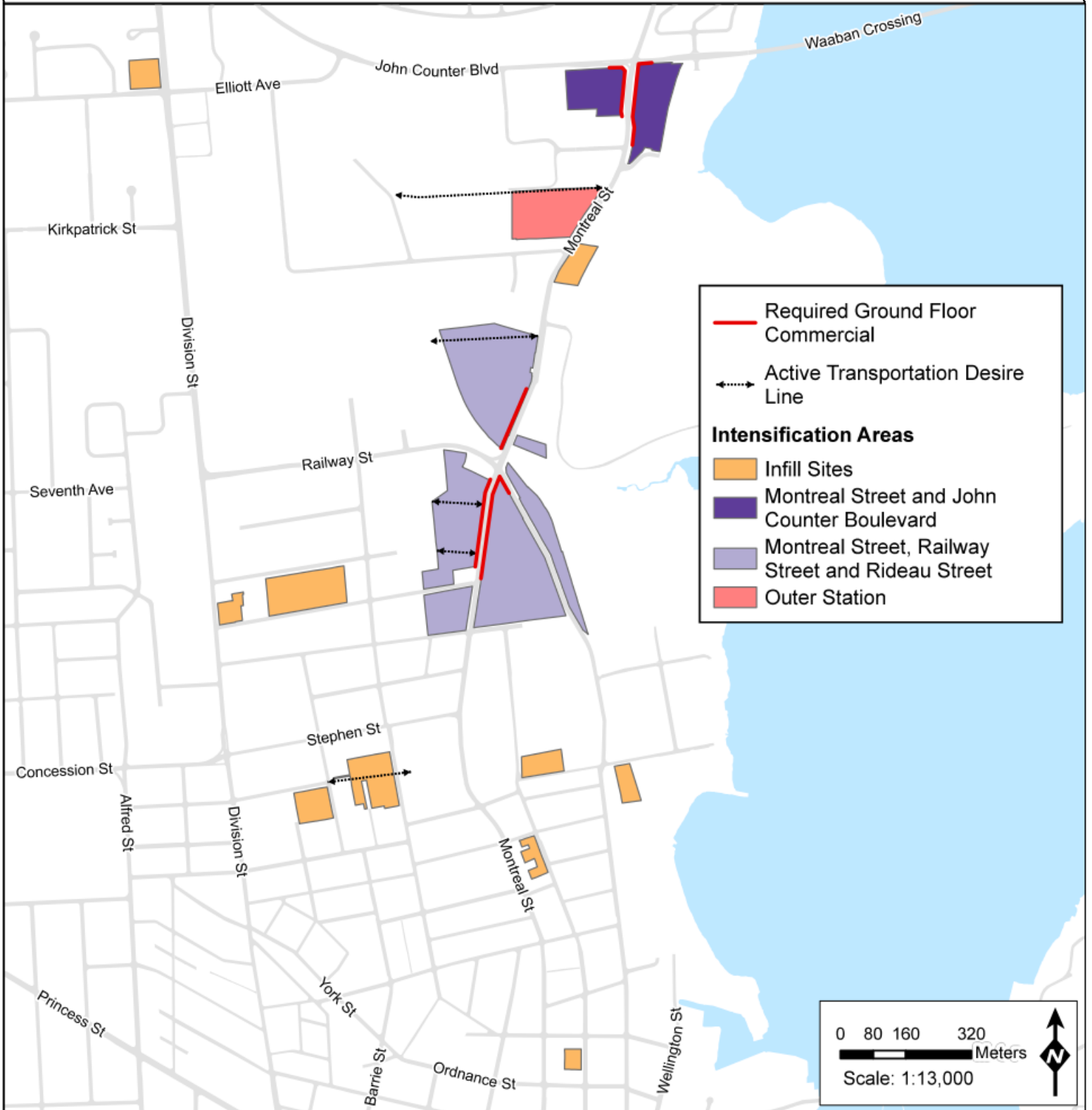
Planning  
Services

### Certificate of Authentication

This is Schedule 'I' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



**By-Law Number 2025-XX**

**A By-Law to Amend By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (Zone Changes, Exceptions, and Holding Overlays, North King’s Town Project)**

**Passed: Date**

**Whereas** the Council of The Corporation of the City of Kingston enacted By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (the “Kingston Zoning By-law”);

**Whereas** the Council of The Corporation of the City of Kingston deems it advisable to amend the Kingston Zoning By-law;

**Therefore be it resolved that** the Council of The Corporation of the City of Kingston hereby enacts as follows:

1. By-Law Number 2022-62 of The Corporation of the City of Kingston, entitled “Kingston Zoning By-law Number 2022-62”, is amended as follows:
  - 1.1. Table 2.4.1. – List of Zones is amended by adding new rows for Mixed Zone 1, Mixed Zone 2, Mixed Zone 3, below the row for Downtown Zone 2, as follows:

Mixed Zone 1	MU1
Mixed Zone 2	MU2
Mixed Zone 3	MU3

- 1.2. Table 2.4.1. – List of Zones is amended by adding a new row for Urban Multi-Residential Zone 11 below the row for Urban Multi-Residential Zone 10 as follows:

Urban Multi-Residential Zone 11	URM11
---------------------------------	-------

- 1.3. New Subsection 4.32. is added as follows:

**“4.32. Separation Distances between Sensitive Uses and Industrial Facilities**

**4.32.1.** Despite anything to the contrary in this By-law, the establishment of a new **sensitive use** must comply with the following minimum separation distances from a Class 1, Class 2 or Class 3 industrial facility, as applicable, located within an Employment Zone. Despite clause 3.19.7., this separation distance must be measured as per the province’s D-series Environmental Land Use Compatibility Guidelines, or any successor thereof:

1. Minimum separation distance from a Class 1 industrial facility located within an Employment Zone: 20 metres;
2. Minimum separation distance from a Class 2 industrial facility located within an Employment Zone: 70 metres; and
3. Minimum separation distance from a Class 3 industrial facility located within an Employment Zone: 300 metres

**4.32.2.** Despite clause 4.32.1, any **sensitive use** within the required separation distance existing as of the date of passing of By-law Number 2025-XX, is considered to comply with this provision and may be expanded as long as it does not further increase the extent or degree of non-compliance with clause 4.32.1.”

1.4. Clause 10.1.1. is amended by deleting “and Downtown Zone 2 (DT2)” and replacing with “, Downtown Zone 2 (DT2), Mixed Zone 1 (MU1), Mixed Zone 2 (MU2), and Mixed Zone 3 (MU3).”

1.5. A new clause 10.1.3. is added as follows:

**“10.1.3.** Where a permitted use includes a reference number in superscript beside the “●” symbol in Table 10.1.2., the following corresponding provision applies:

1. Is required to operate within an enclosed **building.**”

1.6. Table 10.1.2 is amended by inserting new rows for “**catering service**”, “**production studio**”, “**research establishment**”, and “**workshop**” under Non-residential, in appropriate locations alphabetically.

1.7. Table 10.1.2. is amended by inserting the “–” symbol for a catering service, production studio, research establishment, and workshop in the WM1, WM2 column and the DT1, DT2 column.

- 1.8. Table 10.1.2. is amended by inserting a new column titled “MU1, MU2, MU3” as follows:

Use	MU1, MU2, MU3
Residential apartment building	●
dwelling unit in a mixed use building	●
stacked townhouse	●
townhouse	●
Non-residential animal care	—
automobile sales establishment	—
banquet hall	●
building supply store	—
catering service	●
commercial parking lot	—
community centre	●
club	●
creativity centre	●
day care centre	●
department store	—
entertainment establishment	●
financial institution	●
fitness centre	●
service station	—
grocery store	●
funeral establishment	●
hotel	●
laboratory	●
laundry store	●
library	●
museum	●
office	●
personal service shop	●
place of worship	●
production studio	●
public market	●
recreation facility	●
repair shop	—

<b>research establishment</b>	●
<b>restaurant</b>	●
<b>retail store</b>	●
<b>special needs facility</b>	●
<b>training facility</b>	● <sup>1</sup>
<b>transportation depot</b>	—
<b>transportation terminal</b>	—
<b>wellness clinic</b>	●
<b>workshop</b>	● <sup>1</sup>

1.9. New Subsections 10.6., 10.7., and 10.8., are added as follows:

**“10.6. Mixed Zone 1 (MU1)**

**10.6.1.** The use of any **lot** or **building** in the MU1 Zone must comply with the provisions of Table 10.6.1.

**Table 10.6.1. – MU1 Provisions are added as follows:**

<b>Zoning Provision</b>	<b>All permitted uses</b>
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Minimum <b>streetwall height</b> (metres)	10.5
4. Minimum floor to floor <b>height of first storey</b> (metres)	4.5
5. Maximum <b>height</b>	The lesser of:  (a) 20 metres (b) 6 storeys
6. Minimum <b>front setback</b> (metres)	3.0
7. Minimum <b>rear setback</b> (metres)	10.0
8. Minimum <b>exterior setback</b> (metres)	3.0
9. Minimum <b>interior setback</b> (metres)	3.0
10. Minimum <b>stepbacks</b> (metres)	(a) Front lot line and exterior lot line:



	i. <b>storey 5 to 6:</b> minimum 3.0 metres from the exterior wall of the 4 <sup>th</sup> <b>storey</b>
11. Minimum <b>landscaped open space</b>	15%
12. Minimum <b>lot coverage</b>	55%
13. Maximum residential <b>density</b>	175 <b>dwelling units</b> per net hectare

### Additional Provisions for Lots Zoned MU1

**10.6.2.** In addition to the provisions of Table **10.6.1.**, uses in the MU1 Zone must comply with the following provisions:

#### Front Setback of Streetwall

1. A minimum of 75% of the **streetwall** of a **building** must be built to the minimum required **front setback** for the **height** of the **streetwall**.

#### Ground Floor Commercial Uses

2. **Buildings** are required to have ground floor commercial **uses** on the **first storey** where any portion of the **lot** aligns with the area identified as “Required Ground Floor Commercial” on Schedule 3 of this By-law.
3. Where ground floor commercial **uses** are required by Subclause 2., the entire **streetwall** of the **first storey**, excluding areas devoted to a lobby or other shared entrances/exits for other permitted **uses**, must be occupied by commercial **uses**. Portions of the floor area of the **first storey** that do not have an exterior wall facing a **street line** may be occupied by **uses** that service the **building** such as **loading spaces**, waste management facilities and rooms, mechanical rooms, bike parking facilities and other similar **uses**.

#### Main Pedestrian Entrance

4. The main pedestrian entrance to the **building** must be located on the **main wall** facing the **front lot line**.

### Projecting Balconies

5. **Balconies** are only permitted above the second **storey**, to a maximum projection of 1.5 metres from the **main wall**.

### Loading and Parking

6. **Parking structures** are not permitted to occupy any part of a main wall facing a **street line**.
7. **Loading spaces, parking spaces** and **parking lots** are not permitted in a **front yard** or **exterior yard**.

### Planting Strip

8. Where a **lot** is adjacent to a **lot** in a UR Zone, a Commercial Zone or an Employment Zone, within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

### Legally Existing Uses

9. **Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.

## 10.7 Mixed Zone 2 (MU2)

- 10.7.1. For the purpose of the MU2 Zone, the following definitions apply:

1. **Podium** means the base component of any **building** that is no greater than 20 meters in **height** (excluding mechanical penthouses) and only includes the **first storey** through sixth **storeys** of such **building**.
2. **Tower** means any portion of any **building** that is greater than 20 metres in **height**, excluding a **podium**, below grade **building** components and mechanical penthouses.

- 10.7.2. The **use** of any **lot** or **building** in the MU2 Zone must comply with the provisions of Table **10.7.1**.

**Table 10.7.1. – MU2 Provisions**

<b>Zoning Provision</b>	<b>All permitted uses</b>
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Minimum <b>streetwall height</b> (metres)	10.5
4. Minimum floor to floor <b>height of first storey</b> (metres)	4.5
5. Maximum <b>height</b>	The lesser of:  (a) 50 metres (b) 15 storeys
6. Minimum <b>front setback</b> (metres)	3.0
7. Minimum <b>rear setback</b> (metres)	10.0
8. Minimum <b>exterior setback</b> (metres)	3.0
9. Minimum <b>interior setback</b> (metres)	3.0
10. Minimum <b>stepbacks</b> (metres)	(a) Front lot line and exterior lot line: i. <b>storey 5 to 6</b> : minimum 3.0 metres from the exterior wall of the 4 <sup>th</sup> <b>storey</b>
11. Minimum <b>landscaped open space</b>	15%
12. Maximum <b>lot coverage</b>	55%
13. Maximum residential <b>density</b>	300 <b>dwelling units</b> per net hectare

**Additional Provisions for Lots Zoned MU2**

**10.7.3.** In addition to the provisions of Table **10.7.1.**, **uses** in the MU2 Zone must comply with the following provisions:

**Front Setback of Streetwall**

1. A minimum of 75% of the **streetwall** of a **building** must be built to the minimum required **front setback** for the **height** of the **streetwall**.

**Ground Floor Commercial Uses**

2. **Buildings** are required to have ground floor commercial **uses** on the **first storey** where any portion of the **lot** aligns with the

area identified as “Required Ground Floor Commercial” on Schedule 3 of this By-law.

3. Where ground floor commercial **uses** are required by Subclause 2., the entire **streetwall** of the **first storey**, excluding areas devoted to a lobby or other shared entrances/exits for other permitted **uses**, must be occupied by commercial **uses**. Portions of the floor area of the **first storey** that do not have an exterior wall facing a **street line** may be occupied by **uses** that service the **building** such as **loading spaces**, waste management facilities and rooms, mechanical rooms, **bike parking** facilities and other similar **uses**.

#### Main Pedestrian Entrance

4. The main pedestrian entrance to the **building** must be located on the **main wall** facing the **front lot line**.

#### Projecting Balconies

5. **Balconies** are only permitted above the second **storey**, to a maximum projection of 1.5 metres from the **main wall**.

#### Loading and Parking

6. **Parking structures** are not permitted to occupy any part of a **main wall** facing a **street line**.
7. **Loading spaces, parking spaces** and **parking lots** are not permitted in a **front yard** or exterior **yard**.

#### Tower Conditions

8. The maximum floor plate of a **tower** is 800 square metres. **Tower** floor plate includes all areas enclosed within exterior walls, including hallways, elevators, stairs, mechanical shafts, and all similar components.
9. The minimum **separation distance** between a **tower** and another **tower** is 25.0 metres.
10. The minimum **setback** from a **tower** to a **lot line** shared with an adjacent property is 12.5 metres.

11. Despite Subclause 10., where an adjacent property has already been developed with a **tower**, the **tower** is permitted to be located closer than 12.5 metres to the **lot line** shared with that adjacent property so long as the 25.0 metre **tower separation distance** is maintained.
12. The minimum **setback** from a **tower** to the exterior wall of the **podium** is 2.0 metres.

**Planting Strip**

13. Where a **lot** is adjacent to a **lot** in a UR Zone, a Commercial Zone or an Employment Zone, within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

**Legally Existing Uses**

14. **Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted uses and must comply with the provisions of the UR5 Zone.
15. Commercial **uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the CN Zone.

**10.8 Mixed Zone (MU3)**

- 10.8.1. The **use** of any **lot** or **building** in the MU3 Zone must comply with the provisions of Table 10.8.1.

**Table 10.8.1. – MU3 Provisions**

Zoning Provision	All permitted uses
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Minimum <b>height</b> (storeys)	2
4. Maximum <b>height</b>	The lesser of:  a) 4 storeys b) 12.5 metres

5. Minimum <b>front setback</b> (metres)	3.0
6. Minimum <b>rear setback</b> (metres)	8.0
7. Minimum <b>exterior setback</b> (metres)	3.0
8. Minimum <b>interior setback</b> (metres)	a) non-residential buildings: equal to half the height of the building b) <b>residential buildings</b> up to 3 storeys: 1.2 metres c) each additional <b>storey</b> above 3: 1.2 metres d) Despite (b) and (c), for <b>townhouses</b> , where a common party wall is located along a lot line: 0 metres
9. Minimum <b>landscaped open space</b>	30%
10. Maximum <b>lot coverage</b>	45%
11. Maximum residential <b>density</b>	90 <b>dwelling units</b> per net hectare

**Additional Provisions for Lots Zoned MU3**

**10.8.2.** In addition to the provisions of Table 10.8.1., **uses** in the MU3 Zone must comply with the following provisions:

**Front Setback of Streetwall**

1. A minimum of 75% of the **streetwall** of a **building** must be built to the minimum required **front setback** for the **height** of the **streetwall**.

**Loading and Parking**

2. **Loading spaces, parking spaces** and **parking lots** are not permitted in a **front yard** or **exterior yard**.

**Planting Strip**

3. Within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

**Legally Existing Uses**

**4. Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted uses and must comply with the provisions of the UR5 Zone.”

1.10. Clause 12.1.1. is amended by deleting “and Urban Multi-Residential Zone 10 (URM10).” and replacing with “, Urban Multi-Residential Zone 10 (URM10) and Urban Multi-Residential Zone 11 (URM11).”

1.11. **Table 12.1.2.** is amended by inserting a new column titled URM11 as follows:

Use	URM11
Residential apartment building	●
dwelling unit in a mixed use building	● <sup>1</sup>
house	—
semi-detached house	—
stacked townhouse	●
townhouse	●
Non-Residential community centre	●
day care centre	●
elementary school	●
library	●
museum	●
place of worship	●
secondary school	● <sup>2</sup>

1.12. A new Subsection 12.12. is added as follows:

**“12.12. Urban Multi-Residential Zone 11 (URM11)**

**12.12.1.** The **use** of any **lot** or **building** in the URM11 Zone must comply with the provisions of Table 12.12.1.

**Table 12.12.1. – URM11 Provisions**

<b>Zoning Provision</b>	<b>All permitted uses</b>
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Maximum <b>height</b>	The lesser of:  a) <b>4 storeys</b> b) 13.5 metres
4. Minimum <b>front setback</b> (metres)	4.5
5. Minimum <b>rear setback</b> (metres)	7.5
6. Minimum <b>exterior setback</b> (metres)	4.5
7. Minimum <b>interior setback</b> (metres)	1.2 metres for the first three <b>storeys</b> , plus an additional 1.2 metres for the fourth <b>storey</b>
8. Minimum <b>landscaped open space</b>	30%
9. Maximum <b>lot coverage</b>	45%
10. Maximum residential <b>density</b>	90 <b>dwelling units</b> per net hectare

**Additional Provisions for Lots Zoned URM11**

**12.12.2.** In addition to the provisions of Table 12.12.1., **uses** in the URM11 Zone must comply with the following provisions:

- 1. Loading spaces, parking spaces**, except for **accessible spaces**, and **parking lots** are not permitted in a **front yard** or **exterior yard**.
- 2. Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted uses and must comply with the provisions of the UR5 Zone.”



- 1.13. Section 20 – Legacy Exceptions is amended by deleting Legacy Exceptions ‘L102’ and ‘L192’ in their entirety and replacing each with the word “Deleted”.
- 1.14. Section 21 – Exceptions is amended by adding Exception Numbers ‘E186’, ‘E187’, ‘E188’, ‘E189’, ‘E190’, ‘E191’, ‘E192’, ‘E193’, and ‘E194’, as follows:
- “**E186.** Despite anything to the contrary in this By-law, the following provisions apply to the lands subject to this Exception:
- (a) In addition to the **uses** permitted by the applicable Zone, the following **uses** are permitted
    - (i) All **uses** permitted in the UR5 Zone as per the provisions of the UR5 Zone; and
    - (ii) **Dwelling unit in a mixed use building.**
- E187.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **height** is 12 **storeys**.
  - (b) The maximum floorplate size above the sixth **storey** is 1,200 square metres.
  - (c) The maximum **density** is 200 **dwelling units** per net hectare; and
  - (d) Residential **uses** that legally existed on the date of passing of the site specific by-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.
- E188.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **density** is 120 **dwelling units** per net hectare.
- E189.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **height** is 6 **storeys**.

- (b) The maximum **density** is 175 **dwelling units** per net hectare.
- (c) The fifth and sixth **storeys** are required to **stepback** a minimum of 3.0 metres from the exterior wall of the 4th **storey** along the **front lot line**.

**E190.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum number of **dwelling units** is 230.

**E191.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum **density** is 175 **dwelling units** per net hectare.
- (b) The maximum **floor space index** does not apply.
- (c) The fifth and sixth **storeys** are required to **stepback** a minimum of 3.0 metres from the exterior wall of the 4th **storey** along the **front lot line**.

**E192.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum number of **dwelling units** is 40.
- (b) The minimum **rear setback** is 55 metres.

**E193.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) In addition to the **uses** permitted by the applicable Zone, the following uses are permitted:
  - (i) **Special needs facility**; and
  - (ii) **Wellness clinic**;
- (b) The lands subject to this Exception are deemed to be one **lot** for the purposes of interpreting zoning provisions;
- (c) **Uses** existing as of the date of passing of this site specific by-law must comply with the provisions of the URM11 Zone;

- (d) Where a **lot** contains more than one **use**, the required number of **parking spaces** is the sum of all **parking spaces** required;
- (e) The maximum number of **dwelling units** is 50;
- (f) The southernmost **lot line** is deemed to be the **front lot line**;
- (g) The minimum **front setback** is 3.0 metres;
- (h) The minimum **rear setback** is 4.5 metres;
- (i) The minimum **exterior setback** is 3.0 metres;
- (j) The maximum **height** is the lesser of 16.5 metres or 4 **storeys**;
- (k) **Accessible spaces** are permitted in the **front yard**;
- (l) The minimum **drive aisle** width is 5 metres for **accessible spaces** located in the **front yard**;
- (m) A **planting strip** is not required;
- (n) A **canopy** may project a maximum of 4.5 metres into a required **sight triangle**, provided it has a minimum clearance of 2.8 metres above the elevation of the Division Street centreline; and
- (o) A **canopy** may project a maximum distance of 1.7 metres into a required **setback**, provided such **canopy** is **setback** a minimum of 0.5 metres to any lot line.

**E194.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The minimum **front setback** is 1.5 metres.
- (b) The minimum **interior setback** is 0 metres on the north side and 0.6 metres on the south side.
- (c) The minimum size of a standard **parking space** is 2.6 metres wide and 5.2 metres long.
- (d) The minimum **driveway** width is 2.3 metres.

- (e) Unenclosed front **porches** and below-grade steps are permitted to project 1.5 metres out from the main building wall, with no minimum setback from the **front lot line**. Minimum **interior setback** for an unenclosed front **porch** and below-grade steps is 0.5 metres (south) and 0 metres (north).
  - (f) An **accessory building** may be located not less than 0.9 metres from the rear lot line and not less than 0.3 metres from the north interior lot line.
  - (g) **Amenity areas** may be aggregated into spaces of not less than 43 square metres.
- 1.15. Section 22 – Holding Conditions is amended by adding a new Holding Overlay Number H243 as follows:
- “H243.** Prior to the removal of the Holding Overlay, the following conditions must be satisfied:
- (a) The City is satisfied that there is adequate servicing capacity (i.e., water, wastewater, natural gas, and electrical) for the proposed development;
  - (b) A Transportation Impact Study is completed to the satisfaction of the City; and
  - (c) Confirmation from the Chief Building Official that a Record of Site Condition has been completed to the satisfaction of the City, or that no Record of Site Condition is required for the proposed development.”
- 1.16. Section 22 – Holding Conditions is amended by deleting Holding Overlays ‘H75’, ‘H189’, and ‘H203’ in their entirety and replacing each with the word “Deleted”.
- 1.17. Schedule 1 – Zoning Map is amended by changing the zone symbol of select lands to CN, DR, MU1, MU2, MU3, URM8, and URM11 as shown on Schedule “A1”, “A2”, “A3”, “A4”, “A5”, “A6” and “A7” attached to and forming part of this By-Law.
- 1.18. Schedule 2 – Parking Areas is amended by changing certain properties from PA3 and PA4 to PA2 and by adding new locations within ‘Parking Area 2’, as shown on Schedule “B” attached to and forming part of this By-Law.

- 1.19. Schedule 3 – Required Ground Floor Commercial is amended by adding additional locations for ground floor commercial, as shown on Schedule “C” attached to and forming part of this By-Law;
  - 1.20. Schedule 4 - Road Classification is amended by reclassifying Rideau Street to a Collector Road and the Waaban Crossing to an Arterial Road as shown on Schedule “D” attached to and forming part of this By-Law;
  - 1.21. Schedule E – Exception Overlay is amended as shown on Schedule “E” attached to and forming part of this By-Law, as follows:
    - (a) By adding Exception ‘E186’, ‘E187’, ‘E188’, ‘E189’, ‘E190’, ‘E191’, ‘E192’, ‘E193’, and ‘E194’ for select lands;
    - (b) By deleting Legacy Exception ‘L102’ and replacing it with Exception ‘E194’ for the property municipally known as 317 Montreal Street;
    - (c) By deleting Legacy Exception ‘L192’ and replacing it with Exception ‘E186’ for the property municipally addressed as 235 Montreal Street; and
    - (d) By adjusting the boundary of the Legacy Exception ‘L205’ to align with the amended URM2 zone for the property municipally known as 720 – 766 John Counter Boulevard.
  - 1.22. Schedule F – Holding Overlay is amended as shown on Schedule “F” attached to and forming part of this By-Law, as follows:
    - (a) By adding Holding Overlay Numbers ‘H243’ and ‘H244’ for select lands;
    - (b) By adjusting the boundary of Holding Overlay Numbers ‘H29’, ‘H76’ and ‘H154’; and
    - (c) By deleting Holding Overlay Numbers ‘H75’, ‘H189’, and ‘H203’.
  - 1.23. Schedule H – Express Transit Overlay is amended by deleting the inset, the properties fronting on Montreal Street and the frontage of 541 Division Street as shown on Schedule “G” attached to and forming part of this By-Law;
2. This By-Law shall come into force in accordance with the provisions of the *Planning Act*.

Given all Three Readings and Passed: [Meeting Date]

---

**Janet Jaynes**

**City Clerk**

---

**Bryan Paterson**

**Mayor**



**Schedule 'A1'  
to By-Law Number**

File Number: D35-001-2025

Description: Zone Changes to MU1, OS2

**Certificate of Authentication**

This is Schedule 'A1' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Planning  
Services**

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

**Legend**

Lands to be rezoned from:

- |           |            |               |            |
|-----------|------------|---------------|------------|
| CA to MU1 | M2 to MU1  | UR5 to MU1    | N/A to OS2 |
| DR to MU1 | N/A to MU1 | UR6 to to MU1 |            |
| M1 to MU1 | OS2 to MU1 | URM2 to MU1   |            |

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





Planning  
Services

### Schedule 'A2' to By-Law Number

File Number: D35-001-2025  
Description: Zone Changes to MU2

### Certificate of Authentication

This is Schedule 'A2' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

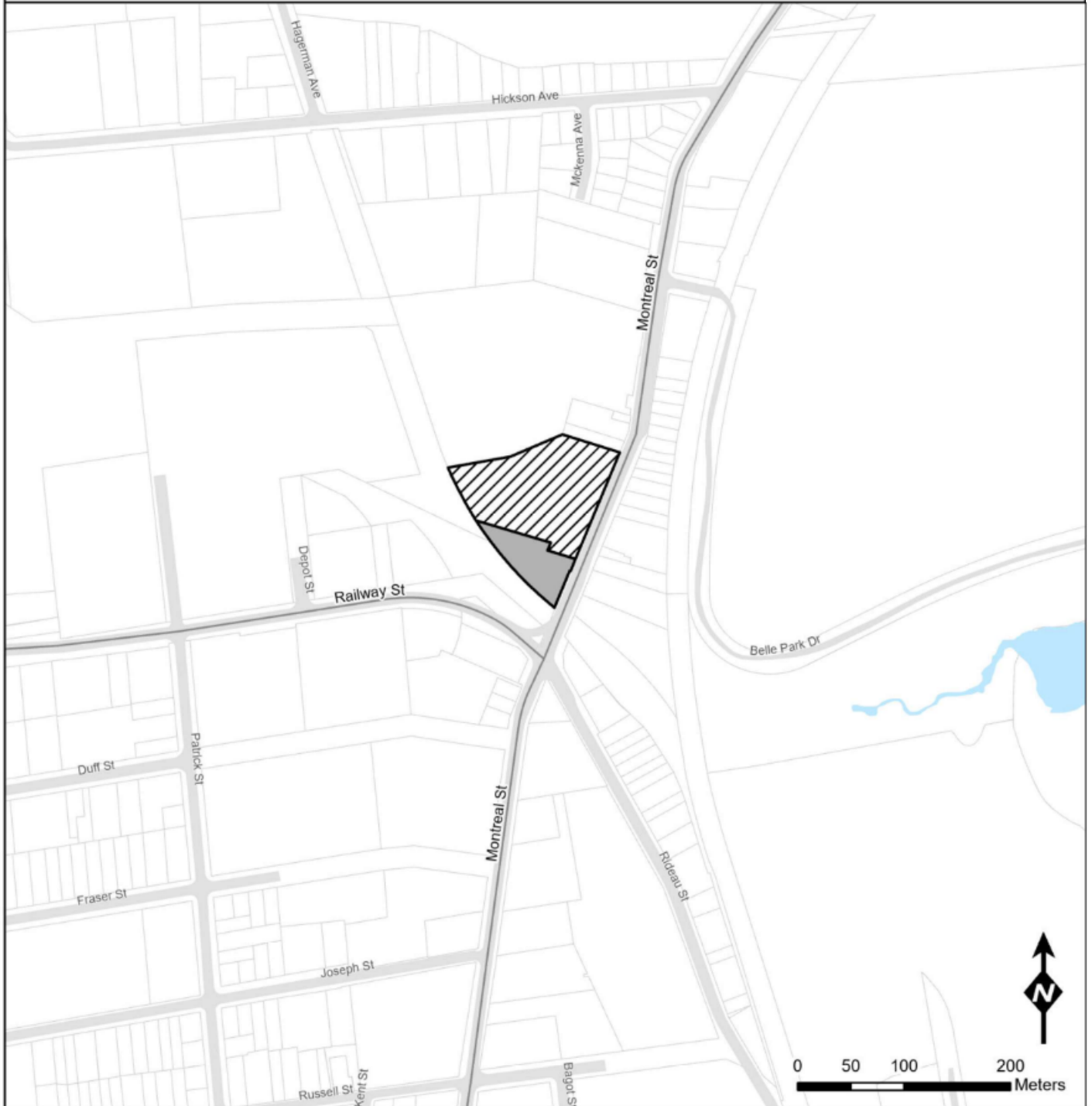
\_\_\_\_\_  
Clerk

### Zoning By-Law 2022-62 Schedule 1, Zone Map

#### Legend

Lands to be rezoned from:

- M1 to MU2
- M2 to MU2







### Schedule 'A3' to By-Law Number

### Zoning By-Law 2022-62 Schedule 1, Zone Map

Planning  
Services

File Number: D35-001-2025  
Description: Zone Changes to MU3

#### Legend

Lands to be rezoned from:

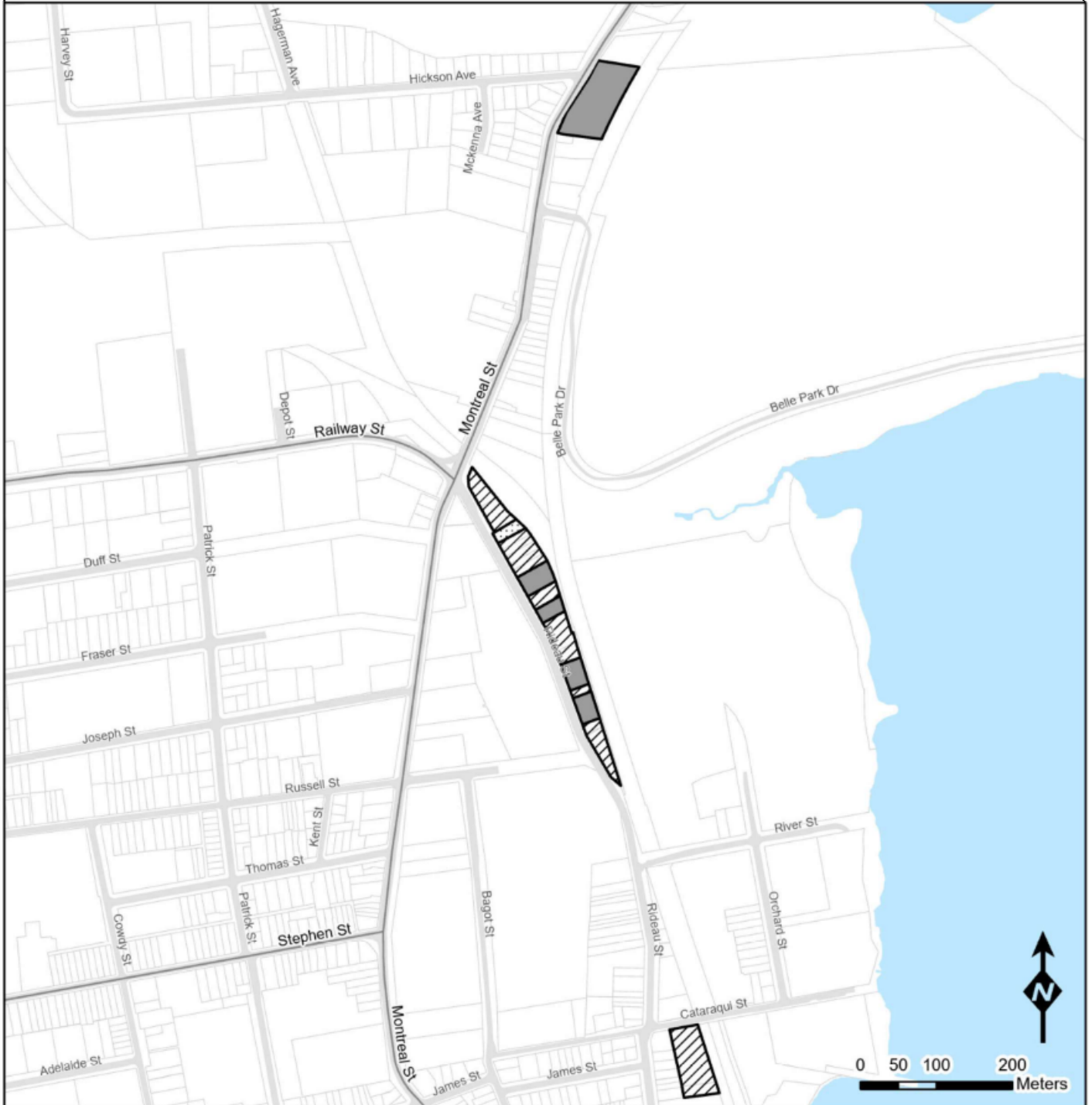
- DR to MU3
- N/A to MU3
- UR5 to MU3

#### Certificate of Authentication

This is Schedule 'A3' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





Planning  
Services

### Schedule 'A4' to By-Law Number

File Number: D35-001-2025  
Description: Zone Changes to URM8

### Certificate of Authentication

This is Schedule 'A4' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

### Zoning By-Law 2022-62 Schedule 1, Zone Map

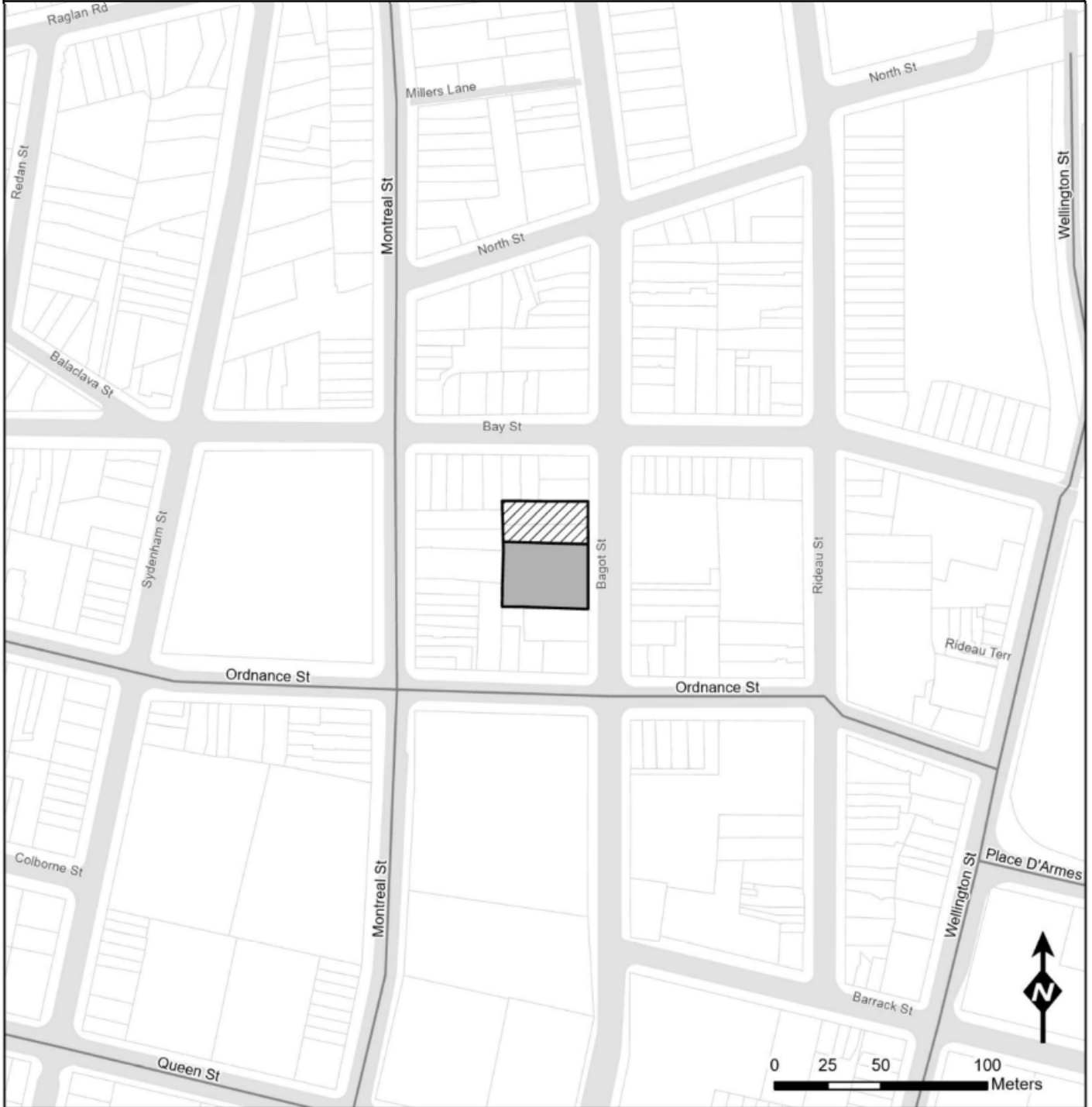
#### Legend

Lands to be rezoned from:

- UR5 to URM8
- N/A to URM8

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





### Schedule 'A5' to By-Law Number

File Number: D35-001-2025  
Description: Zone Changes to URM11

**Planning  
Services**

### Certificate of Authentication

This is Schedule 'A5' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

### Zoning By-Law 2022-62 Schedule 1, Zoning Map

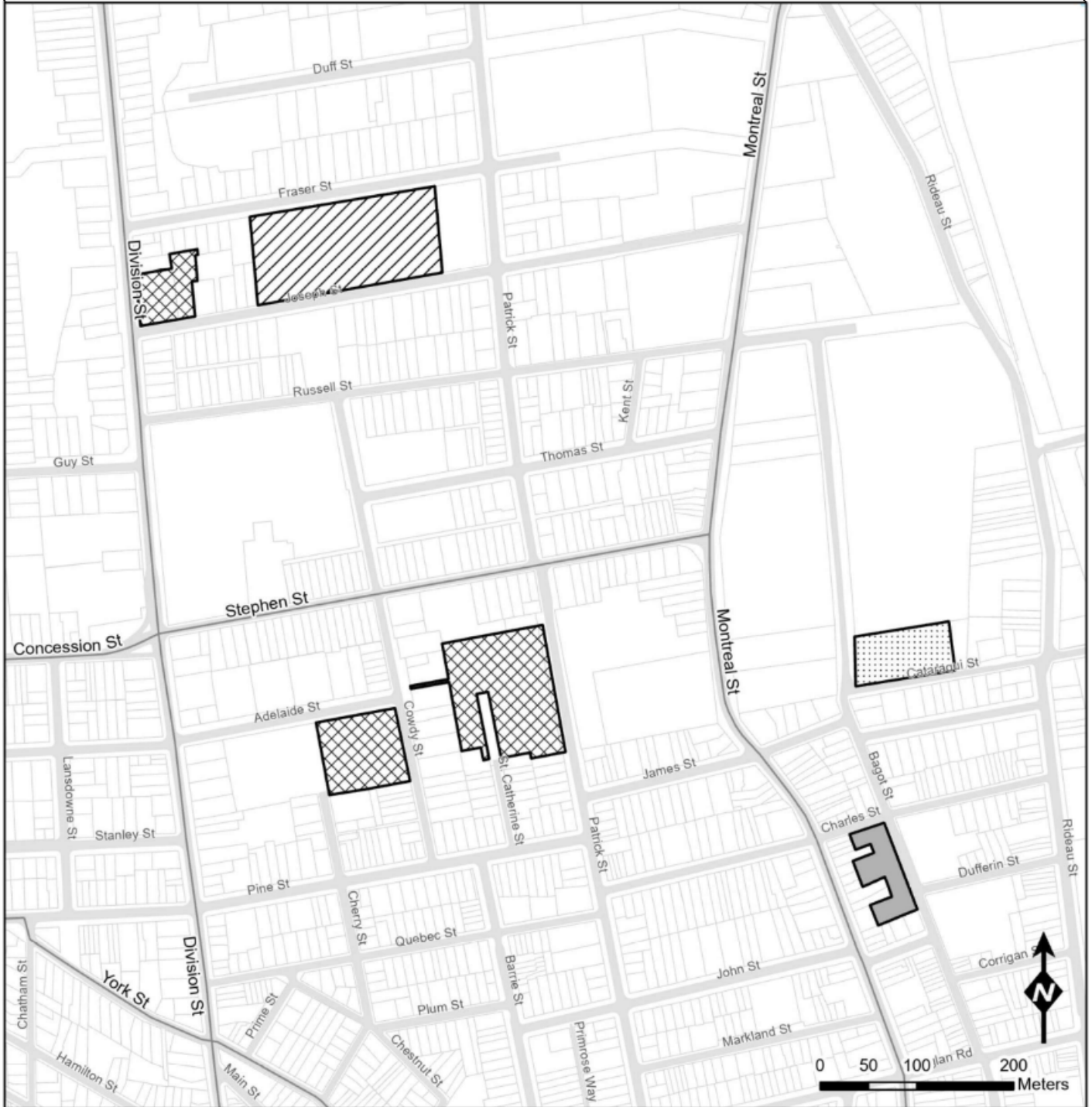
#### Legend

Lands to be rezoned from:

- |             |              |
|-------------|--------------|
| CD to URM11 | UR5 to URM11 |
| DR to URM11 | N/A to URM11 |

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





Planning  
Services

### Schedule 'A6' to By-Law Number

File Number: D35-001-2025

Description: Zone Changes to CN

### Certificate of Authentication

This is Schedule 'A6' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

### Zoning By-Law 2022-62 Schedule 1, Zone Map

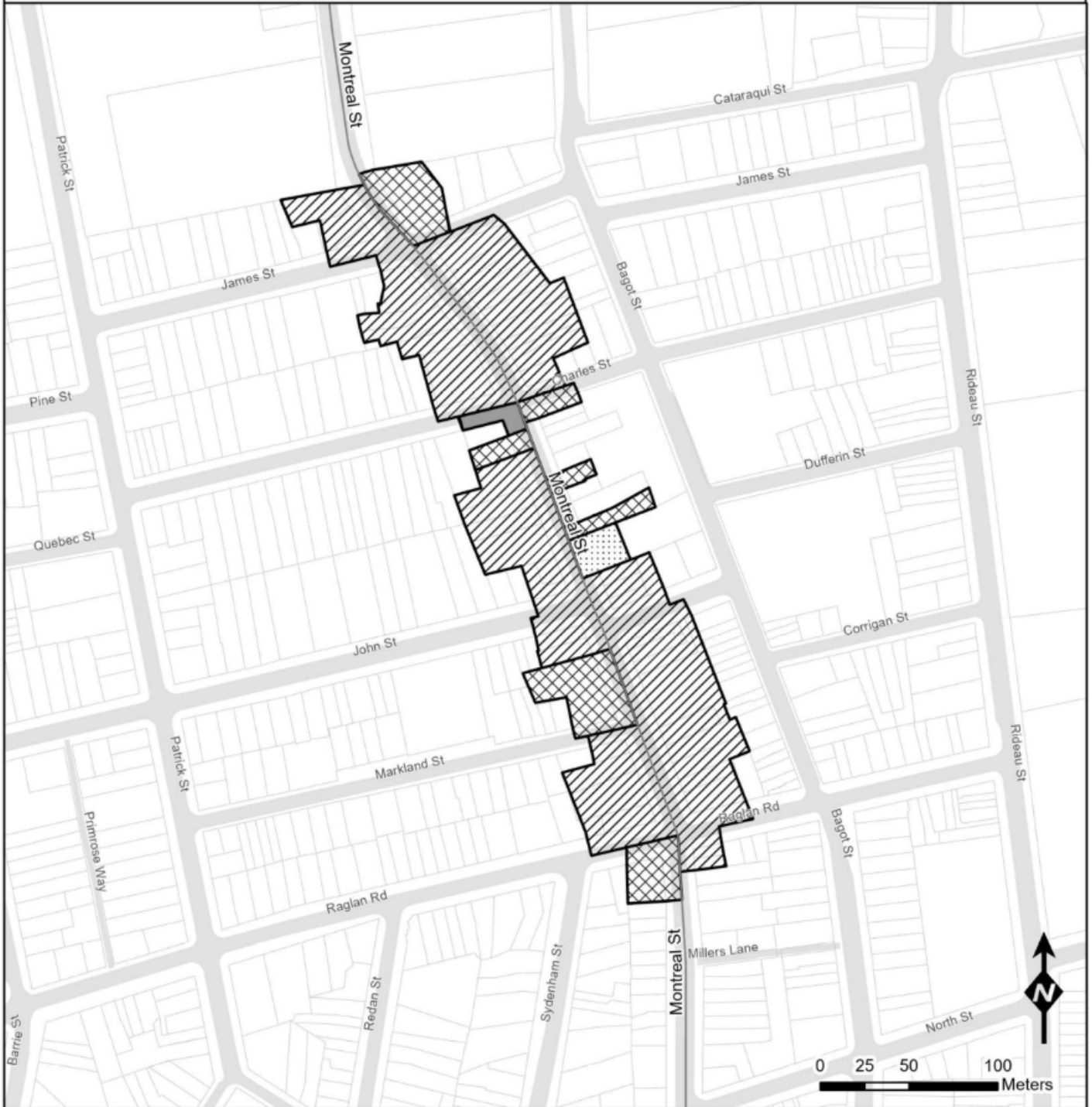
#### Legend

Lands to be rezoned from:

- |           |            |
|-----------|------------|
| CD to CN  | URM3 to CN |
| UR5 to CN | N/A to CN  |

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





Planning  
Services

**Schedule 'A7'  
to By-Law Number**

File Number: D35-001-2025  
Description: Zone Change to DR

**Certificate of Authentication**

This is Schedule 'A7' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

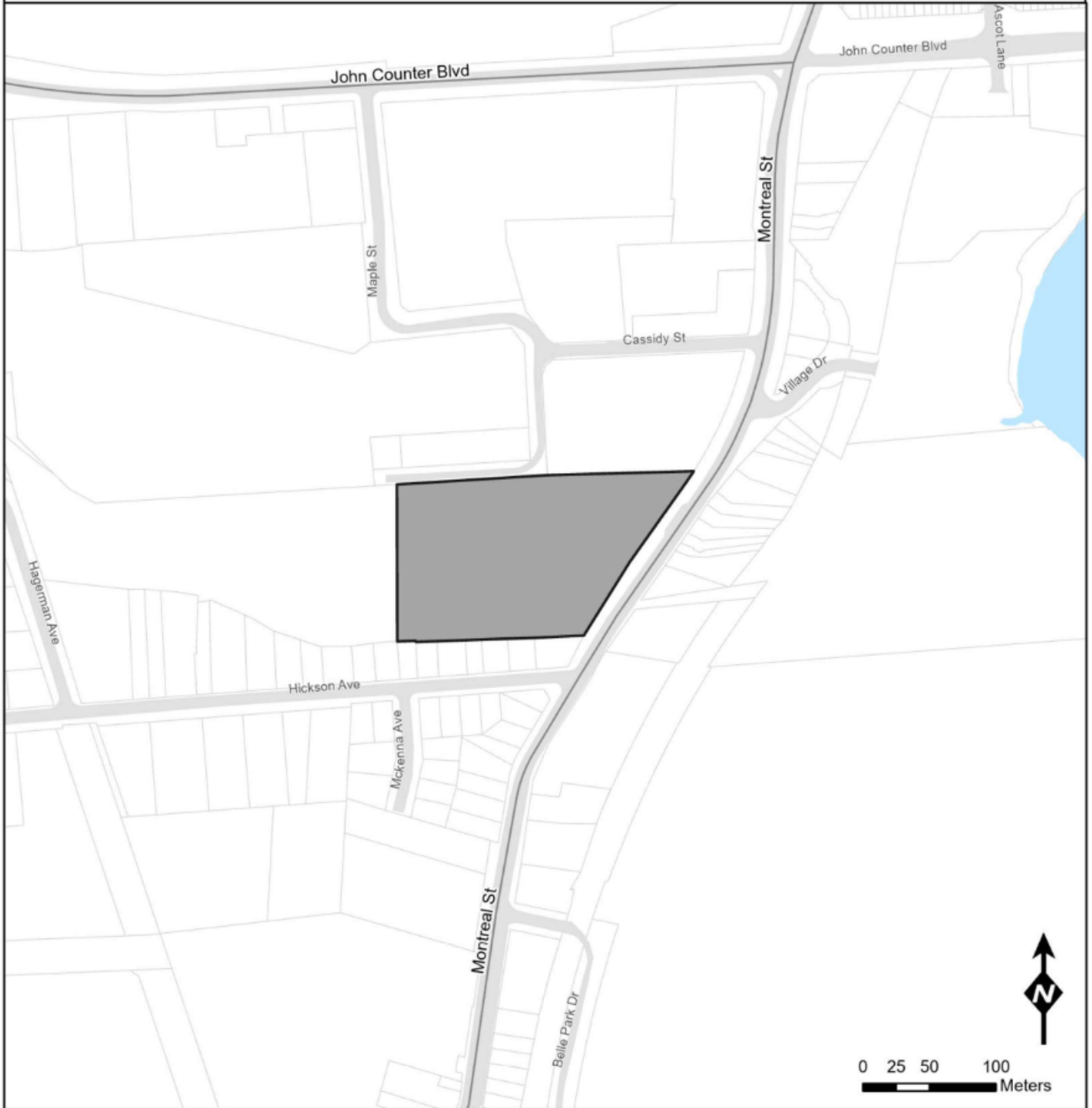
\_\_\_\_\_  
Clerk

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

**Legend**

Lands to be rezoned from:

 N/A to DR





# Schedule 'B' to By-Law Number

# Zoning By-Law 2022-62 Schedule 2, Parking Areas

Planning  
Services

File Number: D35-001-2025

### Legend

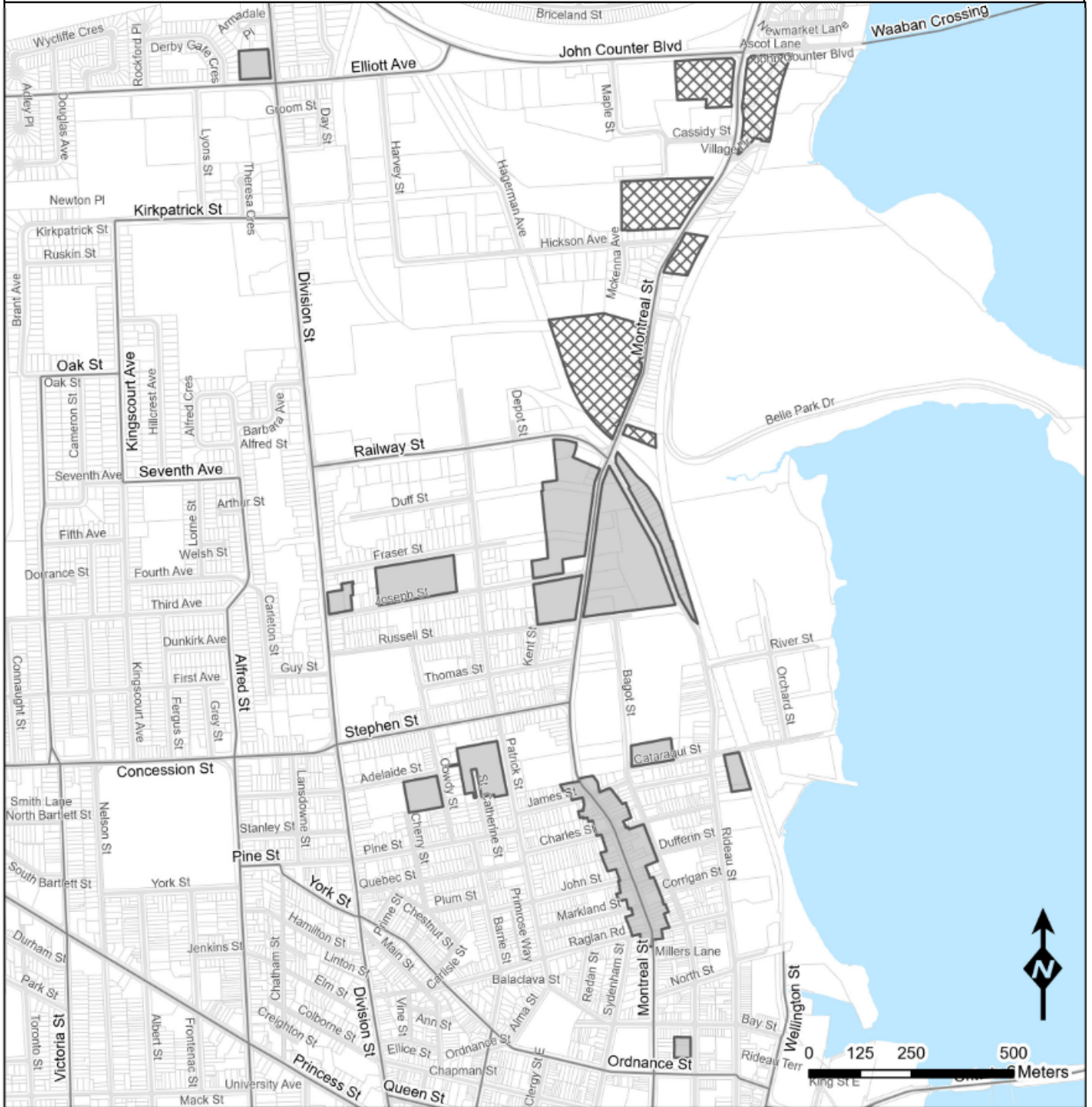
- Change from PA3 to PA2
- Change from PA4 to PA2

## Certificate of Authentication

This is Schedule 'B' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





**Schedule 'C'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'C' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62**

**Schedule 3, Required Ground Floor Commercial**

**Legend**

— Required Ground Floor Commercial to be added

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





**Schedule 'D'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'D' to By-Law Number \_\_\_\_, passed this \_\_\_\_ day of \_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

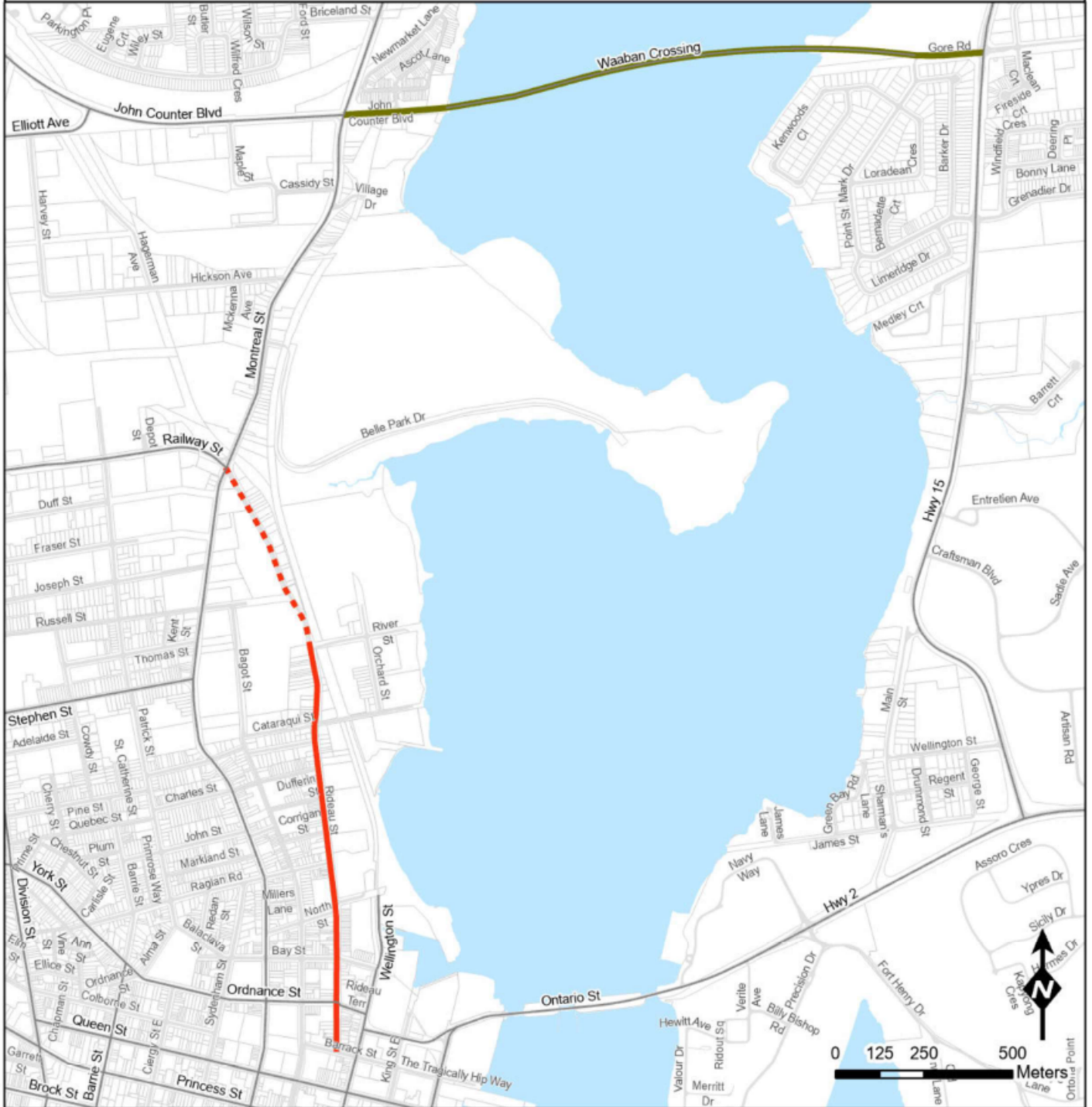
\_\_\_\_\_  
Clerk

**Zoning By-Law 2022-62**

**Schedule 4, Road Classification Urban**

**Legend**

- Road to be reclassified from Local to Arterial
- Road to be reclassified from Arterial to Collector
- Road to be reclassified from Local to Collector







**Schedule 'E'  
to By-Law Number**

File Number: D35-001-2025




**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'E' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

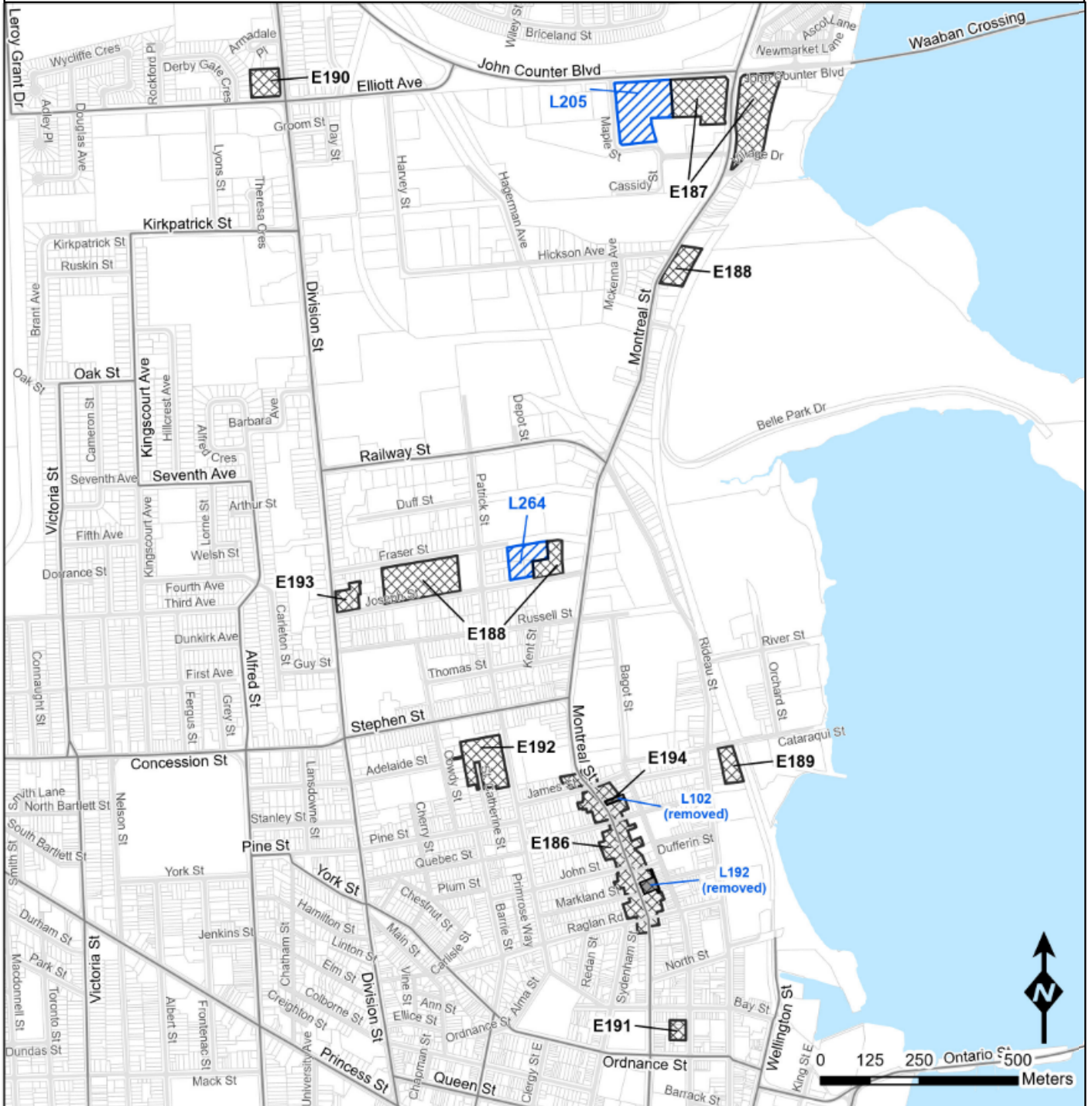
**Zoning By-Law 2022-62  
Schedule E, Exception Overlay**

**Legend**

-  Exception Overlay to be added (numbering as shown)
-  Legacy Exception Overlay to be removed
-  Legacy Exception Overlay to be adjusted as shown

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk





**Schedule 'F'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'F' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk

**Zoning By-Law 2022-62  
Schedule F, Holding Overlay**

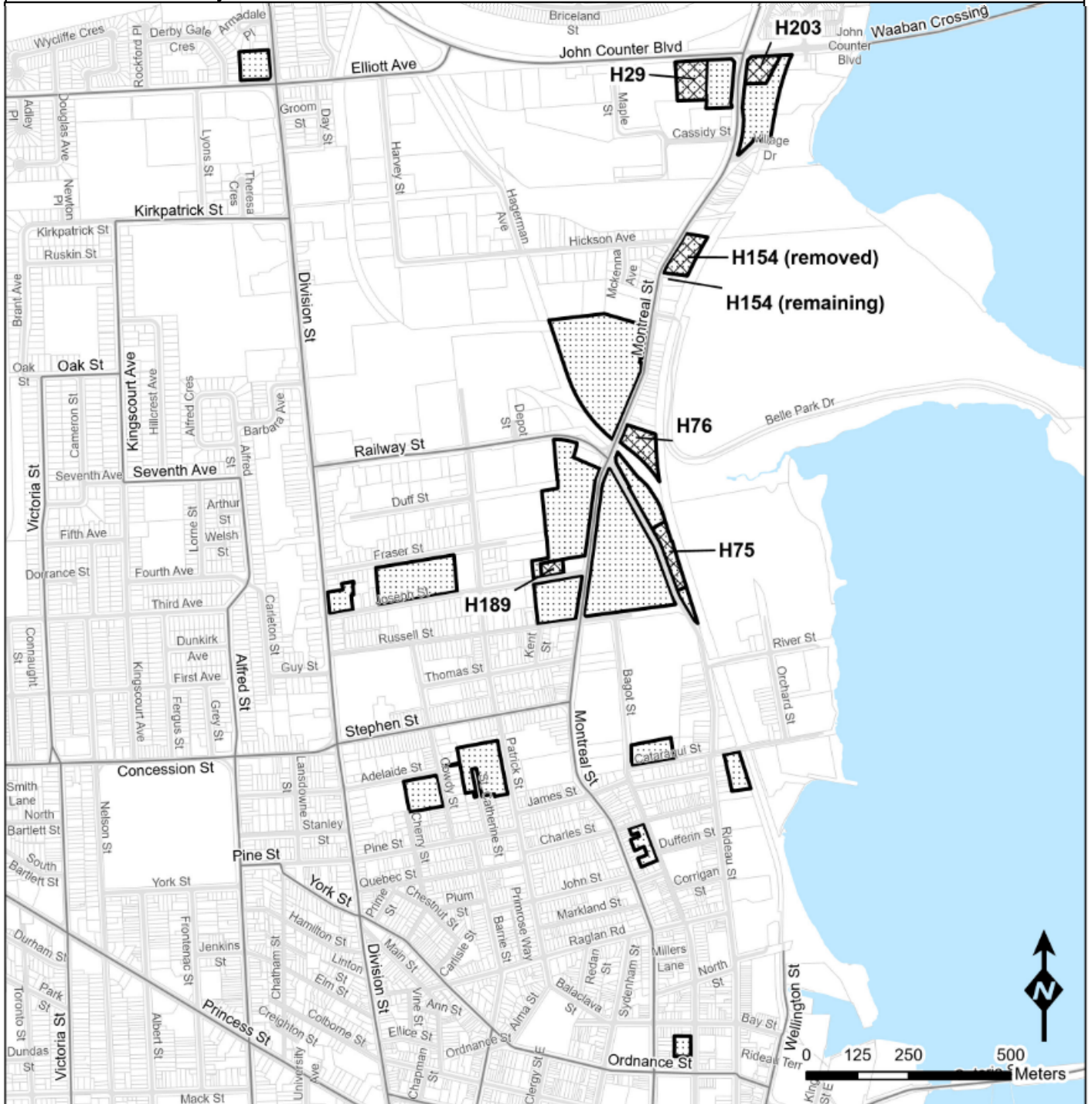
**Legend**



Lands to be added to  
Holding Overlay H243



Existing Holding Overlay to  
be removed (numbering as  
shown)





**Schedule 'G'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'G' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule H, Express Transit Area Overlay**

**Legend**

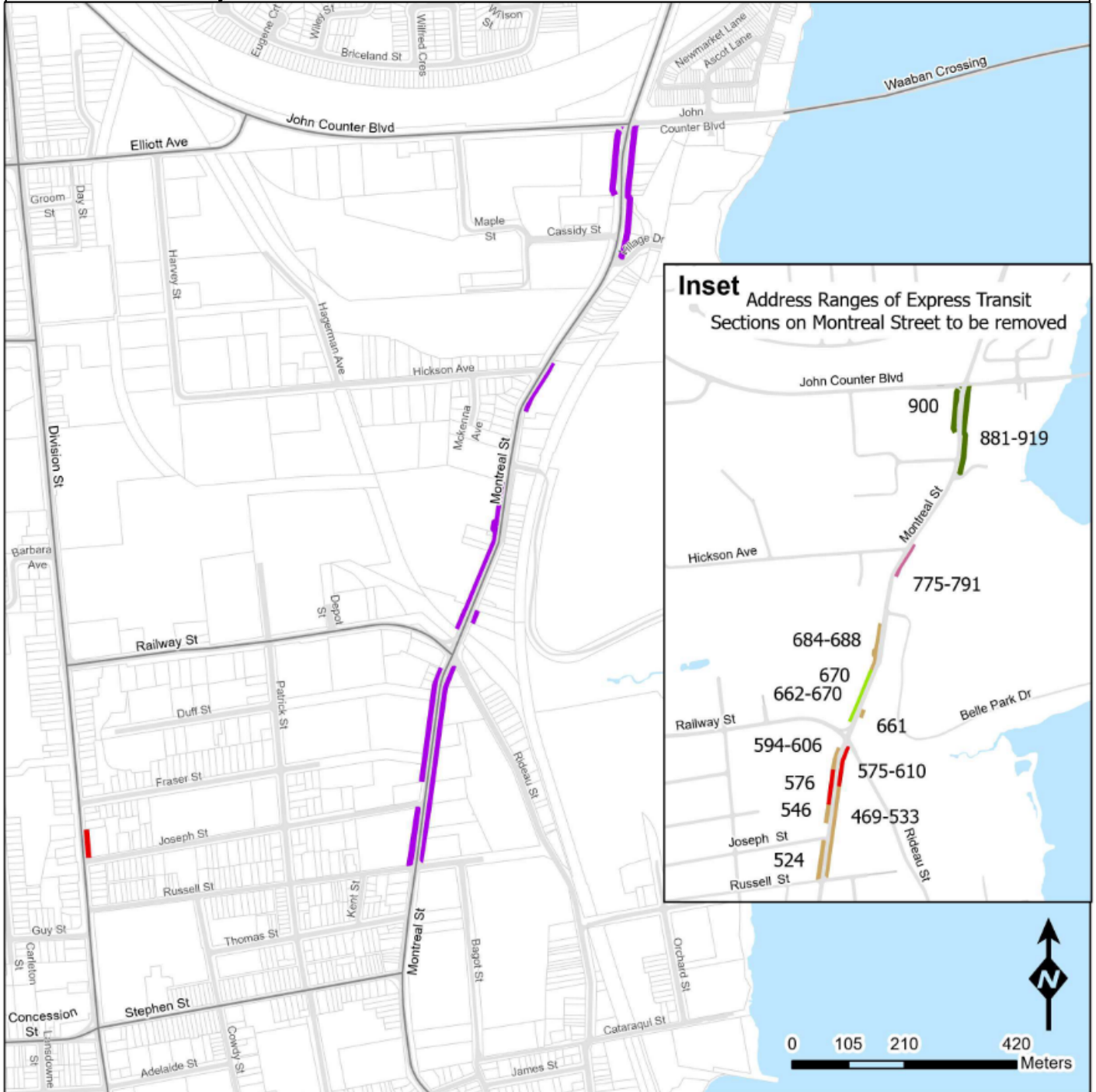
Express Transit Sections to be deleted from:

- Frontage of 541 Division Street
- Montreal Street

See Inset for detail

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



**By-Law Number 202X-XX**

**A By-Law to Amend By-Law Number 8499, “Restricted Area (Zoning) By-Law of the Corporation of the City of Kingston” (North King’s Town Specific Policy Area)**

Passed: **Date**

**Whereas** by Order of the Ministry of Municipal Affairs and Housing, The Corporation of the Township of Kingston, The Corporation of the Township of Pittsburgh and The Corporation of the City of Kingston were amalgamated on January 1, 1998 to form The Corporation of the City of Kingston as the successor municipal corporation and pursuant to the Minister’s Order, any by-laws of the former municipality passed under the *Planning Act* continue as the by-laws covering the area of the former municipality now forming part of the new City; and

**Whereas** the Council of The Corporation of the City of Kingston deems it advisable to amend By-Law Number 8499, as amended, of the former City of Kingston;

**Therefore be it resolved that** the Council of The Corporation of the City of Kingston hereby enacts as follows:

1. By-Law Number 8499 of The Corporation of the City of Kingston, entitled “Restricted Area (Zoning) By-Law of the Corporation of the City of Kingston”, as amended, is hereby amended as follows:
  - 1.1. By deleting the following rows in their entirety within Table 1 “Zones and Red Exceptions Subject to this By-law”:

A	Not Applicable	UR5
A.139	On the approximately 0.53 hectare parcel of land located at the northeast corner of Bagot and Cataraqui Streets and designated A.139 on a copy of Zoning Map Number 17 attached to and forming part of By-Law Number 9230 as Schedule "A", the permitted uses shall also include a Brewers Retail sales outlet and distributing warehouse.	CN
A.142	On the parcel of land identified as 298-394 Montreal Street and designated A.142 on copies of Zoning Map Number 17 and 18 attached to and forming part of By Law Number 9233, as Schedules "A" and "B", the permitted uses shall also include neighbourhood shores and a retail store selling office supplies provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial. Uses established on these premises	URM3

	as of July 28, 1975 plus an area equal to 10 per cent of the previously described area.	
A.143	On the parcel of land identified as 240-246 Montreal Street and Designated A.143 on a copy of Zoning Map Number 18 attached to and forming part of By Law Number 9233 as Schedule "B" the permitted uses shall also include a drive-in restaurant and neighbourhood stores provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial uses established on these premises as of July 28, 1975 plus an area equal to 10 per cent of the previously described area.	UR5
A.145	On the parcel of land identified as 277 Montreal Street and designated A.145 on copies of Zoning Maps Number 17 and 18 attached to and forming part of By Law Number 9233 as Schedules "A" and "B", the permitted uses shall also include neighbourhood stores provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial uses established on these premises as of July 28, 1975 plus an area equal to 10 per cent of the previously described area.	CN
A.146	On the parcel of land identified as 285 Montreal Street and designated A.146 on copies of Zoning Maps Number 17 and 18 attached to and forming part of By Law Number 9233 as Schedules "A" and "B", the permitted uses shall also include neighbourhood stores provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial uses established on these premises as of July 28, 1975 plus an area equal to 10 per cent of the previously described area.	CN
A.147	On the parcel of land identified as 303 Montreal Street and designated A.147 on copies of Zoning Maps Number 17 and 18 attached to and forming part of By Law Number 9233 as Schedules "A" and "B", the permitted uses shall also include neighbourhood stores provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial uses established these premises as of July 28, 1975 plus an area equal to 10 per cent of the previously described area.	CN
A.149	On the approximately 0.08 hectare parcel of land located on the west side of Bagot Street between Bay and Ordnance Streets and designated A.149 on a copy of Zoning Map Number 18 attached to and forming part of By-Law Number 9234, as Schedule "A", the permitted uses shall also include an ambulance service operated by the Ministry of Health.	UR5

A.171	<p>On the parcel of land identified as 212 Montreal Street and 68-70 Raglan Road and designated A.171 on a copy of Zoning Map Number 18 attached to and forming part of By-Law Number 79-263 as Schedule "B", the permitted uses shall also include neighbourhood stores and a retail florist shop provided that these commercial uses do not occupy a total floor area which is greater than the total floor area occupied by commercial uses established on these premises as of the date of passing of this by-law plus an area equal to 10 per cent of the previously described area.</p>	UR5
B3.346	<p>On the approximately 0.67 hectare parcel of land located at 41 Joseph Street, being Part of Lots 191 to 195 of Registered Plan C22, between Joseph Street and Fraser Street and zoned as 'B3.346' on the copy of Zoning Map Number 17 attached to and forming part of By-Law Number 2005-31 as Schedule "A", the following regulations shall apply:</p> <p>(a) Purpose and Requirement for Removal of Holding Symbol: The use and removal of the '-H' Holding Symbol shall be in accordance with the provisions of Section 5.39 of this by-law. To ensure the proper redevelopment of the lands, given the findings of the Environmental Site Assessment and Soils Investigations, development for residential or community facility uses will not proceed until the following have been completed:</p> <ul style="list-style-type: none"> <li>• An acceptable Remediation Plan is provided and carried out to the Municipality's satisfaction and that a Record of Site Condition is filed with the Ministry of the Environment;</li> <li>• A Site Plan Control Agreement is executed by the Owner, which Agreement shall incorporate the recommendations for site remediation and all other applicable municipal conditions.</li> </ul> <p>(b) Permitted Interim Uses: In accordance with Section 5.39 (i) of this by-law.</p> <p>(c) Zone Regulations When '-H' Symbol Removed: Notwithstanding any provisions of the By-Law to the contrary, the provisions of Section 16 for the 'B3' Multiple Family Dwelling Zone shall apply to the 'B3.346' Zone except that:</p> <p>(i) Notwithstanding the provisions of Section 16.2, the following additional uses shall be permitted:</p> <ul style="list-style-type: none"> <li>• Community Centre or Community Hall, including, but not necessarily limited to, counseling services, life skills groups, meeting spaces for clients of Home Base Non Profit Housing Inc. and other social service agencies, kitchen / cafeteria</li> </ul>	URM6

	<p>space, group counseling rooms, drop-in room, copies /supply room and offices for program staff and administration;</p> <ul style="list-style-type: none"> <li>• Crisis Care Shelter shall be limited to a maximum occupancy of 30 persons;</li> </ul> <p>(ii) The maximum gross floor area for the additional permitted uses listed in</p> <p>Clause (c) (i) above shall be 800 square metres;</p> <p>(iii) No Crisis Care Facility, Community Centre or Community Hall use shall</p> <p>be located closer than 20 metres to any other residential zone;</p> <p>(iv) The maximum height of any building shall be 2 storeys, not to exceed 9.0 metres;</p> <p>(v) Notwithstanding the provisions of Section 16.3 (g) and Section 5.3 of this by-law to the contrary, off-street parking facilities for residential uses shall be provided at a ratio of 1 parking space for each of the first two units, plus 1 parking space for each additional 4 units;</p> <p>(vi) Notwithstanding the provisions of Section 16.3 (f) of this by-law to the contrary, the maximum permitted density shall be 69 units per net hectare.</p> <p>(vii) Notwithstanding the provisions of Section 5.33(iv) of this by-law to the contrary, a permitted Crisis Care Shelter use may occupy a portion of a Community Centre or Community Hall building.</p>	
C1.317	<p>On the approximately 0.2 hectare (0.49 acre) parcel of land at the north-east corner of James and Montreal Street know as 351 Montreal Street, and designated 'C1.316' on the copy of Zoning Map Number 17 attached to and forming part of By-Law Number 2001-116 as Schedule "A", the following regulations shall apply:</p> <ul style="list-style-type: none"> <li>(i) Maximum lot coverage of 58 per cent.</li> <li>(ii) Minimum rear yard setback of 6 metres.</li> </ul>	CN
M6.118	<p>On those lands located west of Montreal Street and south of Railway Street and designated M6.97 and M6.118 on a copy of Zoning Map Number 17 attached to and forming part of By-Law Number 8728 as Schedule "A", the permitted uses shall also include the storage, warehousing, distribution, fabrication and otherwise processing of new steel.</p>	M1
M6.223	<p>On the approximately 0.64 hectare parcel of land located on the west side of Montreal Street north of Hickson Avenue and designated M6.223 on a copy of Zoning Map Number 15 attached to and forming part of By-Law Number 87-14 as</p>	M2

City of Kingston By-Law Number 2025-XX

	Schedule "A", the following regulations shall apply: (i) the permitted uses shall also include a restaurant within the limits of the existing roofed building area and adjacent outside areas; (ii) there shall be no expansion or enlargement of the existing buildings.	
M7	Not Applicable	URM2

2. Schedule "A" of Zoning By-Law Number 8499 is amended by removing the lands from Zoning By-Law Number 8499 as shown on Schedules "A1", "A2" and "A3" attached to and forming part of this By-Law.
3. This By-Law shall come into force in accordance with the provisions of the *Planning Act*.

Given all Three Readings and Passed: [Meeting Date]

---

**Janet Jaynes**  
City Clerk

---

**Bryan Paterson**  
Mayor





**Schedule 'A1'  
to By-Law Number**

File Number: D35-001-2025


**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'A1' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Kingston Zoning By-Law 8499  
Schedule A, Red Exceptions**

**Legend**

 Lands to be removed from Zoning By-Law Number 8499

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk







**Schedule 'A3'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'A3' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Kingston Zoning By-Law 8499  
Schedule A, Red Exceptions**

Legend

 Lands to be removed from Zoning By-Law Number 8499

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk

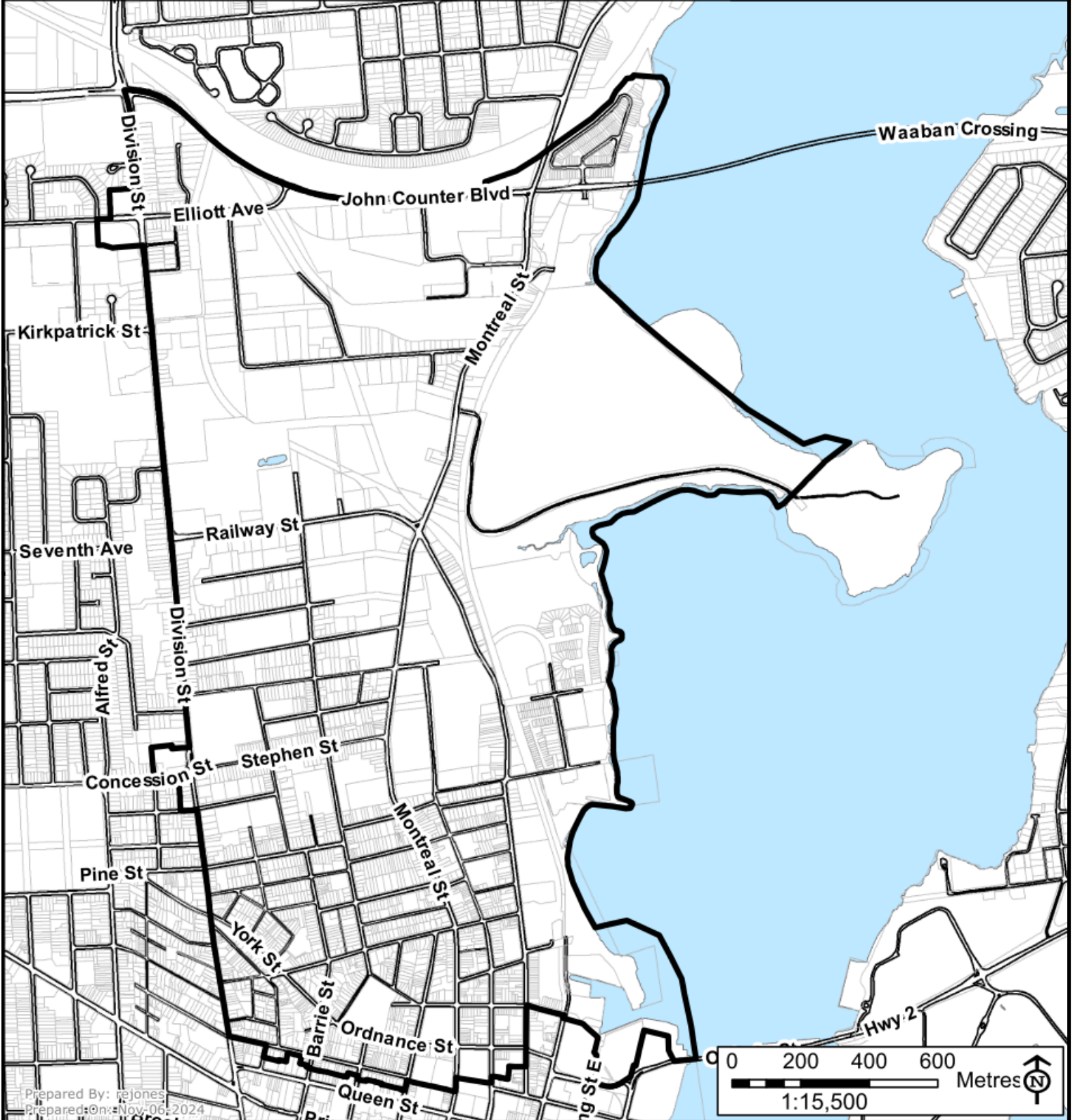
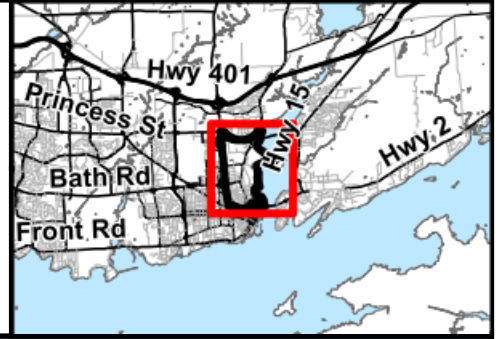




# CITY OF KINGSTON Key Map

File Number: D35-001-2025

 North King's Town Study Area



Prepared By: rejonas  
Prepared On: Nov-06-2024

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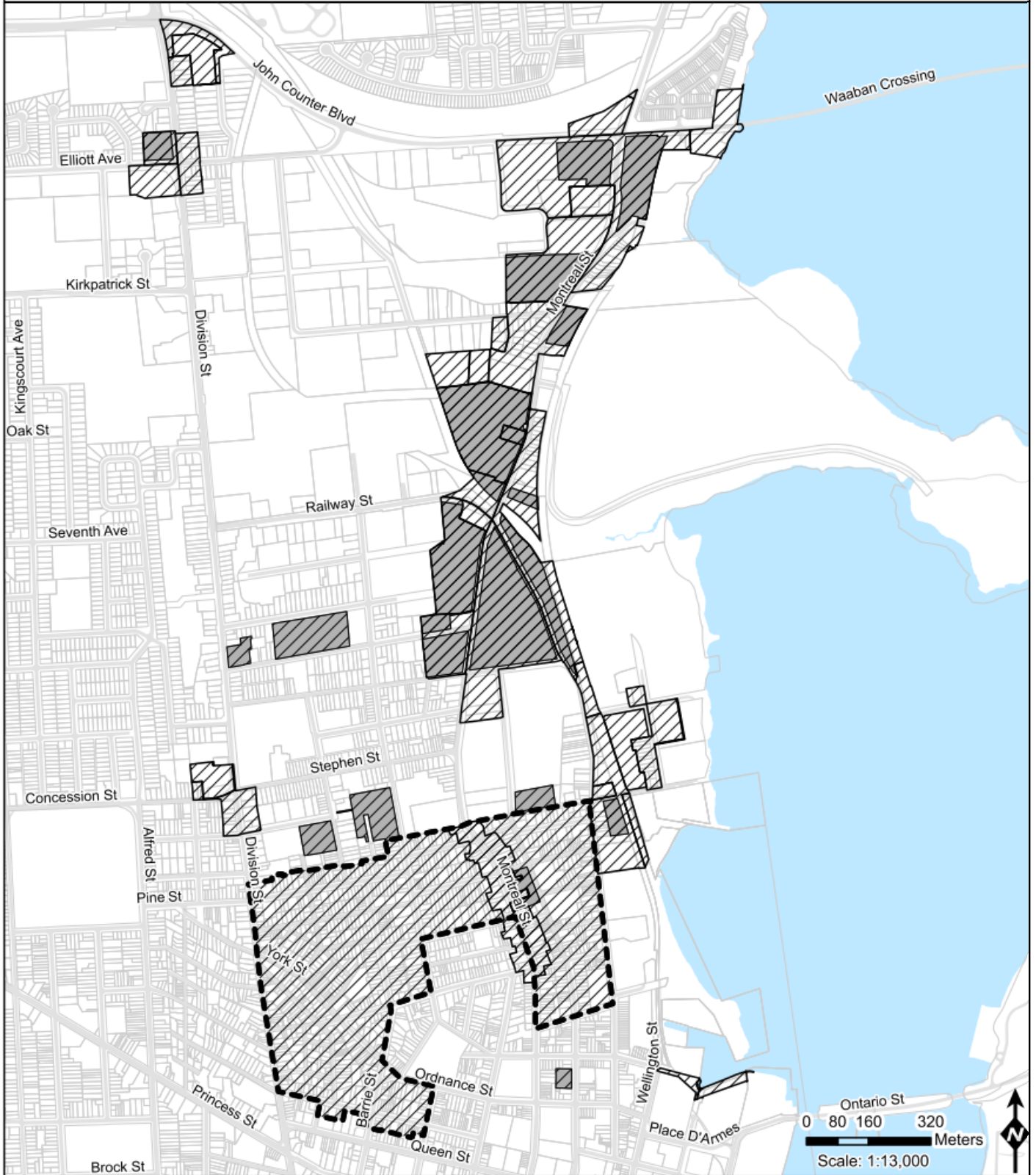


# Key Map

File Number: D35-001-2025

Lands subject to  
Official Plan or  
Zoning By-Law  
Amendment

Proposed  
Intensification Areas  
Lands to be Added  
to Heritage  
Character Area












Planning  
Services

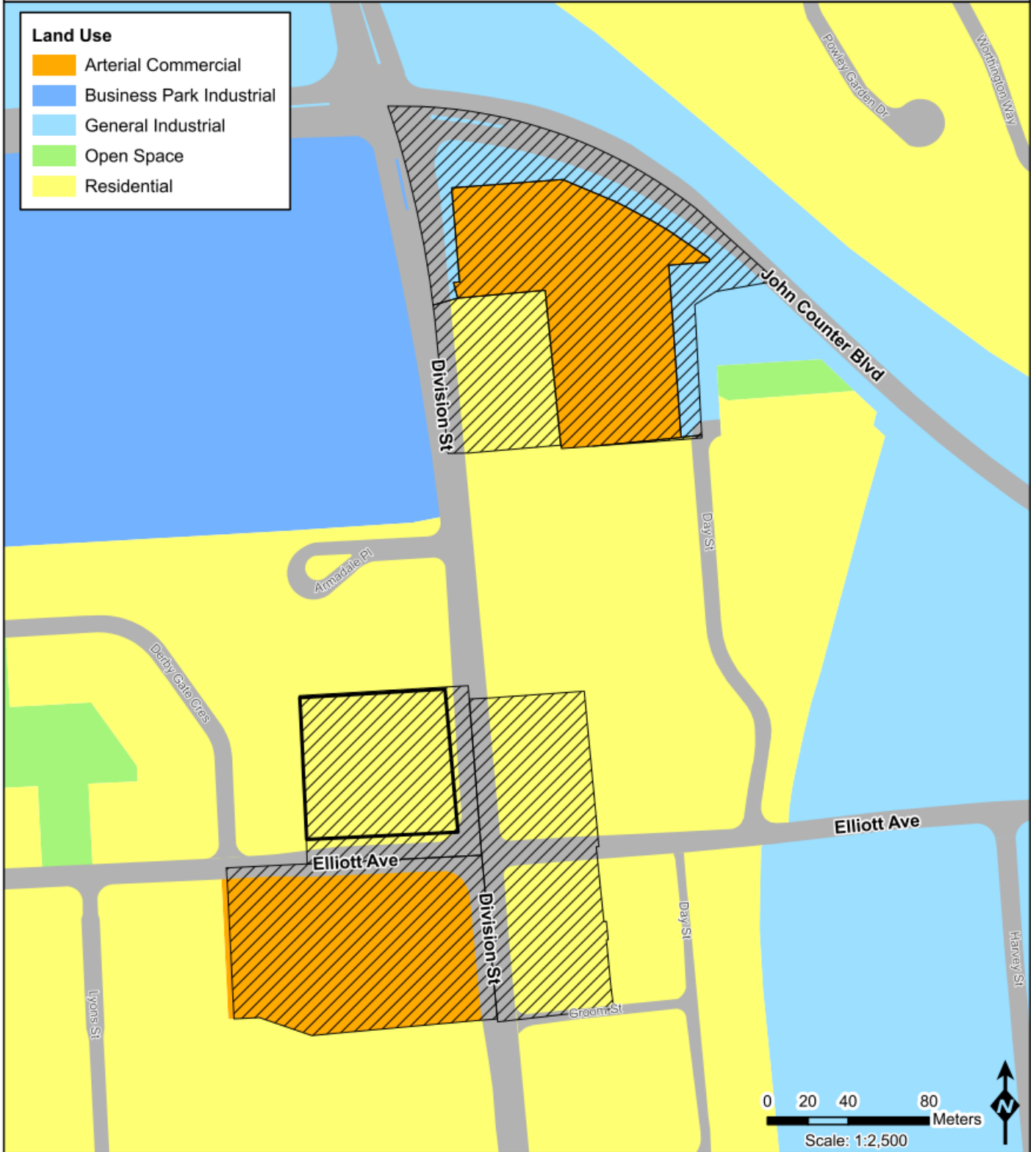
# Official Plan Existing Land Use

## Map 1a

File Number: D35-001-2025

-  Lands to be redesignated
-  North King's Town Specific Policy Area

- Land Use**
-  Arterial Commercial
  -  Business Park Industrial
  -  General Industrial
  -  Open Space
  -  Residential





Planning  
Services

# Official Plan Existing Land Use

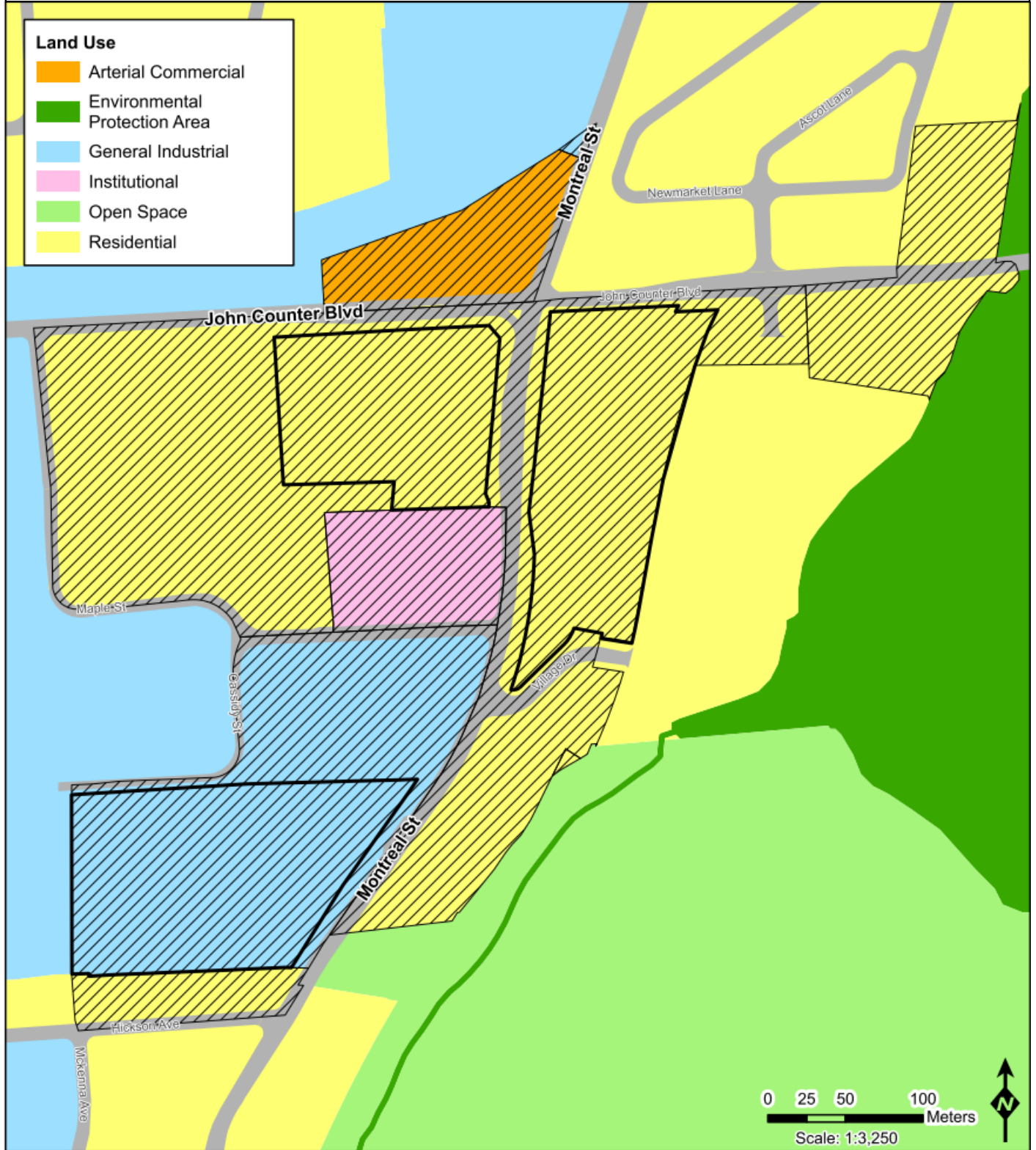
## Map 1b

File Number: D35-001-2025

- Lands to be redesignated
- North King's Town Specific Policy Area

**Land Use**

- Arterial Commercial
- Environmental Protection Area
- General Industrial
- Institutional
- Open Space
- Residential





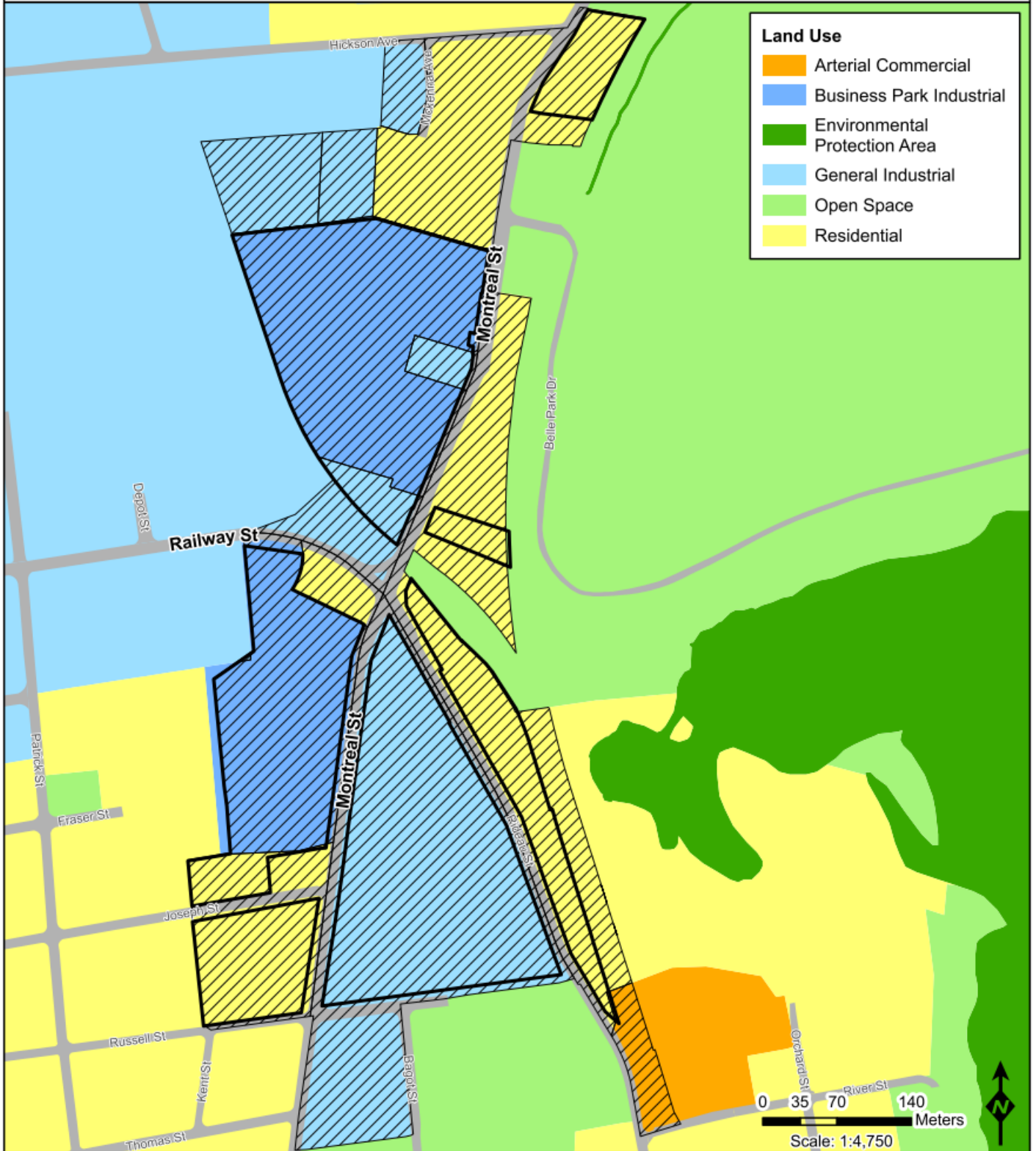
Planning  
Services

# Official Plan Existing Land Use

## Map 1c

File Number: D35-001-2025

- Lands to be redesignated
- North King's Town Specific Policy Area









# Official Plan Existing Land Use

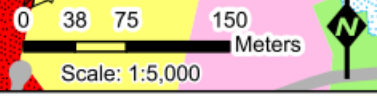
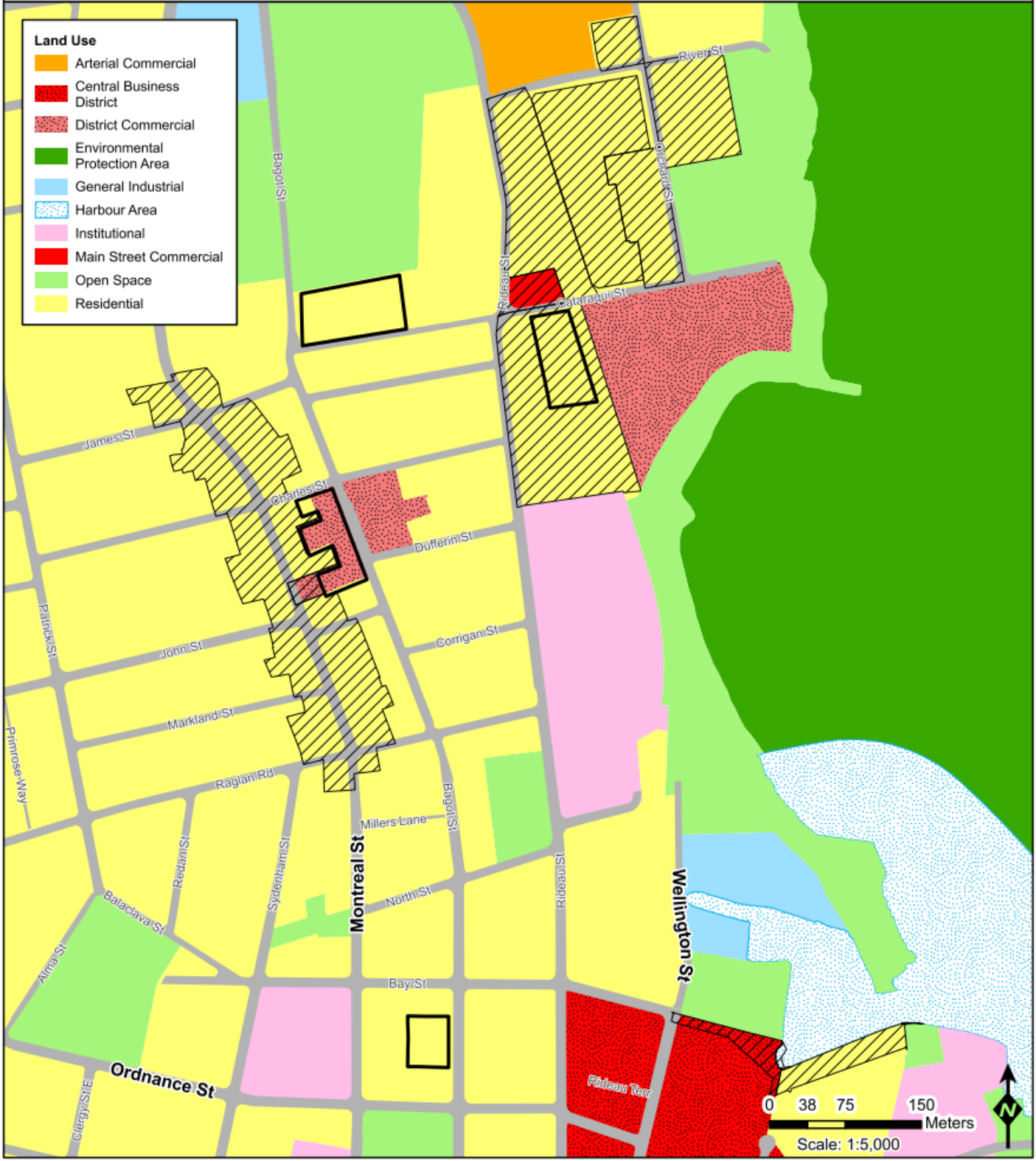
Planning  
Services

Map 1d

File Number: D35-001-2025

-  Lands to be redesignated
-  North King's Town Specific Policy Area

- Land Use**
-  Arterial Commercial
  -  Central Business District
  -  District Commercial
  -  Environmental Protection Area
  -  General Industrial
  -  Harbour Area
  -  Institutional
  -  Main Street Commercial
  -  Open Space
  -  Residential





Planning  
Services

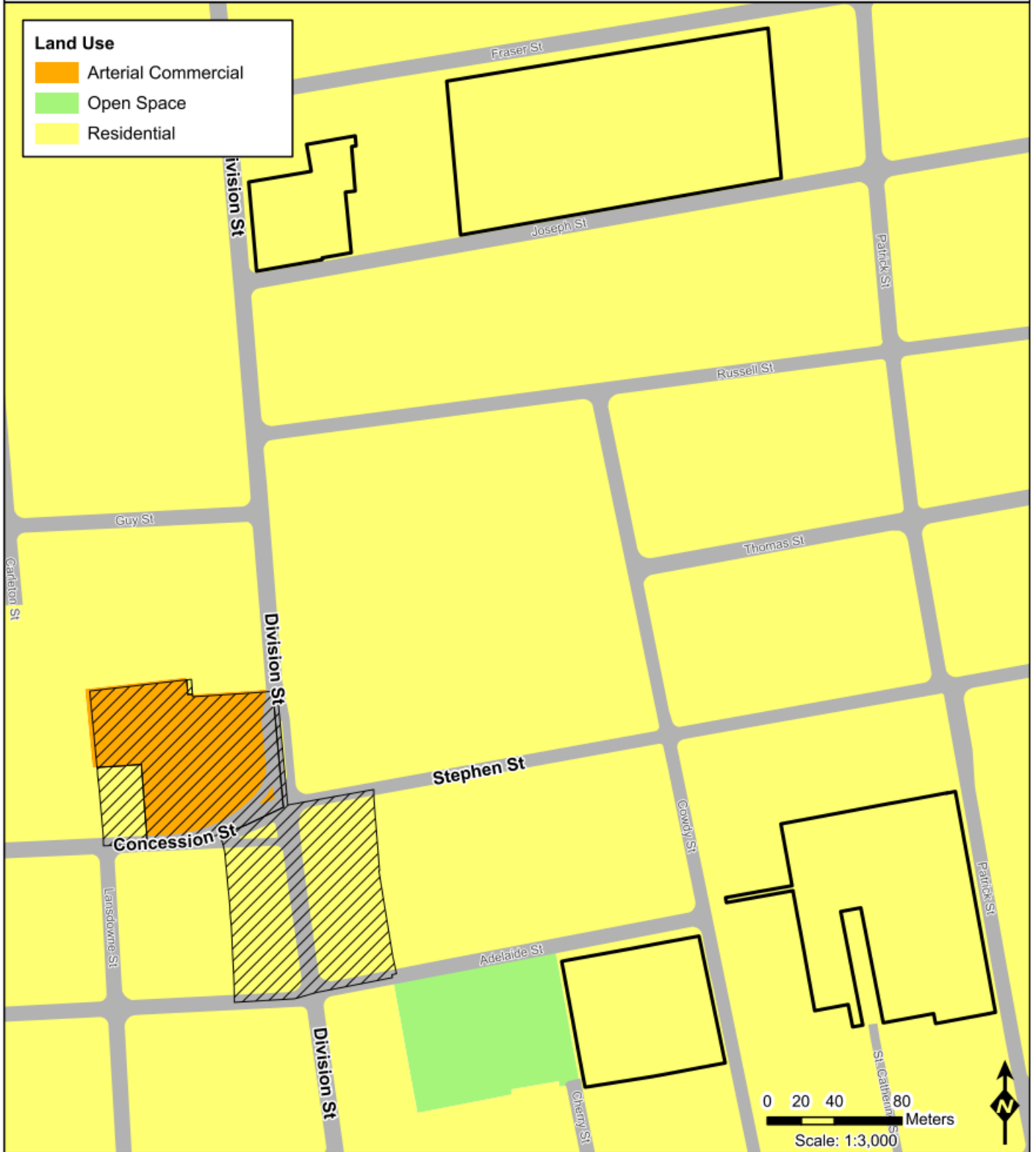
# Official Plan Existing Land Use

## Map 1e

File Number: D35-001-2025

- Lands to be redesignated
- North King's Town Specific Policy Area

- Land Use**
- Arterial Commercial
  - Open Space
  - Residential





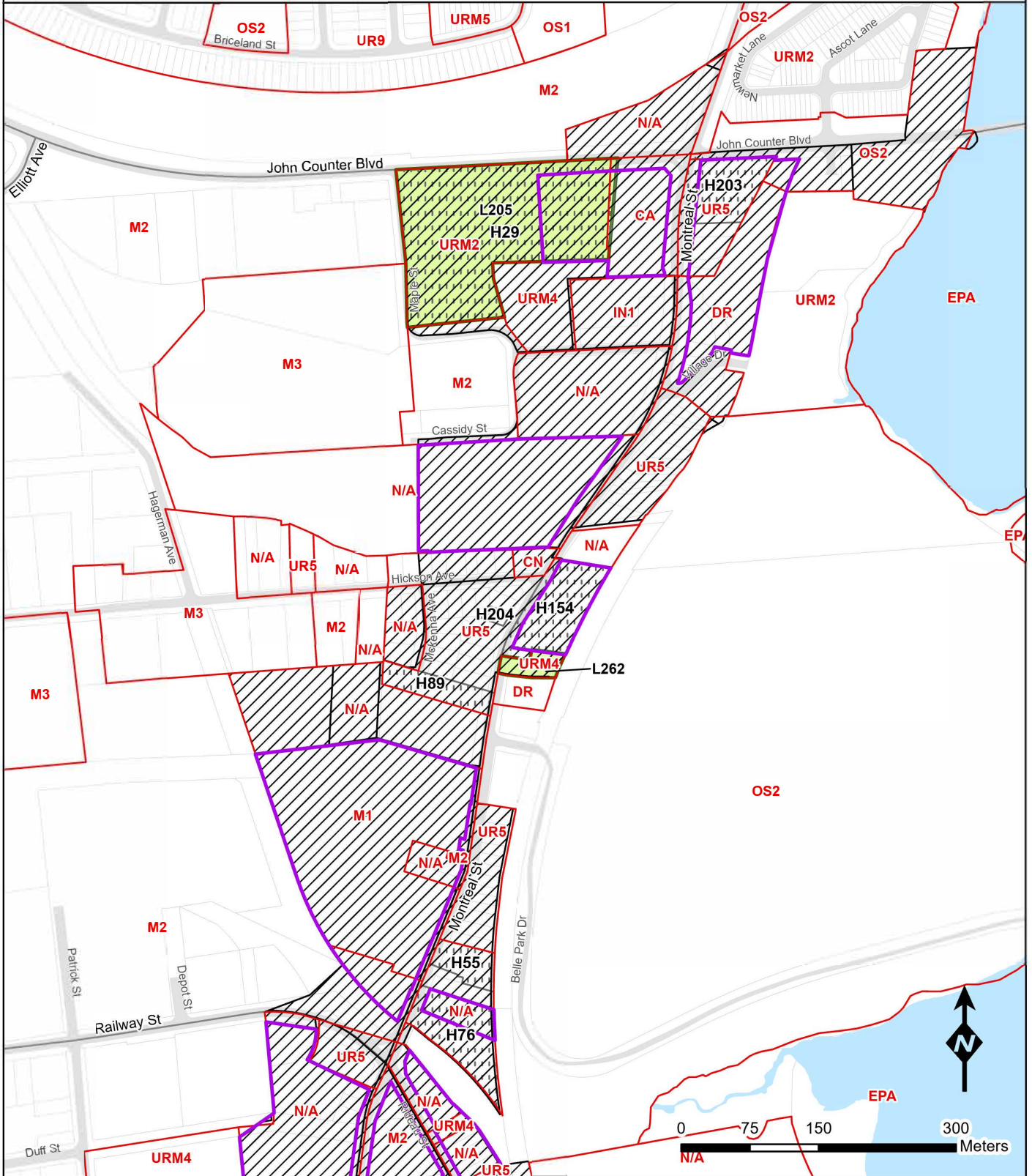
Planning  
Services

**Existing Zoning  
Kingston Zoning By-Law 2022-62  
Map 1 a**

File Number: D35-001-2025

**Legend**

-  Subject Lands
-  Proposed Intensification Areas
-  Zone
-  Holding Overlay (HXX)
-  Legacy Exceptions (LXX)
-  Exceptions (EXX)



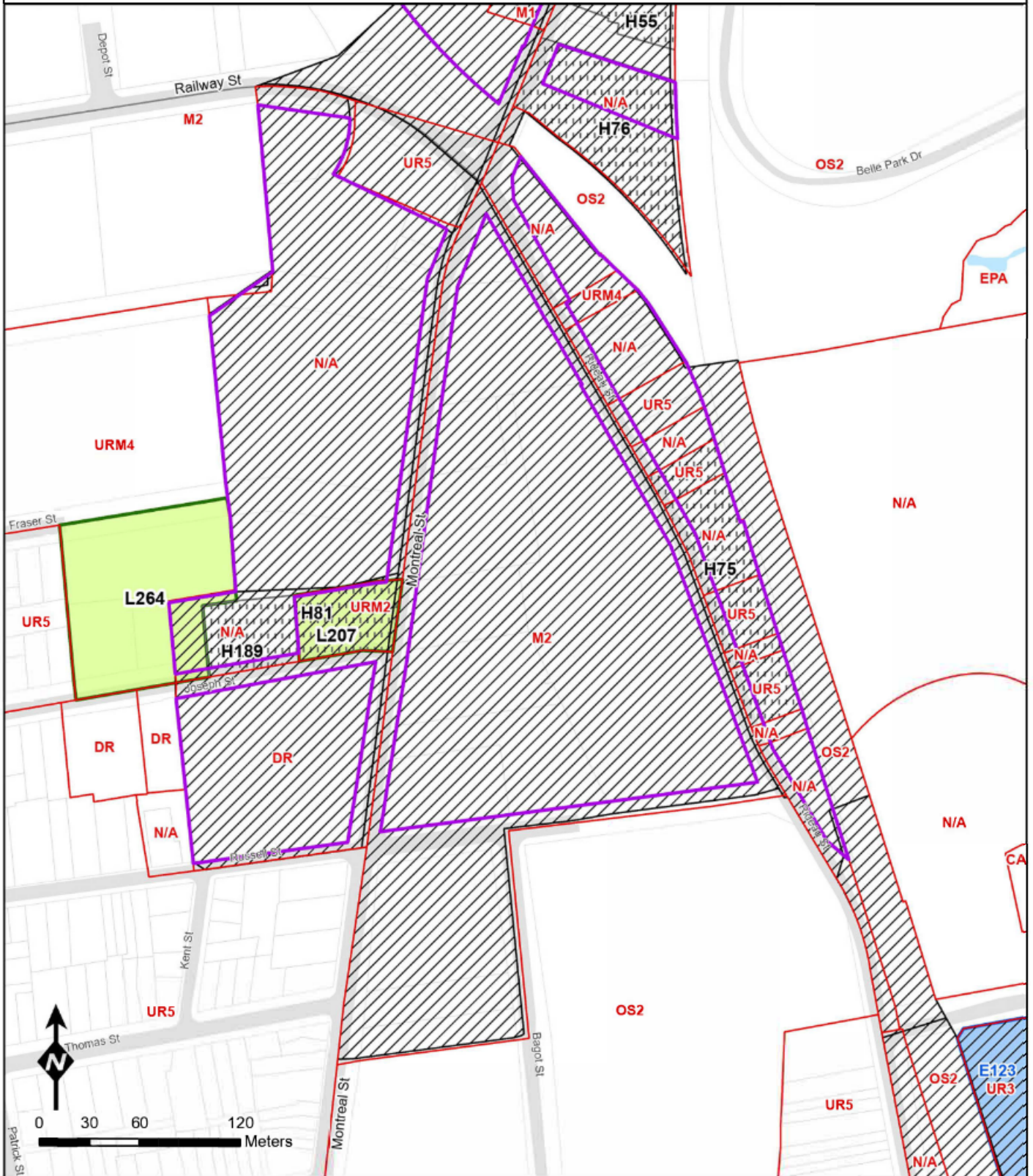


**Existing Zoning**  
**Kingston Zoning By-Law 2022-62**  
**Map 1b**

File Number: D35-001-2025

**Legend**

-  Subject Lands
-  Proposed Intensification Areas
-  Zone
-  Legacy Exceptions (LXX)
-  Exceptions (EXX)
-  Holding Overlay (HXX)





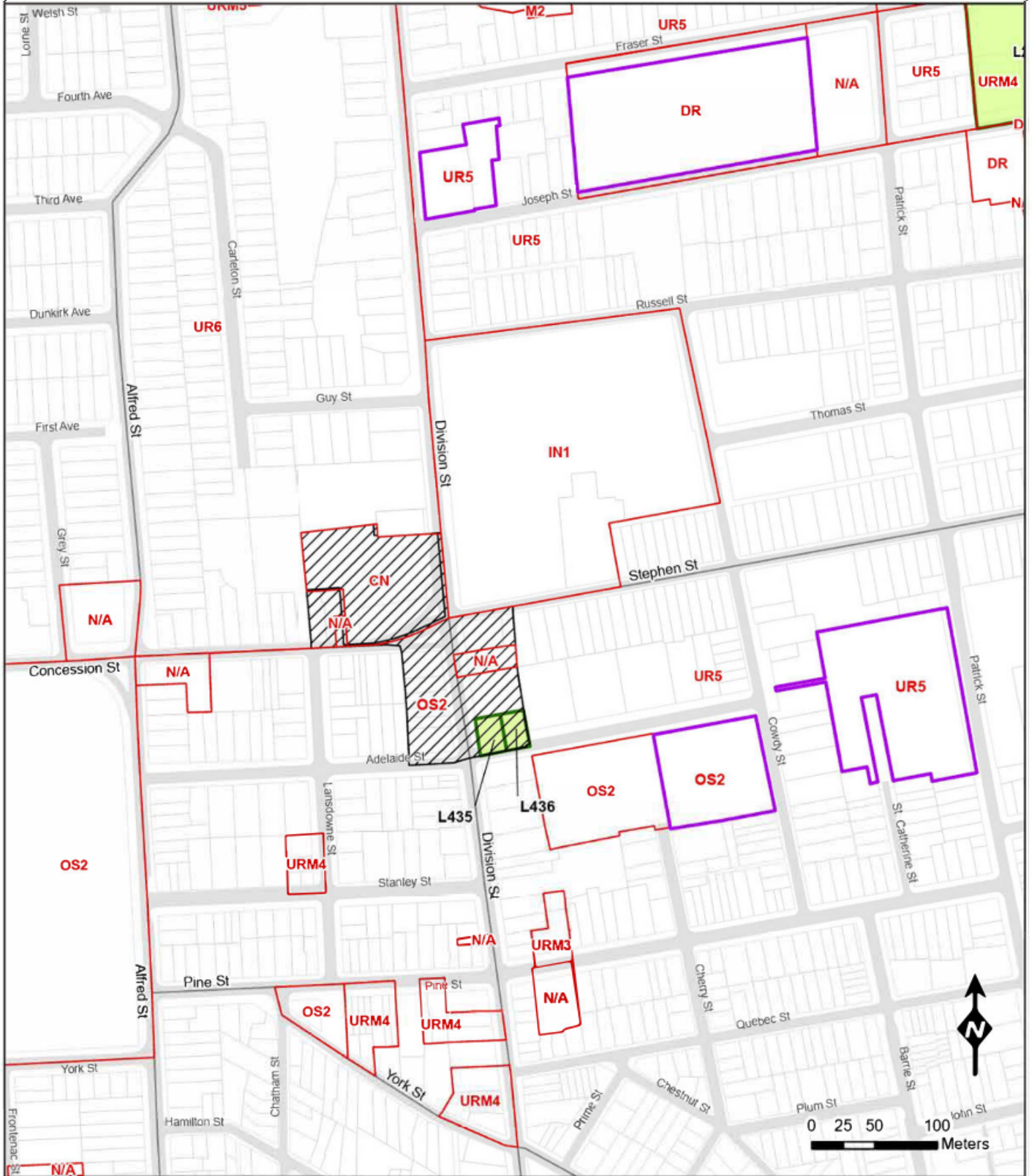


**Existing Zoning**  
**Kingston Zoning By-Law 2022-62**  
**Map 1d**

File Number: D35-001-2025

**Legend**

- Subject Lands
- Proposed Intensification Areas
- Zone
- Legacy Exceptions (LXX)
- Exceptions (EXX)
- Holding Overlay (HXX)





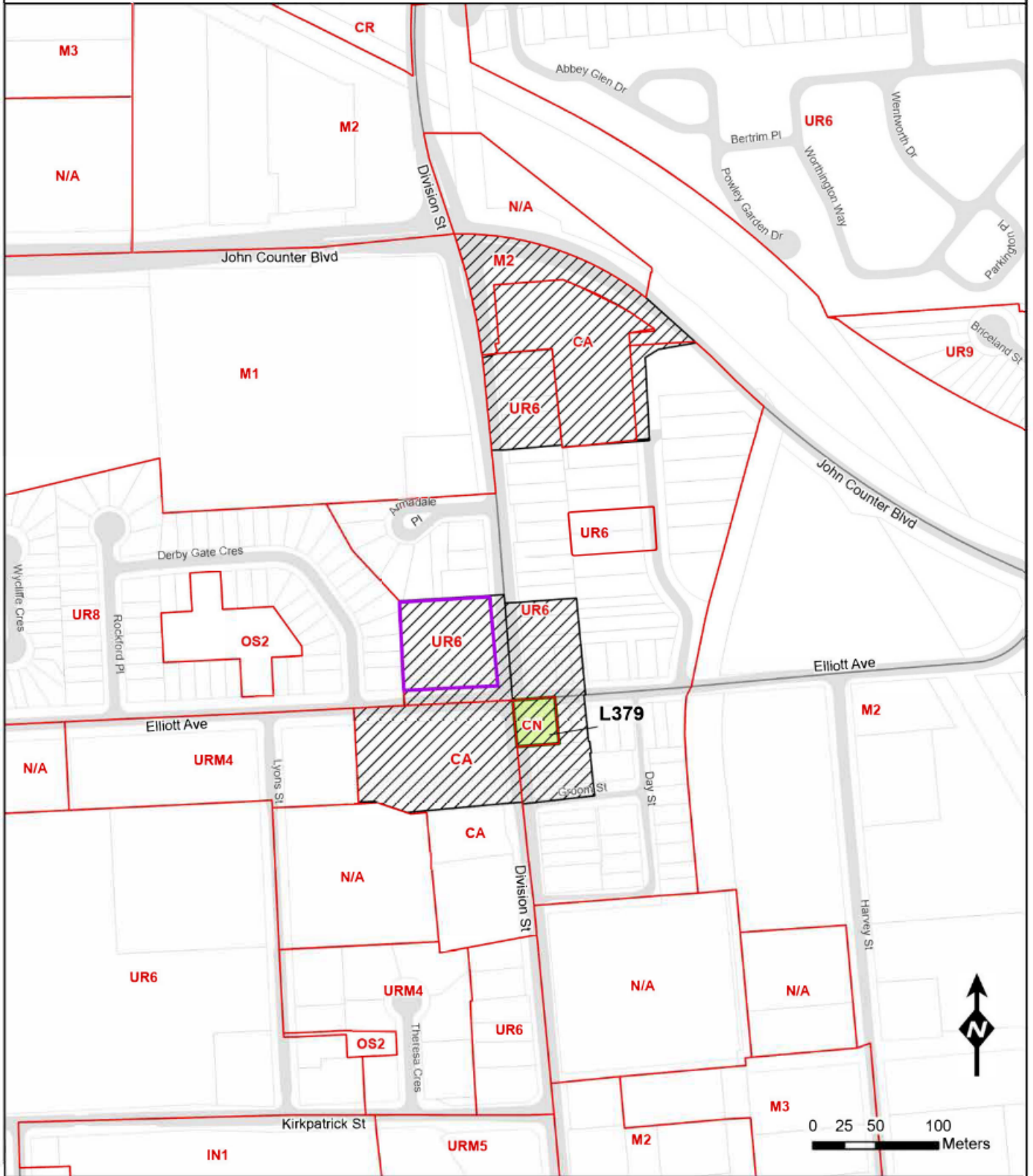
Planning  
Services

**Existing Zoning  
Kingston Zoning By-Law 2022-62  
Map 1e**

File Number: D35-001-2025

**Legend**

- Subject Lands
- Proposed Intensification Areas
- Zone
- Legacy Exceptions (LXX)
- Exceptions (EXX)
- Holding Overlay (HXX)





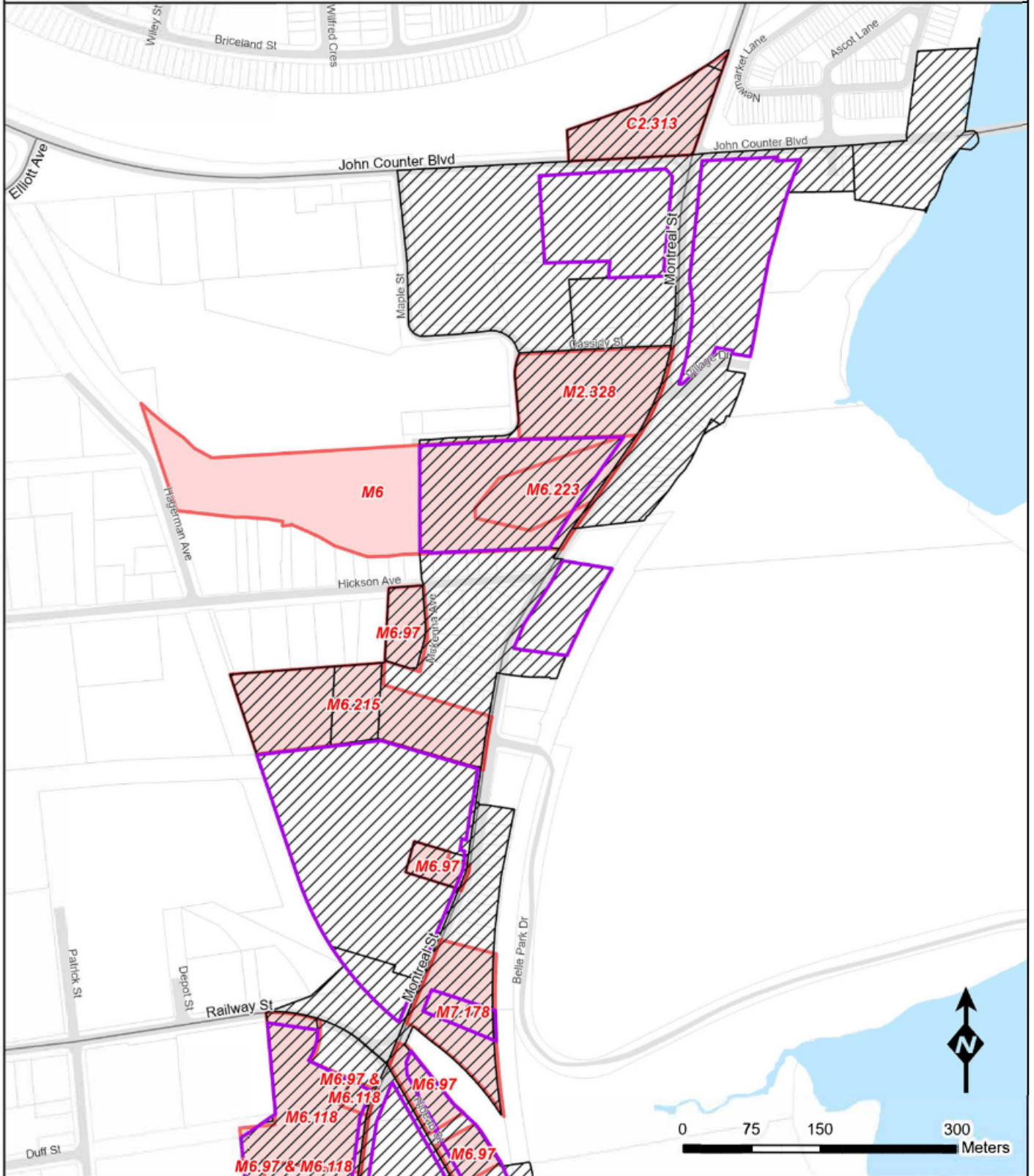
Planning  
Services

**Red Exceptions  
Kingston Zoning By-Law 8499  
Map 1a**

File Number: D35-001-2025

**Legend**

- Subject Lands
- Proposed Intensification Areas
- Red Exception Zone (Refer to Zoning By-Law 8499)









Planning  
Services


**Red Exceptions  
Former Zoning By-Law 8499  
Map 1b**

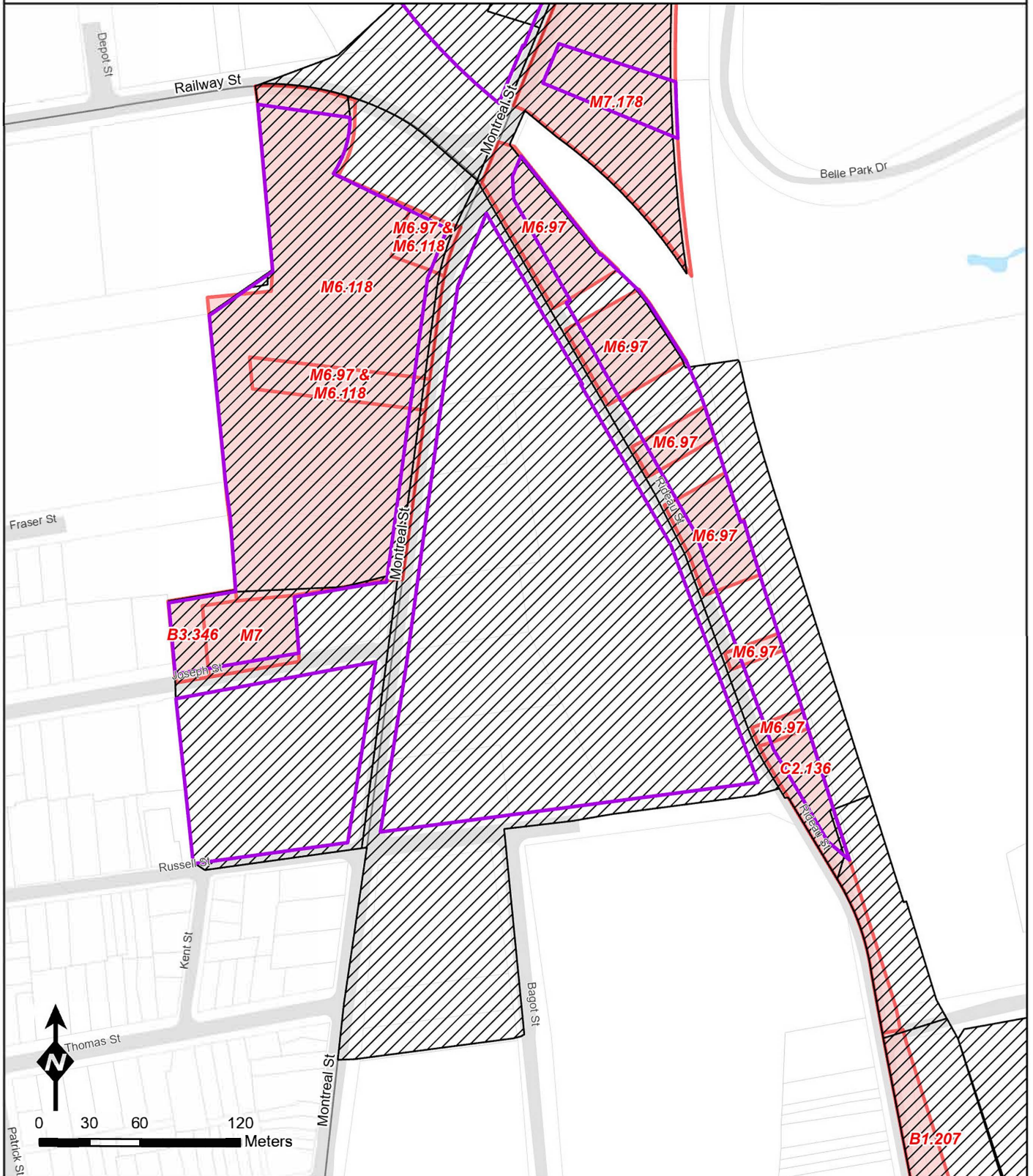
File Number: D35-001-2025

**Legend**

 Subject Lands

 Proposed Intensification Areas

 Red Exception Zone (Refer to Zoning By-Law 8499)





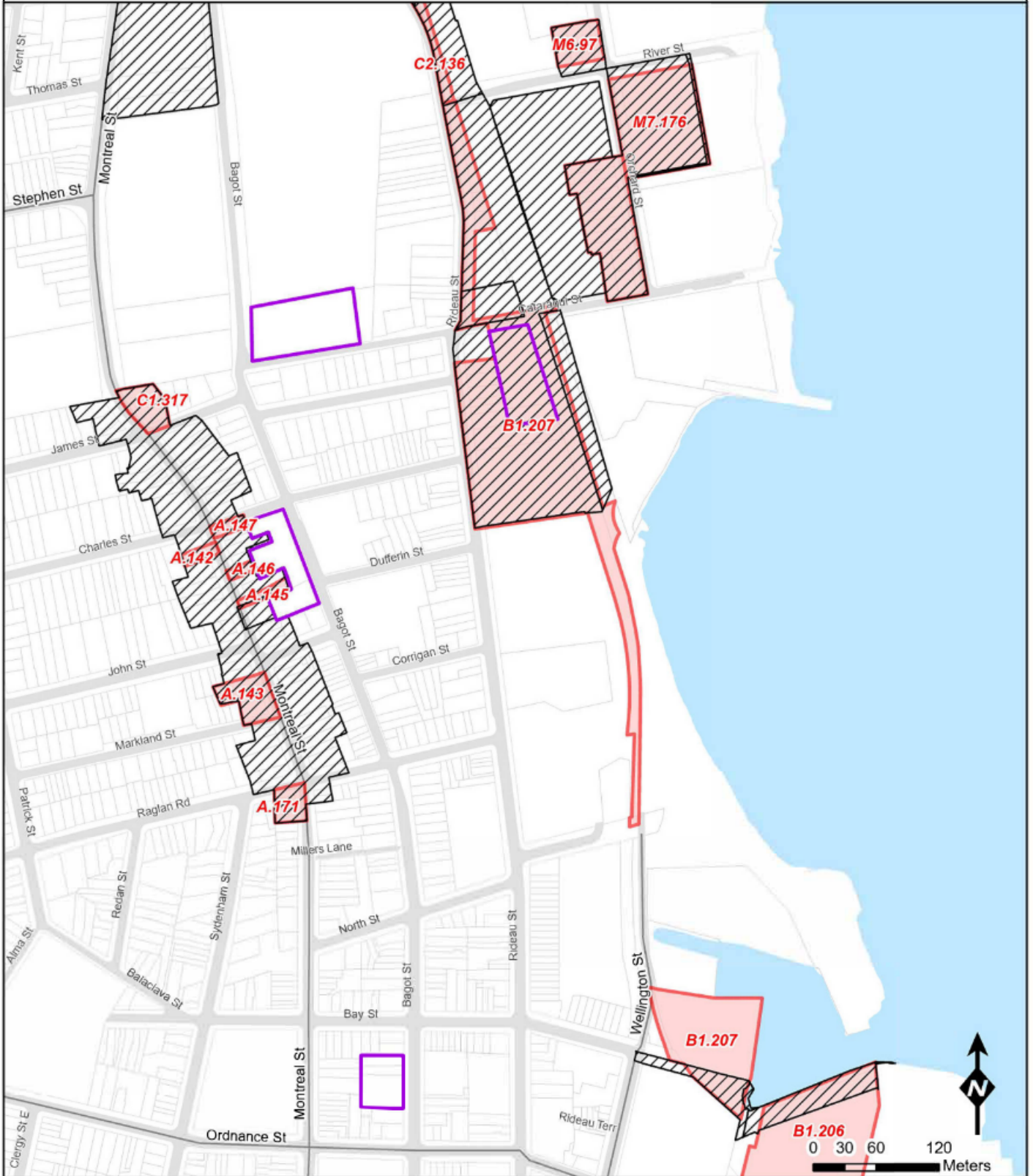
Planning  
Services

**Red Exceptions  
Kingston Zoning By-Law 8499  
Map 1c**

File Number: D35-001-2025

**Legend**

-  Subject Lands
-  Proposed Intensification Areas
-  Red Exception Zone (Refer to Zoning By-Law 8499)



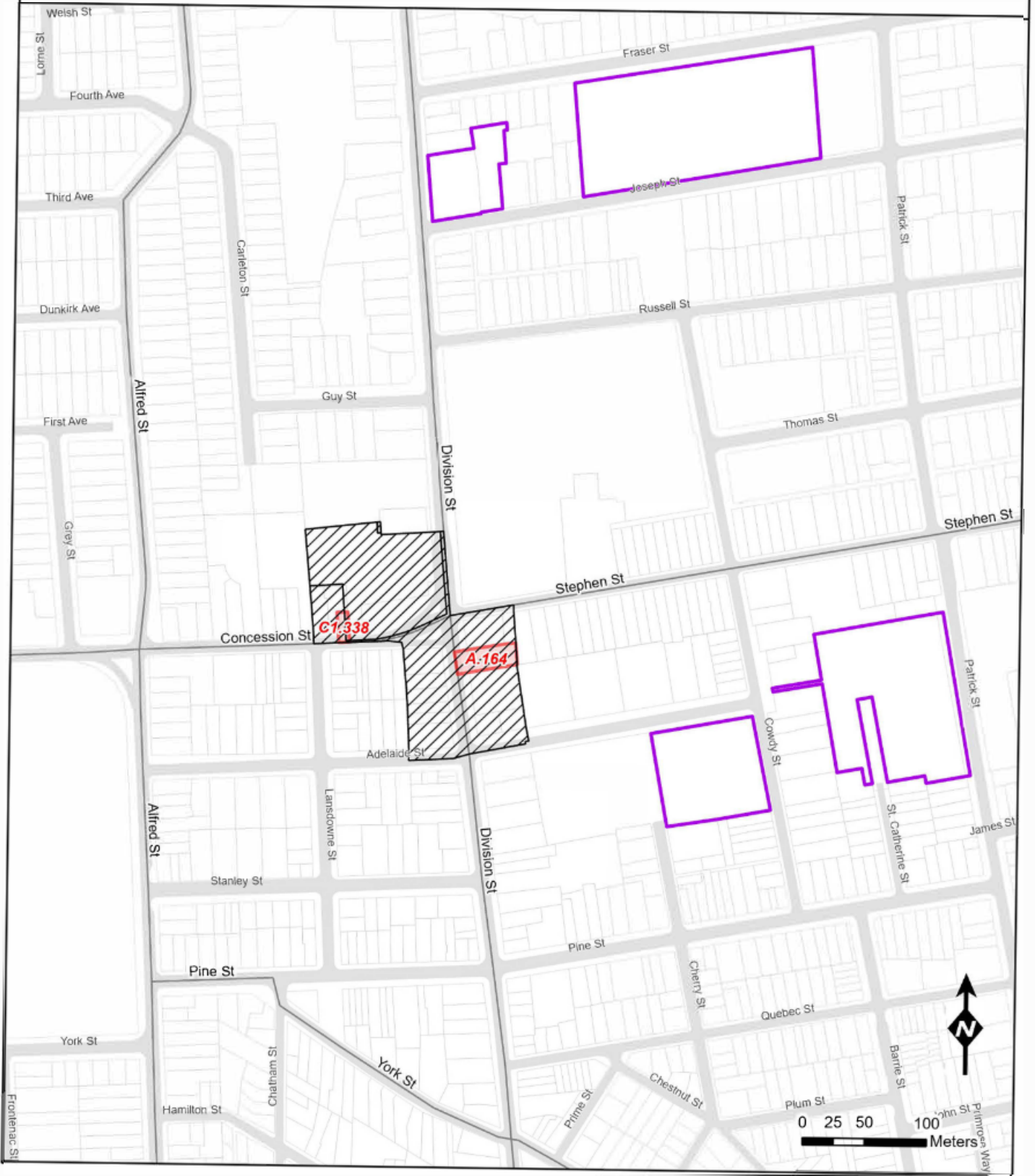


**Red Exceptions  
Kingston Zoning By-Law 8499  
Map 1d**

File Number: D35-001-2025

**Legend**

- Proposed Intensification Areas
- Red Exception Zone (Refer to Zoning By-Law 8499)
- Subject Lands



## North King's Town Specific Policy Area Proposed Official Plan Amendment

This document includes proposed amendments to the Official Plan to implement the North King's Town Specific Policy Area.

### 2.3.3 Secondary Planning Areas

Section 2.3.3 is proposed to be amended as shown in the tracked changes below:

- ~~b. the North King's Town area, which includes the Old Industrial and Inner Harbour areas (Planning Area Number 5);~~
- ~~eb.~~ the Princess Street Corridor and Centres (Planning Area Number 4); and
- ~~cd.~~ the Clogg's Road area (Planning Area Number 14).

### 3.2.29 Express Transit Areas – Policies

Section 3.2.29 is proposed to be amended as shown in the tracked changes below:

- ~~g. where a property with frontage along Montreal Street is located within the boundary of the North King's Town study area, the land use and built form must conform with the final North King's Town Secondary Plan Phase 2 recommendation report, when endorsed by Council; and,~~
- ~~hg.~~ entering into an Agreement under Section 45(9) of the *Planning Act* that secures any condition as the committee considers advisable and as are set out in the decision, including the construction of the new *development* will be completed with all occupancy permits obtained within 3 years of the date of the Committee of Adjustment's approval, or the decision will automatically lapse. Where deemed appropriate by the Committee of Adjustment, the Committee may grant the provision of an additional year in response to an application requesting an extension of the timeline.

### 3.4.C Main Street Commercial

Section 3.4.C is proposed to be amended by adding the following new policies:

#### Montreal Street Main Street

- 3.4.C.14.** The Montreal Street Main Street, generally extending from James Street to Ragland Road, has traditionally contained a mixture of residential and local small-scale commercial uses, functioning as a main street to support the surrounding neighbourhoods.

- 3.4.C.15. This area is not intended for significant infill or intensification that would substantially alter the character of the existing low-rise buildings that line the street and create a sense of enclosure. New *development* is intended to integrate into the existing built form with minimal impacts.
- 3.4.C.16. The ground floor of buildings within the Montreal Street Main Street may be used for commercial or residential uses.
- 3.4.C.17. New automotive sales and uses, gas stations, gas bars and drive-through facilities are prohibited.
- 3.4.C.18. Parking will be permitted in rear or interior yards only. New *development* will not be permitted to establish front yard parking.
- 3.4.C.19. Infill and redevelopment of existing properties will:
  - a. respect the massing, configuration and setbacks of existing surrounding properties to establish a visually continuous pedestrian scale street wall of up to 3 storeys with facades located close to the sidewalk.
  - b. maintain the appearance of narrow frontages and fine grain parcel fabric and contribute to a consistent façade rhythm along the streetscape.

### 3.4.H Mixed Use

A new section 3.4.H is proposed to be added with the following policies:

The Mixed Use designation offers *development* opportunities that combine residential, commercial or compatible light industrial uses on the same property. *Development* in these areas is intended to enhance the pedestrian streetscape, promote active transportation and contribute to complete communities by clustering a mix of uses in proximity to each other.

#### Goal:

To provide opportunities for a broad range of compatible uses in the same building or separate buildings on a site in a compact built form.

#### Policies:

##### Permitted Uses

- 3.4.H.1. The Mixed Use designation will permit a broad range of residential, commercial and light industrial uses while maintaining land use compatibility. Some uses within the Mixed Use designation may not be appropriate for all locations and as such, the zoning by-law may limit uses in certain areas to maintain land use compatibility.

- 3.4.H.2. A full range of residential housing options will be permitted, however more intensive forms of housing are encouraged, preferably in the form of mid-rise or high-rise buildings, subject to the policies of Sections 2.6 and 2.7 of this Plan.
- 3.4.H.3. Home occupations are permitted in accordance with Section 3.1.7 of this Plan.
- 3.4.H.4. A variety of commercial uses are contemplated, such as retail stores, restaurants, personal services, professional offices, creativity centres, recreational, entertainment, and other similar uses intended to provide support and services to the community. Automobile related uses, such as automobile sales, service or repair shops will be directed to alternate land use designations to foster the evolution of the pedestrian realm within the Mixed Use designation.
- 3.4.H.5. Small-scale, light industrial uses with low risk of noise impacts, heavy truck traffic and fugitive emissions, such as workshops, warehousing, and other similar uses that can operate entirely within an enclosed building, may be permitted by the zoning by-law, subject to the policies of Section 2.7. Outdoor storage of goods and materials is not permitted.
- 3.4.H.6. The zoning by-law may require commercial uses on the ground floor.
- 3.4.H.7. Where ground floor commercial uses are required, the zoning by-law may establish a minimum floor-to-ceiling height on the ground floor.

### 3.9.A.8 Inner Harbour

Section 3.9.A.8 is proposed to be deleted as shown in the tracked changes below:

~~3.9.A.8. The Kingston Inner Harbour has been the subject of much scientific study and review to determine how the area may be remediated, where warranted, and how it can be rehabilitated to a cohesive, desirable mixed use waterfront area. The area, shown schematically on Schedule 13, will be included in the planning study for the North King's Town Secondary Plan.~~

### 3.17 Site Specific Policies

Section 3.17 is proposed to be amended as shown in the tracked changes below:

#### ~~102 Fraser Street and 342 Patrick Street, Schedule 3-D, SSP Number 6~~

- 3.17.6. ~~The manufacturing plant municipally known as 102 Fraser Street and shown on Schedule 3-D as Area 6 is recognized as an established industrial operation and may be recognized as an existing industrial use in the implementing zoning by-law.~~ The facility located at the corner of Fraser Street and Patrick Street, known municipally as 342 Patrick Street, is recognized as a community facility use. ~~However, the properties are designated as Residential on Schedule 3-A.~~ It is the intent of this Plan that

should ~~one or both of these uses~~this use be discontinued or relocated, the respective property should be converted to a residential use such that it can be integrated into the surrounding residential area. The Plan also intends that further analysis be undertaken to determine an appropriate density level for such conversion to ensure *compatibility* with the adjacent structure of densities in the area.

### Inner Harbour, Schedule 3-D, SSP Number 8

**3.17.8.** ~~The Official Plan recognizes the particular status of the Inner Harbour Area as a special residential policy area which was assessed in the context of the Inner Harbour Area Study. It is the policy of this Plan to~~This Plan permits the use and *development* of lands ~~within the Inner Harbour Specific Policy Area~~ shown on Schedule 3-D as Area 8 in accordance with the Residential policies of this Plan and the following specific policies:

The maximum permitted density is fifty (50) *residential units* per net hectare. ~~However, an increase in density may be permitted pursuant to Section 37 of the Planning Act for the provision of the following:~~

- ~~a. the establishment of assisted housing for special needs groups (i.e. rent geared to income, persons with disabilities) which is set aside for management by an organization or authority approved by the City of Kingston, in which case a maximum density increase of 15% or 7.5 residential units per hectare may be granted;~~
- ~~b. dedication of parkland in excess of the required dedication, in which case a maximum density increase of 15% or 7.5 residential units per hectare may be granted; and,~~
- ~~c. dedication of lands required for the provision of a new road allowance in which case a maximum density increase of 15% or 7.5 residential units per hectare may be granted.~~

~~In no instance will the total increase in density for provision of the above-noted matters exceed 25% or 12.5 residential units per hectare. Any application for the *development* for residential purposes must be supported by a study designed in accordance with the Provincial regulations and standards to assess on-site soil contamination, and identify remedial works and clean-up procedures required to eliminate health risks to the occupants of any residential *development*.~~

Mandatory filing of a Record of Site Condition is required if the change in use of land is from industrial or commercial to a residential land use.

### 2-6 & 8 Cataraqui Street, Schedule 3-D, SSP Number 17

**3.17.17.** a. 2 – 6 Cataraqui Street

Within the District Commercial designation shown on Schedule 3-A and located at 2-6 Cataraqui Street, and shown on Schedule 3-D as part of Area 17, it is the intent of this Plan to recognize the historic character of the Woolen Mill building, its waterfront site and its unique mix of land uses. The building houses a mix of land uses ranging from artisan workshops to businesses and professional offices and a restaurant. It is the intent of this Plan to encourage the use of this old industrial building for a range of specialty type and incubator commercial, professional and business uses.

The building is historically designated, and is to be maintained as a prominent feature on the Inner Harbour waterfront. Parking for the building is to be provided on site. Where practical, residential uses clearly separated from the commercial and business uses may be permitted within the existing building.

Should the commercial, professional and business uses within the building cease to be viable, it is the intent of this Plan to support the *development* of the lands for residential purposes which are to be located within the existing building as suitably altered within its heritage designation. The public trail along the waterfront areas of the site must be protected and developed for public purposes as part of any further *development* plans.

Mandatory filing of a Record of Site Condition is required if there is a change in use of land from industrial or commercial to a residential use.

**b. 8 Cataraqui Street**

Within the District Commercial designation shown on Schedule 3-A and located at 8 Cataraqui Street, and shown on Schedule 3-D as part of Area 17 it is the intent of this Plan to recognize the importance of the *development* of this site in conjunction with its waterfront location, the historic Woolen Mill building, Molly Brant Point to the east, and the linear park system created along the Great Cataraqui River/Rideau Canal to the south and east of the property.

The Official Plan recognizes a proposal for a mixed residential/commercial building of up to six storeys on the site. The architectural and site plans for the *development* of this property must ensure:

- that the historic Woolen Mill building retains its prominence on the Inner Harbour waterfront, especially as viewed from the water side and the LaSalle Causeway;



- that architecturally any proposed building(s) are *compatible* with the existing Woolen Mill building;
- that the placement of any new buildings on the site considers ways to protect sight lines of the Great Cataraqui River/Rideau Canal; and,
- that the site design incorporates universal design and appropriate streetscaping treatment, including hard and soft landscaping elements, to enhance pedestrian and vehicular access to the City's downtown.

It is the intent of this Plan to encourage the commercial use of the site for a range of specialty type and incubator commercial, professional and business uses. Internal blocks of the building may be developed as artisan/residential lofts subject to the permitted uses and regulations of the implementing zoning by-law. Parking for the building is to be provided on site, in accordance with the regulations of the zoning by-law, and appropriately buffered and screened from the abutting streets.

The public trail along the waterfront on the east side of this site is an important feature of the *development*. Pedestrian links from the pathway must connect through the site to any ~~pedestrian systems built along Wellington Street~~ *future active transportation corridor built to the west of this site*. The public trail is subject to the Waterfront Pathway policies of Section 3.9.15 of this Plan.

Mandatory filing of a Record of Site Condition is required if there is a change in use of land from industrial or commercial to a residential use.

### **70-72 Cataraqui Street, Schedule 3-D, SSP Number 28**

- 3.17.28.** Within the District Commercial designation shown on Schedule 3-A, and located at 70-72 Cataraqui Street, shown on Schedule 3-D as Area 28, it is the intent of this Plan to allow a range of small-scale commercial uses including a veterinary clinic. It is the intent of this Plan to also support the *development* of the lands for residential purposes ~~without an amendment to this Plan, but~~ subject to a zoning by-law amendment, provided that the residential uses comply with the policies of the ~~Inner Harbour Area~~ Site Specific Policy (3.17.8), and the uses are demonstrated to be *compatible* with adjacent uses.

### **722, 730 & 766 John Counter Boulevard, Schedule 3-D, SSP Number 44**

**3.17.44.** The property located at 722, 730 and 766 John Counter Boulevard, shown on Schedule 3-D as Area 44, is within the Residential [and Mixed Use](#) designation, as shown on Schedule 3-A. It is the intent of this Plan to permit the existing limestone building, known as 730 John Counter Boulevard, to be used for a commercial school, in addition to the uses permitted within the Residential [and Mixed Use](#) designations. Use of the existing building as a commercial school is subject to the following site specific policies:

- a. operations associated with the commercial school shall not emit any noise, vibration, glare, fumes, odours, etc. which cause a nuisance or inconvenience within or outside of the premises and must be *compatible* with the surrounding residential buildings;
- b. the commercial school will generate minimal traffic; and,
- c. prior to re-use of the building from a commercial school to a residential dwelling, day care or other *sensitive* land use, the owner must file a Record of Site Condition to demonstrate that the change in use is appropriate and in accordance with provincial legislation.

## 4.6 Transportation

Section 4.6 is proposed to be amended as shown in the tracked changes below:

### Cycling Routes and Pathways

[4.6.9.1](#) [As shown on Schedule 5, the lands formerly contemplated as an extension of Wellington Street from Bay Street to Montreal Street have been re-envisioned to serve as a critical active transportation corridor to support sustainable and efficient modes of travel.](#)

[4.6.9.2](#) [In limited cases, for future development proposed on existing lots of record that directly abut the planned active transportation corridor between Bay Street and Montreal Street, the use of municipal lands for resolving access constraints may be considered at the sole discretion of the City. Development applications will be required to demonstrate that all reasonable steps to resolve access constraints have been taken on the site. The extent of this consideration by the City will generally be limited to an allowance of the minimum drive aisle width required to accommodate vehicular access and/or viable pedestrian access to the property.](#)

### Protection of Road Allowances

**4.6.17.1** Planned road allowance widths are indicated on Table 1 for new roads and for existing roads, where widening is planned. Adequate road allowances must be protected to meet future needs.

Table 1 Official Plan Road Widening			
Designated Road	From	To	Designated Width
Montreal St	<del>Railway St</del> <u>Russell St</u>	Hwy 401	30.5m
Montreal St	Stephen St	<del>Railway St</del> <u>Russell St</u>	26.2m

### Major Road Projects

**4.6.35.** The City plans the following major road projects, as referenced in the Kingston Transportation Master Plan (2015) and the 2014 Development Charges By-law, subject to any necessary *Environmental Assessment Studies*, (not listed in priority):

- a. John Counter Boulevard widening from two to four lanes from Princess Street to Sir John A. Macdonald Boulevard;
- b. John Counter Boulevard Grade Separation;
- c. Centennial Drive new construction of a four lane road from Gardiners Road (southerly) to Resource Road;
- d. Third Crossing new construction of a two lane bridge extending from John Counter Boulevard to Gore Road;
- ~~e. Wellington Street new construction as a two lane road from Bay Street to Montreal Street;~~
- ef. Division Street widening from four to six lanes from John Counter Boulevard to Highway 401;
- fg. Mid-Block Arterial new construction of a two lane road from Montreal Street to John Counter Boulevard;
- gh. Highway 15 intersection/capacity improvements from Highway 2 to Highway 401;
- hi. Gardiners Road widening from four to six lanes from Centennial Drive to North Ramp of Highway 401;

- ij. City-wide intersection/corridor improvements;
- jk. Cataraqui Woods Drive new construction of a two lane road from Sydenham Road to Centennial Drive ;
- kl. Leroy Grant Drive new construction of a two lane road from Elliott Avenue to Concession Street;
- lm. John Counter Boulevard new construction from Division Street to Third Crossing (taper from four to two lanes); and
- mn. Princess Street Traffic Operations.

**4.6.35.1** ~~The feasibility of the Wellington Street Extension, listed in Section 4.6.35 (e) and (g), will be examined through a future secondary planning process. The approximate boundaries for the Secondary Planning Area are identified on Schedule 13. The mid-block two lane road from Montreal Street to John Counter Boulevard listed in Section 4.6.35.f. will be examined in the context of city-wide transportation impacts through a future update to the Kingston Transportation Master Plan. In the interim, for enhanced access to lands located in the Old Industrial Area, a partial local road extending Hagerman Avenue to the north may be considered by the City as part of a future plan of subdivision or other development application.~~

### **7.3.D Heritage Character Areas**

Section 7.3.D is proposed to be amended as shown in the tracked changes below:

#### **St. Lawrence Ward Heritage Character Area**

**7.3.D.5.** The St. Lawrence Ward Heritage Character Area, as shown on Schedule 9, is one of the oldest areas of the City with an urban style that has survived since the 1800's. ~~It is the intent of this Plan: It is recognized that the heritage character of the area was created through the combination of buildings, street pattern, varying street widths and public spaces. It is the intent of this Plan to maintain the heritage integrity of the area with the application of the following policies:~~

- ~~a. to recognize the heritage character of the area as created through the combination of buildings, street pattern, varying street widths and public spaces; and,~~
- ~~b. to undertake further investigations that will define appropriate boundaries and conservation policies.~~
- a. The City will undertake further investigations that will define appropriate boundaries of a future heritage conservation district study area;

b. Development involving the demolition of existing buildings will be discouraged. The City may require a heritage impact assessment prior to permitting demolition, where applicable.

c. Development will be encouraged to adaptively re-use buildings in a manner that conserves the attributes that contribute to the heritage character of the area; and

d. Applications requiring Planning Act approval may be required to submit studies, such as urban design or heritage impact assessment, to demonstrate how the proposed development is compatible with the characteristics of the St. Lawrence Ward Heritage Character Area and any adjacent cultural heritage resources. The North King's Town Cultural Heritage Study should be consulted for characteristics and context.

## **10A. Downtown & Harbour Specific Policy Area**

Section 10A is proposed to be amended as shown in the tracked changes below:

### **Road Improvements**

**10A.3.2.** Planned road improvements that are within or related to the Downtown and Harbour Area include:

- a. an additional lane added to Place D'Armes to create two way traffic flow; and.
- b. providing for the potential to close Ontario Street in front of City Hall for special events; and;
- ~~c. the extension of Wellington Street, the future of which will be considered through secondary planning and a comprehensive process of public engagement.~~

## **Section 10. Specific Policy Areas and Secondary Plans**

Section 10 is proposed to be amended by adding the following new section.

### **10H. North King's Town Specific Policy Area**

The following policies apply to the North King's Town Specific Policy Area, shown on Schedule NKT-1. The North King's Town lands are generally bounded by John Counter Boulevard to the north, Division Street to the west, the Great Cataraqui River to the east, and an irregular boundary along Colborne and Bay Streets to the south. The overall intent of the North King's Town Specific Policy Area is to facilitate intensification in appropriate locations in a manner that fulfills the following vision statement:

North King's Town is at the heart of Kingston's 21st century community, building on a legacy of providing great places for people to live, work, and play, and fostering innovative growth that continues to diversify the city's economy and enhance its quality of life. It is a place for the arts and industry; a hub for recreation and community services, supporting active and accessible daily life; and home to walkable neighbourhoods, with strong connections to jobs, amenities, open spaces, the waterfront, and neighbouring communities so residents from a variety of backgrounds and income levels can grow, thrive, and age in place. North King's Town is a resilient and sustainable community that values and protects the urban wilderness adjacent to the Great Cataraqui River, and honours its rich and diverse cultural heritage, including the spiritual connections that Indigenous Peoples have with the area.

The policies in this Specific Policy Area are to be implemented in accordance with the following supporting studies, which should be referred to when assessing policy intent:

- Phase 1 – Visioning Report and Preliminary Market Analysis;
- Cultural Heritage Study;
- Mobility Plan;
- Servicing Master Plan; and
- Financial and Implementation Plan.

## 10H.1 Principles

The following principles are intended to help implement the vision statement for North King's Town:

- a. To create a welcoming and inclusive setting for people to gather, recreate, work, and live;
- b. To enhance options for movement within North King's Town, to the waterfront, and to surrounding neighbourhoods, with an emphasis on active transportation and transit;
- c. To cluster new *development* to create hubs of activity and investment, and a compact, walkable, built form;
- d. To diversify the economic and employment base and enhance customer access to businesses;
- e. To conserve natural and cultural heritage resources and enhance public access to open spaces and the waterfront;
- f. To respect Indigenous traditions and use of the land, and honour the Belle Island Accord;
- g. To support arts and cultural uses and activities;

- h. To implement sustainable and resilient plans, technologies, and design approaches;
- i. To provide a wide variety of housing options; and
- j. To identify opportunities for residential intensification, primarily through the redevelopment of larger, vacant or underutilized parcels of land.

## 10H.2 General Policies

### Intensification Areas

- 10H.2.1.** The intensification areas shown on Schedule NKT-1 generally represent larger parcels of land that are primarily vacant or underutilized and contribute to establishing a node or a corridor or otherwise represent an appropriate infill opportunity.

### Permitted Uses

- 10H.2.2.** The permitted uses are established by the applicable land use designation as shown on Schedule 3 of this Plan. Ground floor commercial uses are required for intensification areas on Montreal Street as shown on Schedule NKT-1.

### Maximum Building Heights and Transition to Adjacent Uses

- 10H.2.3.** Maximum building heights will be regulated by the zoning by-law. Minor variance applications to increase the height of mid-rise and high-rise buildings may be considered subject to conformity with the land use compatibility policies of Section 2.7 and other built form and urban design principles of this Plan.
- 10H.2.4.** The zoning by-law will include performance standards to create appropriate separation of residential uses from industrial uses and to facilitate a suitable transition between new mid-rise and high-rise buildings and existing low-rise residential uses.
- 10H.2.5.** *Development* applications will be encouraged to locate amenity area and landscape open space in yards adjacent to low-rise residential uses to provide for appropriate transition between the existing and proposed uses.

### Land Use Compatibility

- 10H.2.6.** The intensification areas shown on Schedule NKT-1 contain or are adjacent to a variety of land uses, including residential uses and existing or planned industrial uses. Future *development* of the intensification areas

is subject to the land use compatibility policies of Section 2.7 of this Plan and the following:

- a. Where minimum separation distances between sensitive uses and Class I, II, or III industrial uses recommended by the Province's D-6 Guidelines, or similar provincial guidance, cannot be achieved, reduced separation distances may be considered in the zoning by-law subject to appropriate and effective mitigation; and
- b. *Development* of certain uses may be constrained due to land use conflicts that cannot be mitigated through site and building design.

### Urban and Sustainable Design

- 10H.2.7.** *Intensification and infill development* must be appropriately designed and be sensitive to the physical character of adjacent neighbourhoods, including existing cultural heritage features, where applicable. This may be achieved through the use of stepbacks, setbacks from property lines, architectural styles, building materials, patterns of fenestration, preservation of mature vegetation, and maintaining the appearance of consistent lot frontages and streetscape rhythm.
- 10H.2.8.** The primary façade of new buildings should be oriented towards the street. Where buildings are situated on a corner of two streets, the building should be designed to address both frontages. Where a *development* involves both public and private streets, priority should be given to orienting the building toward the public street.
- 10H.2.9.** *Development* situated on a corner of two streets will be encouraged to provide outdoor amenity area adjacent to the intersection to facilitate space for tree planting and gathering spaces.
- 10H.2.10.** To improve pedestrian circulation and to increase pedestrian permeability and connectivity, mid-block connections will be utilized to limit maximum building width and length. These mid-block connections should be arranged to align with other pathways and connections to facilitate pedestrian movement and are encouraged to incorporate landscaping, amenity areas and parkettes. Generally, mid-block connections should be wide enough to provide landscaping, seating areas, street furniture, lighting, bicycle parking and other placemaking opportunities and further support active transportation. Buildings flanking mid-block connections should be designed to address the connection, including windows and entrances to promote use and surveillance of the connection.
- 10H.2.11.** *Developments* are encouraged to incorporate architectural elements, such as canopies, awnings, recessed entrances, covered walkways, trees and other similar elements to provide weather protection and optimize pedestrian comfort.



- 10H.2.12.** New *development* is encouraged to establish community gardens that provide a shared space to grow and harvest produce, flowers or native plants.
- 10H.2.13.** New *development* is encouraged to preserve existing trees by incorporating them into amenity and landscaped open space areas, wherever possible. However, it is recognized that tree removal may be required to facilitate *development*, especially where remediation is required by provincial legislation to establish sensitive land uses.
- 10H.2.14.** New *development* is encouraged to incorporate fruit trees, nut trees and other edible plantings to promote food security and facilitate opportunities for foraging. The trees should be situated appropriately to avoid creating hazards associated with fallen fruit or nuts.
- 10H.2.15.** New *development* is encouraged to establish ‘Little Forests’ within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and provide greater biodiversity.
- 10H.2.16.** Where *development* is proposed adjacent to a public park or open space, including the K&P and Waterfront Trails, the public spaces should be integrated into the design, including pedestrian connections, façade designs and landscape connections. Loading, parking and servicing areas should be screened from view with fencing and landscaping.
- 10H.2.17.** *Developments* are encouraged to incorporate sustainable features, such as solar panels or green roof components into the roof design of buildings. Hard and soft landscaping components may also be included to create high quality amenity areas that reduce the urban heat island effect and mitigate stormwater runoff.
- 10H.2.18.** Buildings and windows should be oriented and designed such that natural means of heating, cooling, ventilating, lighting interior spaces and avoiding intrusive overlook are maximized.
- 10H.2.19.** Where *development* is intended to occur in phases, the first phase should include buildings fronting onto a public street.
- 10H.2.20.** New *development* proposals may be required to submit an urban design study at the City’s discretion in accordance with Section **Error! Reference source not found.** to demonstrate that the urban design policies of this Plan have been appropriately considered, especially, but not limited to, where the proposed *development* is:
- a. adjacent to existing low-rise residential uses;
  - b. adjacent to properties that are listed or designated under the *Ontario Heritage Act*;

- c. located within St Lawrence Ward Heritage Character Area; or
- d. 4 or more storeys in height.

- 10H.2.21.** Building massing will be articulated or broken up through a continuous rhythm of building fronts achieved through a pattern of projections and recessions, entrances, display spaces, signage, and glazed areas. The intent is to create the sense of having multiple buildings along the width of the building.
- 10H.2.22.** Buildings are encouraged to provide vertical and horizontal articulation and a variety of materials to create interesting facades and forms.
- 10H.2.23.** High-rise buildings are encouraged to employ high-quality architectural design.
- 10H.2.24.** Mid-rise and high-rise buildings will be required to stepback above the fourth storey along each streetline to reduce the visual mass of the building and enhance the pedestrian realm. The zoning by-law will regulate the depth of the stepback.
- 10H.2.25.** Balconies are encouraged in new *development*, and may project from the building face above the second storey. Balconies of new *development* must not encroach into the public road allowance.
- 10H.2.26.** Where individual unit entrances are provided on the ground floor for residential uses, each unit must have an independent pedestrian access. Some entrances may be raised above the sidewalk level to provide transition from the public realm to the private realm and/or to provide private amenity space or landscaping to buffer the residential units from the public realm.
- 10H.2.27.** Building entrances should be easily accessible for pedestrians with a direct path of travel from public sidewalks and pathways.
- 10H.2.28.** Parking lay-by areas that front onto the street and cross the pedestrian realm will be discouraged.
- 10H.2.29.** The exterior design of the ground floor should reflect the intended unit sizes, including entrances, signage and glazing to provide a strong integration between the public and private realms.
- 10H.2.30.** Blank side wall conditions may be acceptable up to a height of two storeys if designed with a material finish that complements the architectural character of the main building façade. Blank walls are not permitted facing a street or public open space and are only appropriate where they exist in proximity to an existing building.

- 10H2.31. Commercial units should be accessed directly from individual entrances on the street side of the building. Where parking has been provided in the rear or side of a building, a secondary access to the commercial unit may be provided from the parking area provided the principal entrance remains along the street frontage.
- 10H2.32. Commercial units on a street corner should be designed to wrap around the corner and address both street frontages by incorporating signage, glazing, entrances, landscaping, patio spaces and other similar detail details
- 10H2.33. Advertising and associated signs related to non-residential uses must be designed and situated so as to be *compatible* with adjoining residential uses.

### Transportation

- 10H.2.34. As outlined in the North King's Town Mobility Plan, it is intended for the transportation mode share to evolve over time to support greater active transportation and transit ridership and fewer vehicle trips.
- 10H.2.35. The Intensification Areas identified on Schedule NKT-1 are located within *walking distance* of an express transit route. *Development* of Intensification Areas are encouraged to utilize the express transit service to reduce automobile usage.
- 10H.2.36. 'Active Transportation Desire Lines' are shown on Schedule NKT-1 and represent the approximate locations where the City intends to secure a multi-use pathway through future development applications within these intensification areas. These locations were identified within the North King's Town Mobility Plan as being important connections to the existing and planned active transportation network. The City will work collaboratively with the property owner to ensure the future multi-use pathway is integrated into the *development* proposal and provides necessary connections to the existing and planned active transportation network, while not unnecessarily constraining redevelopment.
- 10H.2.37. The City will secure road widenings as outlined in Section 4.6 of this Plan to accommodate the future transportation demands associated with redevelopment of the intensification areas.
- 10H.2.38. The zoning by-law will prescribe appropriate parking rates to assist in transitioning to a mode share that relies less on private automobiles and more on public transit and active transportation.
- 10H.2.39. Parking areas will be developed according to the following policies:

- a. Underground vehicular parking will be encouraged wherever feasible.
- b. Above-grade parking structures integrated into the podium of buildings are permitted provided they are located to the rear and visually screened from the pedestrian realm.
- c. Where surface parking is provided, it must be located at the rear of buildings or within an interior side yard. Where no other arrangement is feasible, surface parking may be located within front or exterior yards for accessible spaces only.
- d. Planting strips, landscaped traffic islands, and/or paving articulation should be used to define vehicle routes and smaller parking courts that provide pedestrian walkways, improve edge conditions, and minimize the negative visual impact of surface parking.
- e. Preferential parking for bicycles, energy efficient vehicles and car-share services is encouraged.

**10H.2.40.** Vehicle access points and loading and servicing areas must be appropriately located, and developed in accordance with the following policies:

- a. Wherever possible, vehicular access to on-site parking, loading, and servicing facilities are expected to be provided from side streets and rear lanes.
- b. Loading and service areas must be screened from prominent public areas and adjacent residential areas.
- c. Service and drop-off area circulation must not interfere with accessible pedestrian circulation.
- d. Garbage, loading, servicing, and utility functions shall be integrated within the interior of a building or located within the rear yard or interior side yard.

### **Parks and Open Space**

**10H.2.41.** Where *development* is proposed adjacent to a public park or open space, including the K&P and Waterfront Trails, the public spaces should be integrated into the design through pedestrian connections, façade design, tree planting and landscaping.

**10H.2.42.** As *intensification* occurs, future *developments* are encouraged to include parkettes in accordance with the following:

- a. Parkettes are intended to be small in size, to accommodate all-season uses, and to contain hardscape surfaces and elements, such as sitting areas and public art, along with adequate soft landscaping.
- b. Where publicly accessible open space is required as part of the *development* of private property, this open space will be secured through parkland dedication, donation, acquisition, or a combination of these methods.
- c. Parkette features should reinforce the street edge and the parkette should be configured to allow for the functional design and placement of public amenities, such as street trees or benches.
- d. The final decision on the design of a parkette, and the facilities or amenities to be included in a parkette, will be at the discretion of the City.

**10H.2.43.** The City will evaluate parkland dedication options through future *development* applications at 158 Patrick Street to preserve public access to the tobogganing hill in the southwestern portion of the site, adjacent to the Catherine Street road allowance.

### **Cultural Heritage**

**10H.2.44.** New *development* in the intensification areas that contain or are adjacent to built heritage resources may be required to submit a Cultural Heritage Evaluation Report to evaluate the significance of existing buildings or a Heritage Impact Statement to assess potential impacts to protected built heritage resources, as appropriate.

**10H.2.45.** *Development* applications may be required to reduce building heights and increase setbacks in order to mitigate impacts on adjacent built heritage resources.

**10H.2.46.** Redevelopment of existing buildings through adaptive reuse is encouraged.

### **Servicing and Stormwater Management**

**10H.2.47.** *Development* applications will be required to submit stormwater management plans to control the quality and quantity of stormwater to the satisfaction of the City. *Development* applications will be encouraged to incorporate innovative approaches to improve water quality, reduce runoff and promote infiltration.

## Holding Overlay

- 10H.2.48.** The intensification areas will be subject to a holding overlay:
- a. to ensure adequate servicing capacity related to water, wastewater, gas and electricity.
  - b. for the purposes of assessing and mitigating transportation impacts.

## 10H.3 Area-specific Policies

The following area-specific policies apply to the *intensification* areas as shown on Schedule NKT-1.

### Montreal Street and John Counter Boulevard

- 10H.3.1.** The following additional policies apply to new *intensification* in the Montreal Street / John Counter Boulevard Area:
- a. The intensification areas located at this intersection are intended to form a gateway into North King's Town with mid-rise and high-rise mixed use *development* framing Montreal Street. Future *developments* will be subject to the policies of Section 8.11 of this Plan and will provide enhanced public realm facilities, including wide sidewalks, street trees, street furniture and landscaping.
  - b. In recognition of the existing built form near this intersection, the zoning by-law will enable maximum floorplate sizes and maximum building heights to be consistent with the existing high-rise apartment buildings in this area.

### Outer Station Area

- 10H.3.2.** The following additional policies apply to new *development* in the Outer Station Area.
- a. The Outer Station Intensification Area is the site of Kingston's first railway station. While the conservation of this *cultural heritage resource* is of utmost importance, this Plan recognizes its redevelopment potential and that conservation efforts may depend on redevelopment. New *development* within this site will be guided by the following:
    - Any redevelopment of the original railway station building should maintain its historic form based on archival record. Alternative materials may be proposed subject to review by the City, however, if the structure is determined to be structurally

compromised, as a last resort, other construction and design options that maintain the historic form may be considered.

- The City will encourage adaptive re-use of the existing *built heritage resources* and *cultural heritage landscape* attributes in their original locations on site. In the absence of any feasible alternative to redevelopment, and instead of demolition, the City may consider relocation of *built heritage resources* or *cultural heritage landscape* attributes to locations within the site provided it has been demonstrated to the satisfaction of the City that the relationship of the station building to Montreal Street is maintained and that relocation would enhance public access to the *cultural heritage resources*. The City will not support off-site relocation.
  - The City will collaborate with the property owner to explore creative options and strategies that ensure the greatest degree of conservation of *built heritage resources* and *cultural heritage landscape* attributes.
  - Schedule NKT-1 identifies the City's desire for an active transportation link through the lands to provide an east/west connection from Montreal Street to Hagerman Street and the K&P Trail.
  - The maximum building height, setbacks and other performance standards, as appropriate, will be determined through a site-specific zoning by-law amendment, which must be supported by a Conservation Plan, Heritage Impact Assessment and urban design study. The maximum building height of these lands should not detract from the intersections of Montreal Street and John Counter Boulevard and Montreal Street, Rideau Street, and Railway Street being the main nodes of redevelopment within North King's Town.
- b.** Studies and assessments required in support of future *development* applications on the site include, but are not limited to, the following:
- Stage 1 and 2 archaeological assessments, at a minimum, to ensure that significant *archaeological resources* have been appropriately *conserved*. Although some archaeological assessment may have been completed on the property, historical mapping shows that there were many other structures on the property, including a large engine house, and any archaeological evidence of these structures should be

inventoried and evaluated for potential conservation and interpretation. The findings of the archaeological assessment should inform the Heritage Impact Assessment and its strategies for heritage conservation.

- A Conservation Plan that includes consideration of the following factors:
  - Description of the *built heritage resource* and *cultural heritage landscape* features that exist on the property and assessment of their significance and current condition;
  - Discussion of available repair and conservation methods and an analysis to identify a proposed repair and conservation approach, including long-term conservation, monitoring and maintenance measures, as appropriate.
  - Description of built form, massing, building heights, locations, setbacks, stepbacks and materiality that future *development* should utilize/consider to appropriately *conserve* the *built heritage resource* and maintain visibility from the public realm.
  - Identification of view corridors to ensure the *built heritage resources* remain visible to the public from Montreal Street.
  - A Temporary Protection Plan for the conservation of *built heritage resources* and *cultural heritage landscape* features during construction.
  - Methods of incorporating remaining elements of the former station buildings within/around new *development* while distinguishing new versus old.
- A Heritage Impact Assessment, prepared to the satisfaction of the City and consistent with municipal Provincial policies and guidelines, and based on the municipal designation by-law for the Outer Station property.

### **Montreal Street, Rideau Street and Railway Street**

**10H.3.3.** The following additional policies apply to new *infill* and *intensification* in the Montreal Street, Railway Street and Rideau Street intersection, which is intended to transition over time into the primary hub of redevelopment within North King's Town:

- a. Increased building heights and densities have been contemplated within this area to assist in redeveloping *brownfield* properties.



- b. Building heights throughout this hub of redevelopment are intended to be primarily mid-rise, generally up to six storeys in height. Building heights may transition to high-rise, generally 15 storeys in height, in proximity to the Montreal Street, Rideau Street and Railway Street intersection, where adequate separation from low-rise residential and cultural heritage resources can be achieved and where parcel sizes permit. The zoning by-law may limit building heights to low-rise, generally up to four storeys in height, along the periphery of the intensification areas to facilitate a transition to the existing low-rise residential neighbourhoods abutting the North King's Town Specific Policy Area.
- c. Where high-rise buildings are permitted by the zoning by-law, a minor variance application may be utilized to increase building height generally to a maximum of 20 storeys, provided the *development*:
- would not have a negative impact on the *cultural heritage landscape* and *built heritage resources* associated with the Rideau Canal;
  - would not generate significant additional shadowing on any surrounding residential properties;
  - Incorporates appropriate transition to any adjacent low-rise residential uses through such methods as increased yard setbacks, increased building setbacks, introduction of intervening ground-oriented dwellings or units, or other approaches supported by an urban design study;
  - can be supported by available transportation and servicing networks;
  - integrates a variety of building heights within the broader proposal to contribute to an interesting, varied and focal point skyline in the area; and
  - demonstrates a signature architectural design respecting the prominent and visible location along Montreal Street.
- d. High-rise buildings will consist of smaller floor plate towers above larger podium bases to mitigate shadow and wind impacts. The zoning by-law will establish maximum floor plate sizes and setbacks to facilitate this built form.
- e. The implementing zoning by-law may limit the number of high-rise buildings on individual sites within this intensification area to

reinforce mid-rise pedestrian scale *development* as the predominant built form.

- f. Adequate separation distances will be required to be established between sensitive uses and adjacent industrial uses.

## Proposed Mapping Changes


- **Schedule 2, City Structure Map** – Amendments to change the boundary of the Business District for mixed use areas.
- **Schedule 3-A, Land Use** – Amendments to redesignate the intensification areas and adjacent areas to a Mixed Use land use designation, redesignate properties along Montreal Street between James Street and Ragland Road to Main Street Commercial, and adjustments to increase the lands within the Open Space designation.
- **Schedule 3-D, Site Specific Policy Areas** – Amendments to the boundary of Areas 6 and 8
- **Schedule 4, Transportation** – Amendment to the road classification of Rideau Street, removal of the southern extension of Wellington Street from future road, and addition of Waaban Crossing.
- **Schedule 5, Pathways** – Addition of a new pathway/trail, and change of Waaban Crossing from future major road to major road.
- **Schedule 9, Heritage and Protected Views** – Amendment to show the new proposed boundary of the St. Lawrence Ward Heritage Character Area and changes to protected views.
- **Schedule 13, Detailed Planning Areas** – Amendment to delete North King's Town from a future secondary plan area and adding it as a new Specific Policy Area
- **Schedule NKT-1, North King's Town Specific Policy Area** – Addition of a new schedule to show the proposed intensification areas



**Schedule 'B'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 2  
City Structure**

 Lands Subject to change  
from Business District to  
Housing District

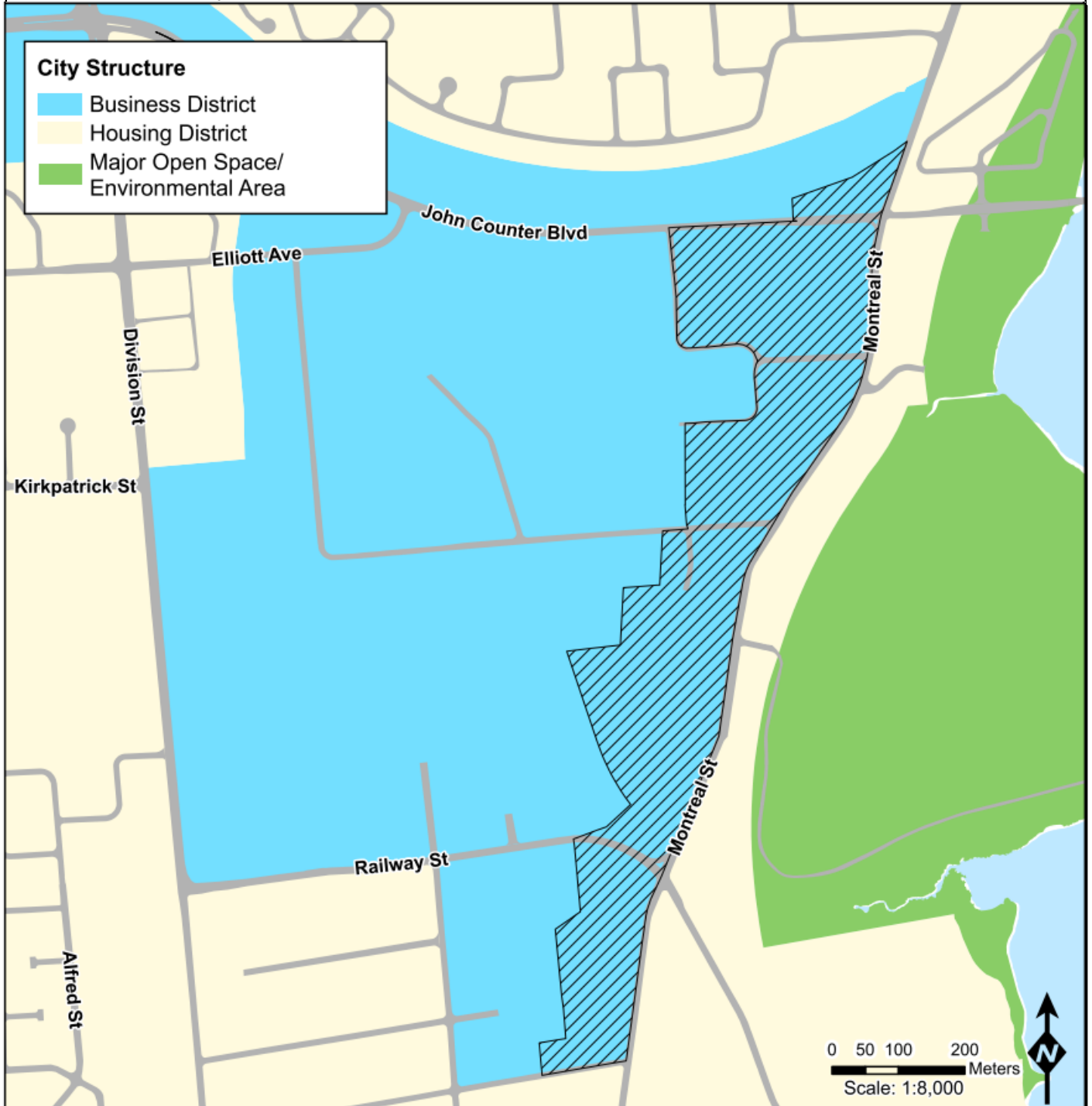
**Planning  
Services**

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This is Schedule 'B' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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Mayor

\_\_\_\_\_  
Clerk








Planning  
Services

**Schedule 'C1'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 3A  
Land Use**

Lands to be redesignated from:

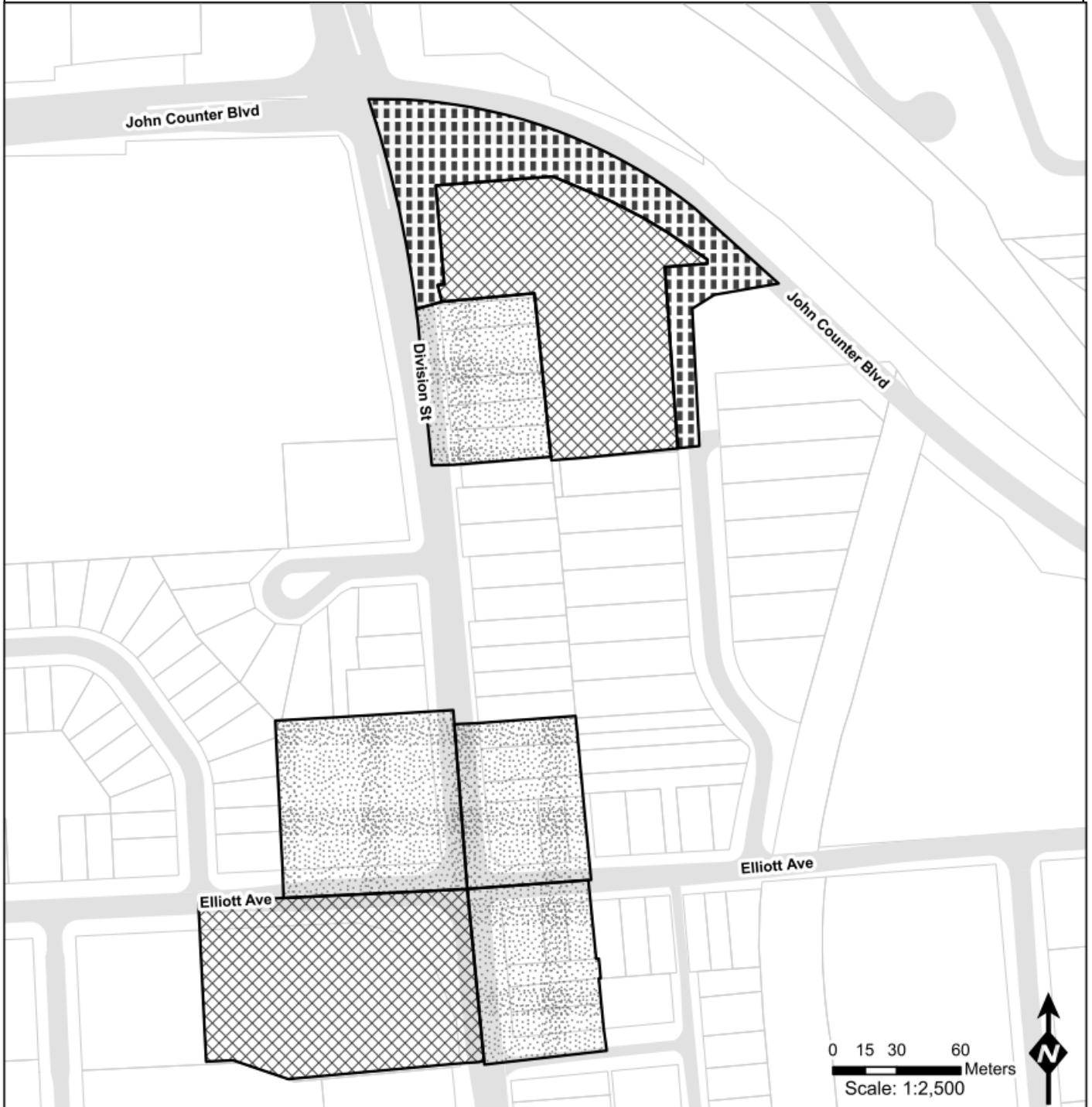
-  Arterial Commercial to Mixed Use
-  General Industrial to Mixed Use
-  Residential to Mixed Use

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**Schedule 'C2'  
to By-Law Number**

File Number: D35-001-2025






**Official Plan  
Schedule 3A  
Land Use**

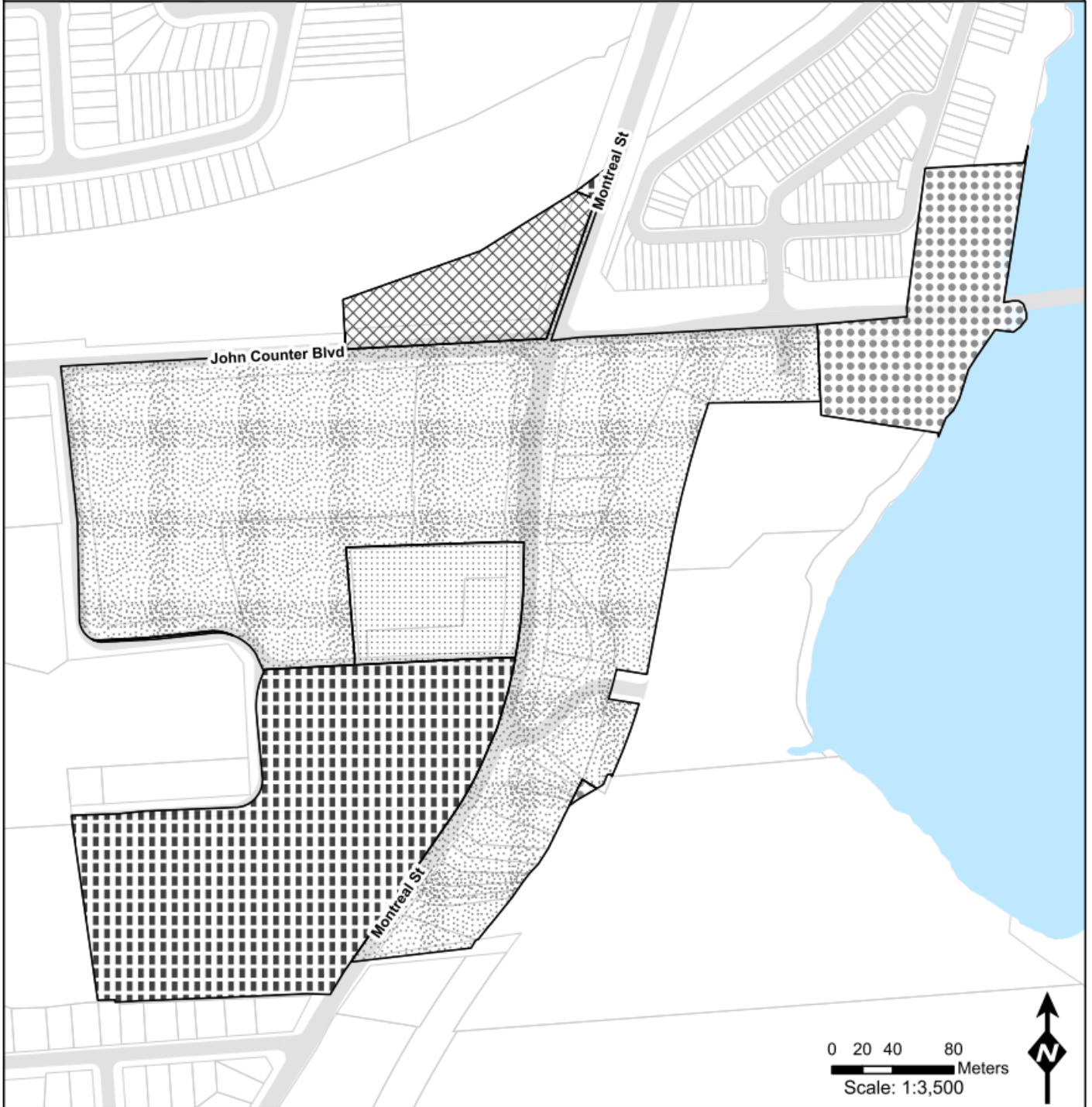
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Mayor Clerk

Lands to be redesignated from:

-  Arterial Commercial to Mixed Use
-  General Industrial to Mixed Use
-  Institutional to Mixed Use
-  Residential to Mixed Use
-  Residential to Open Space





Planning  
Services

**Schedule 'C3'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 3A  
Land Use**

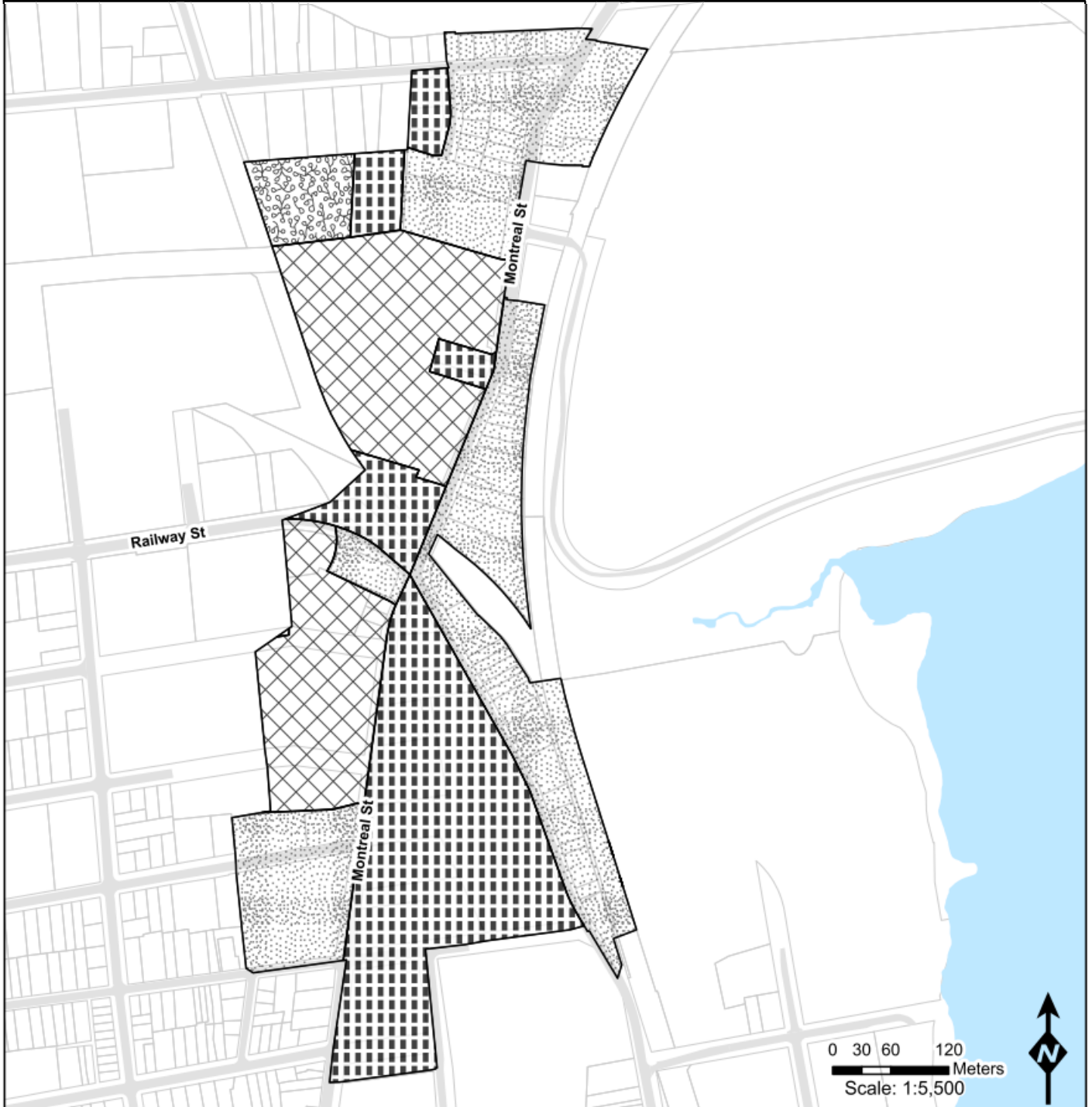
- Lands to be redesignated from:
- Business Park Industrial to Mixed Use
  - General Industrial to Mixed Use
  - General Industrial to Open Space
  - Residential to Mixed Use

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**Schedule 'C4'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 3A  
Land Use**

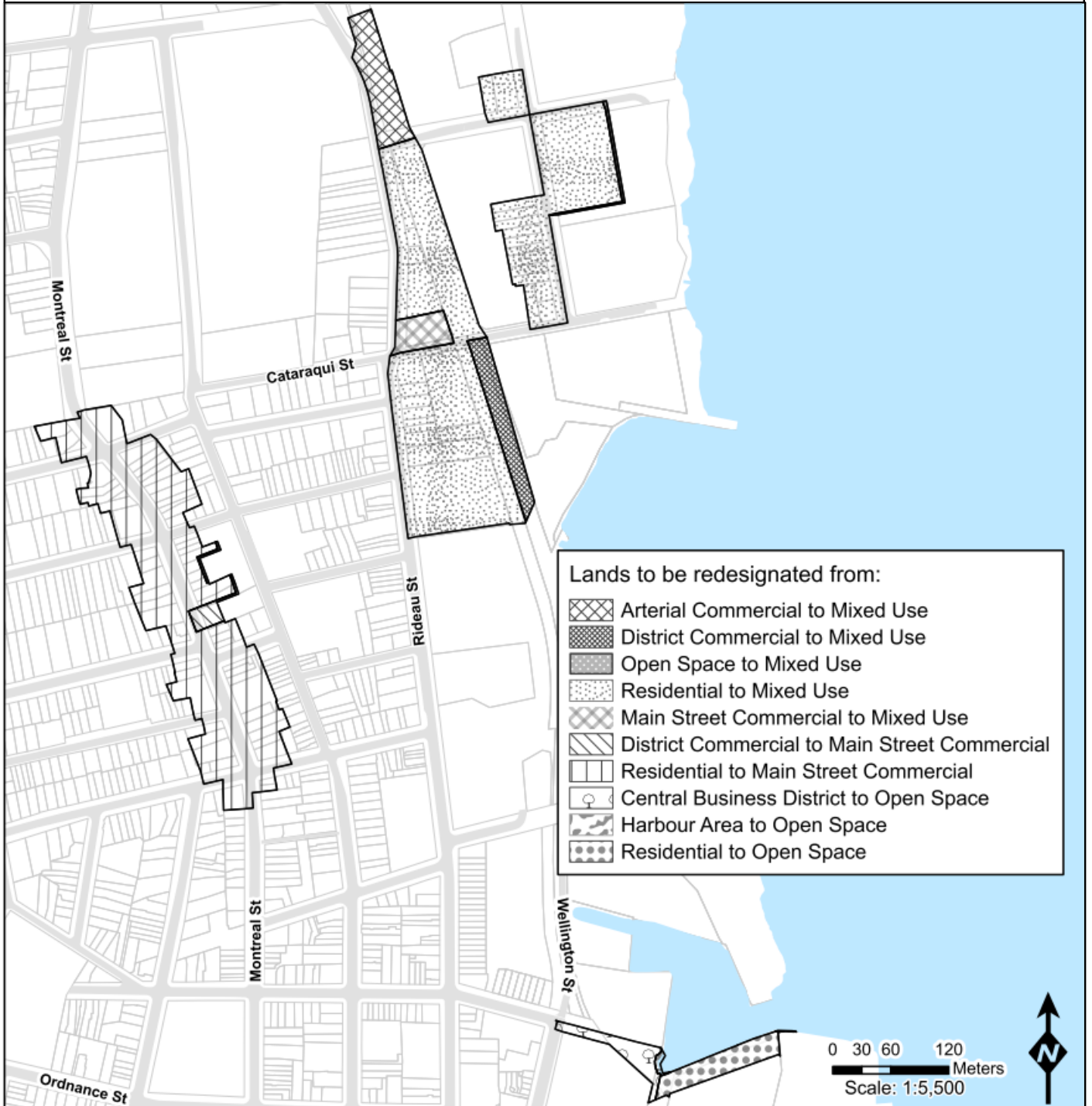
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Services**

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**Schedule 'C5'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 3A  
Land Use**

Lands to be redesignated from:

-  Arterial Commercial to Mixed Use
-  Residential to Mixed Use

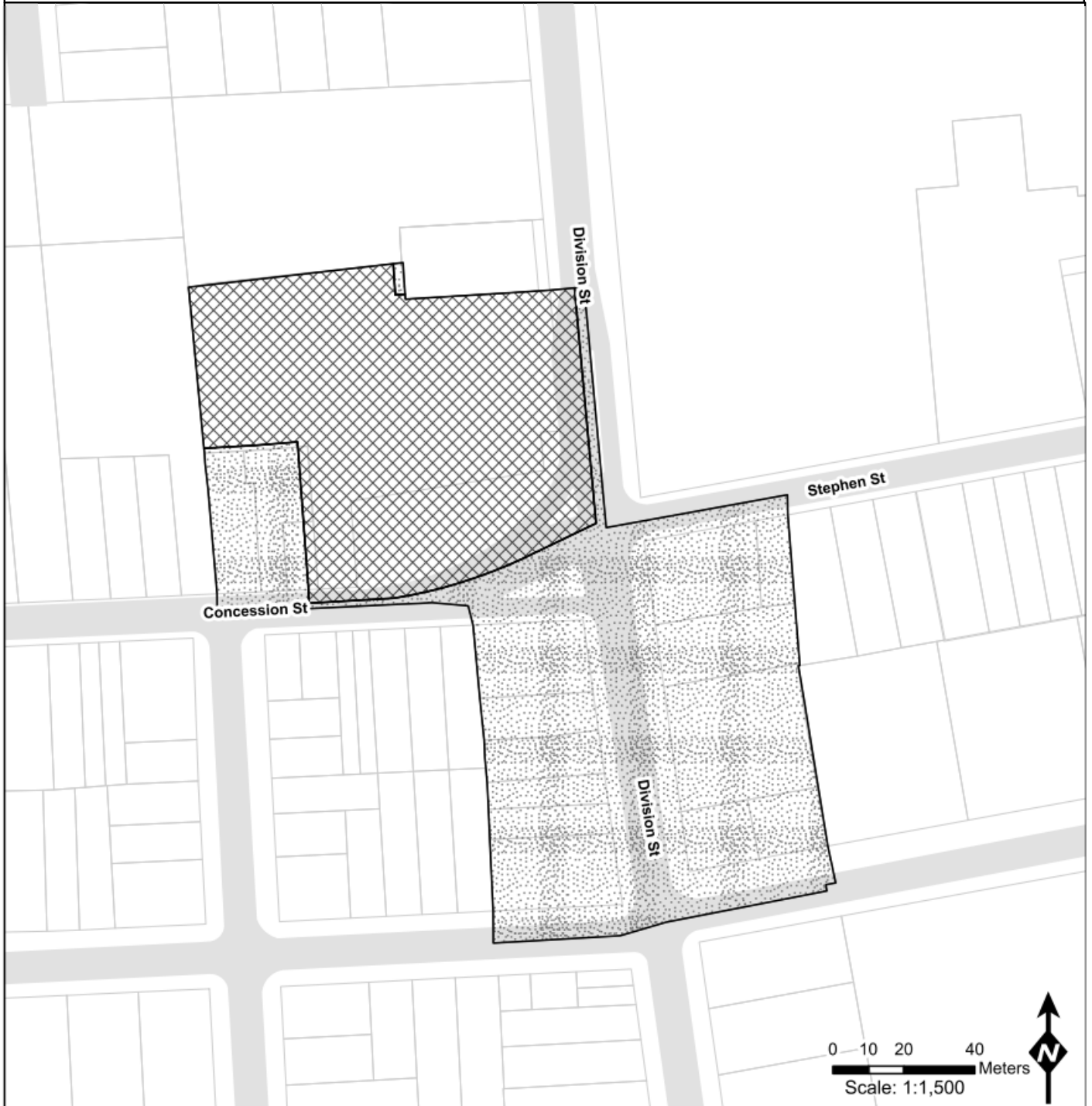
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**Schedule 'D'  
to By-Law Number**

File Number: D35-001-2025




**Official Plan  
Schedule 3D  
Site Specific Policy Areas**

**Certificate of Authentication**

This is Schedule 'D' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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-  Lands to be removed from Site Specific Policy Area
-  New Boundary of Site Specific Policy Area 6
-  New Boundary of Site Specific Policy Area 8





**Schedule 'E'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 4  
Transportation**

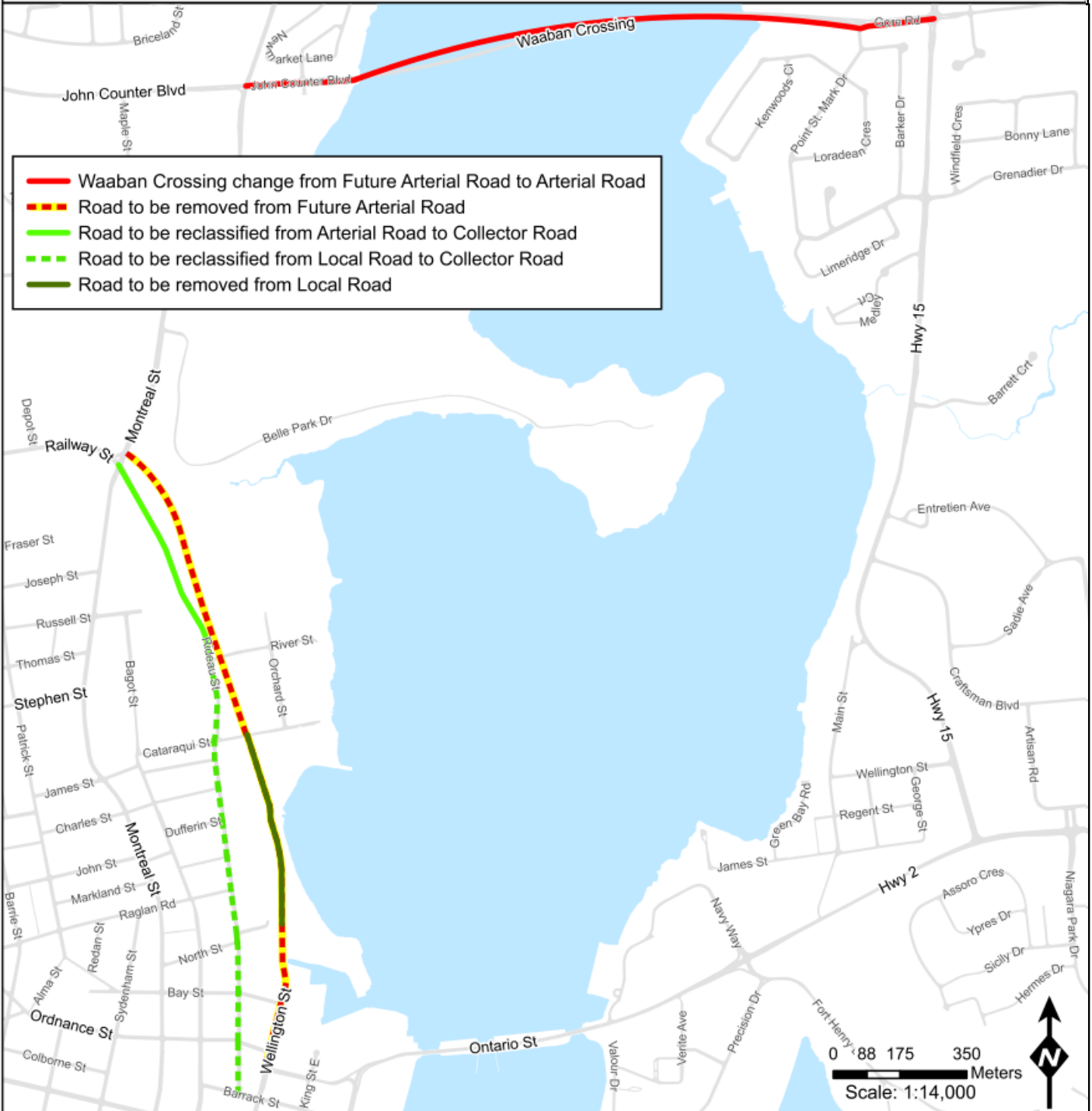
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**Schedule 'F'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 5  
Pathways**

Change from Future Major Road to Major Road

Proposed Pathway or Trail

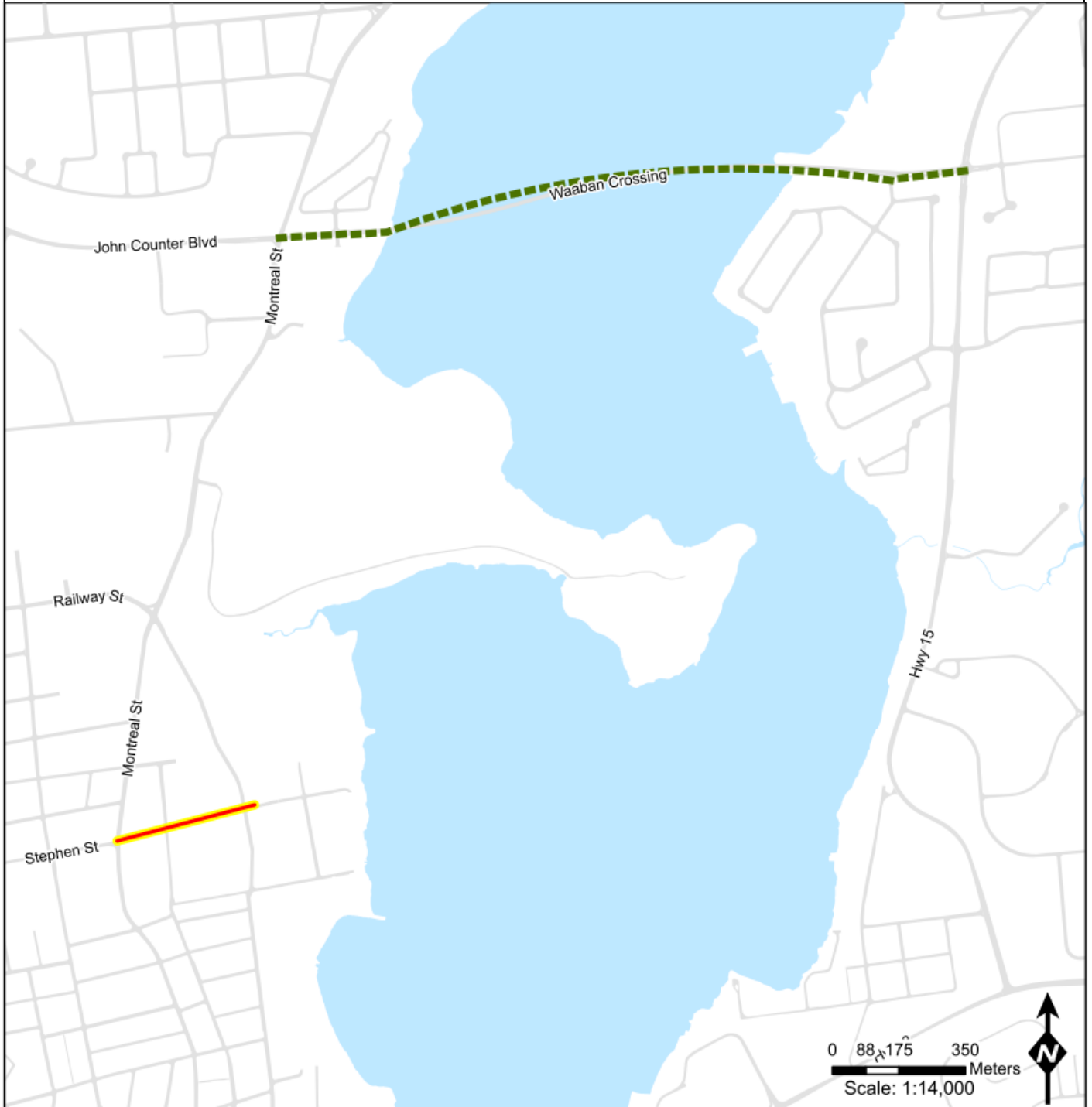
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**Schedule 'G'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule 9  
Heritage and Protected Views**

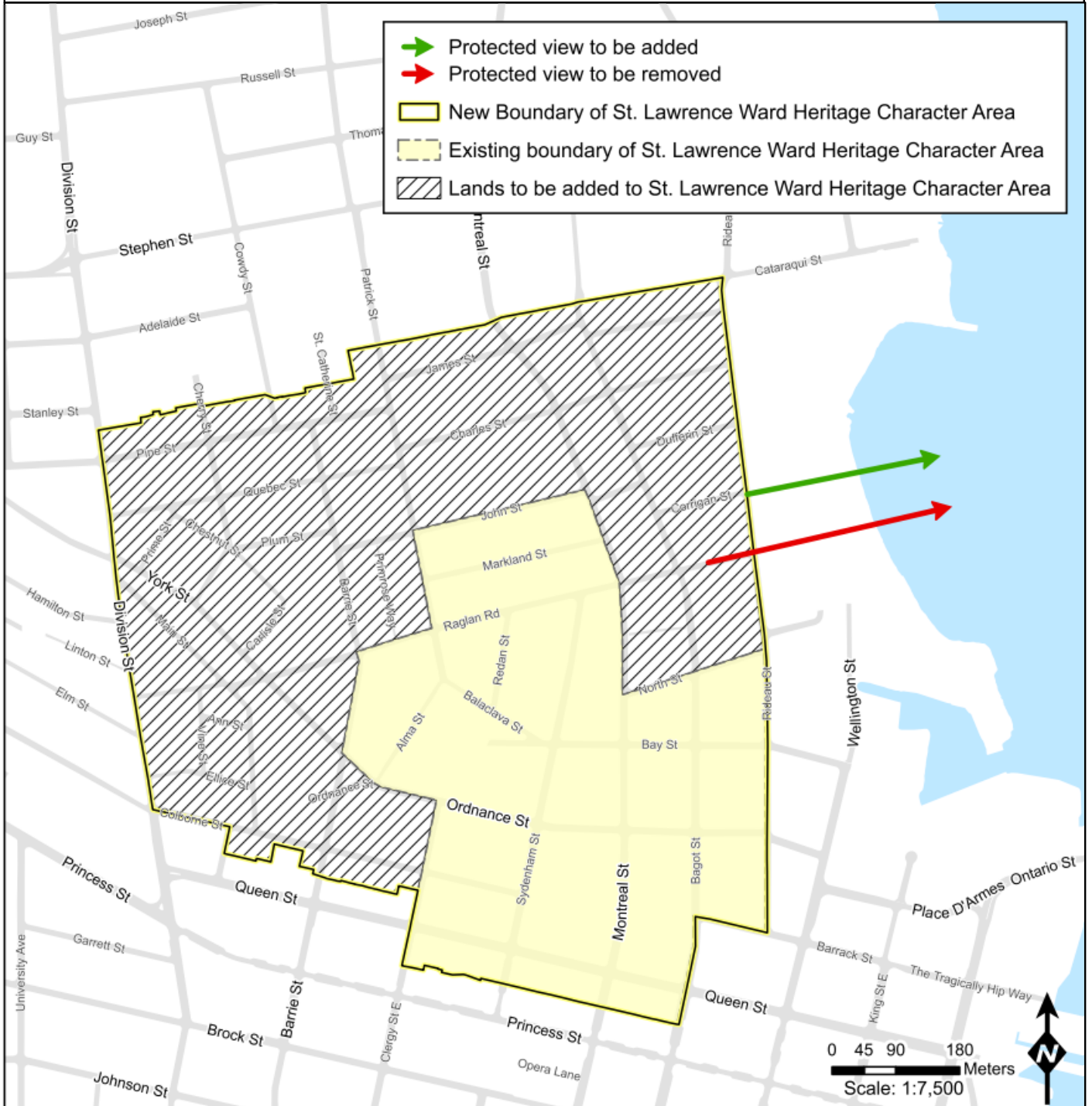
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# Schedule 'H' to By-Law Number

File Number: D35-001-2025

Planning  
Services

# Official Plan Schedule 13, Detailed Planning Areas

Amend by deleting Future Secondary Plan Study Area 5 (North King's Town) and replacing it with a new Specific Policy Area 5:

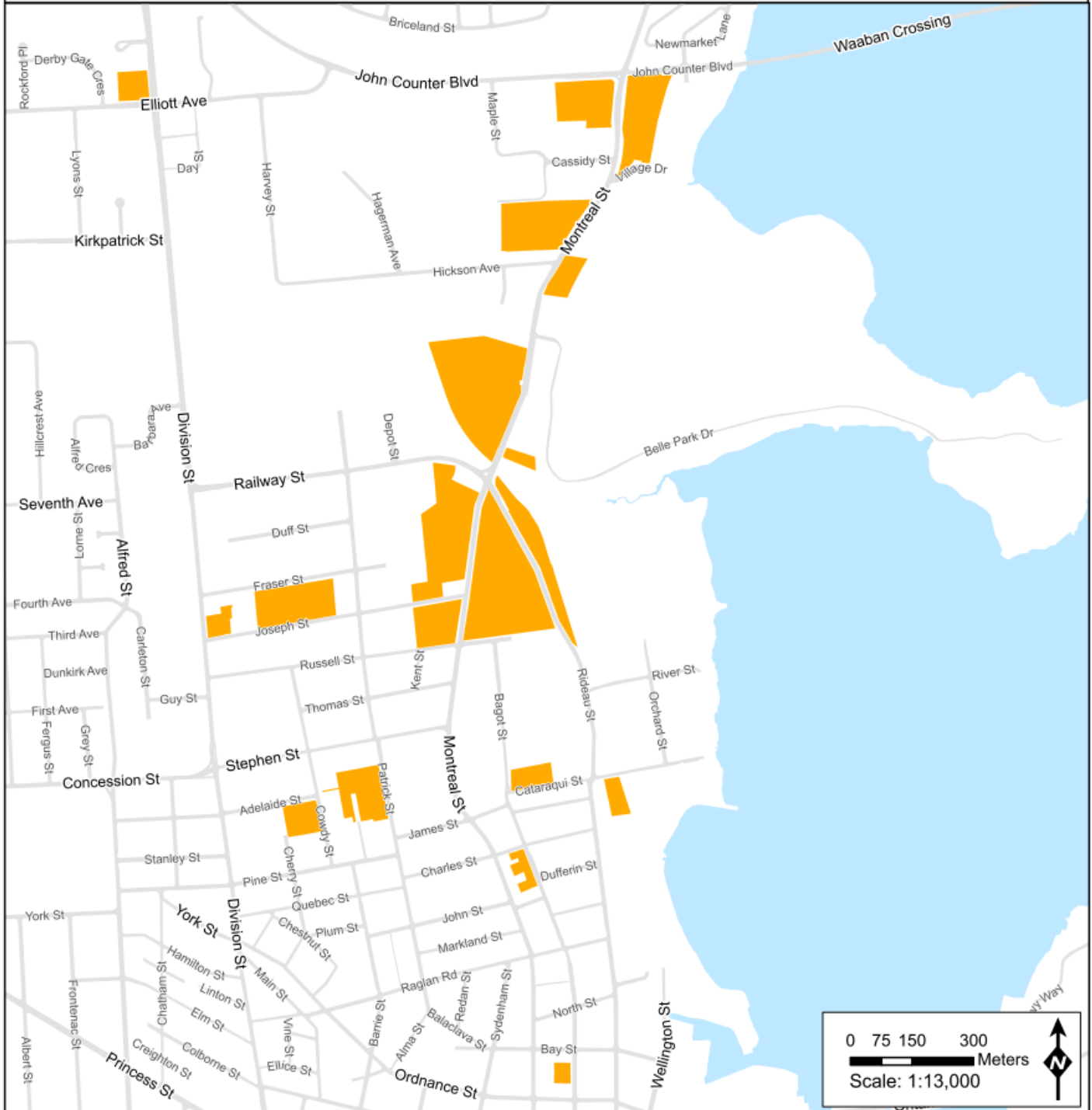
 North King's Town Specific Policy Area

## Certificate of Authentication

This is Schedule 'H' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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**Schedule 'I'  
to By-Law Number**

File Number: D35-001-2025

**Official Plan  
Schedule NKT-1  
North King's Town Specific Policy Area**

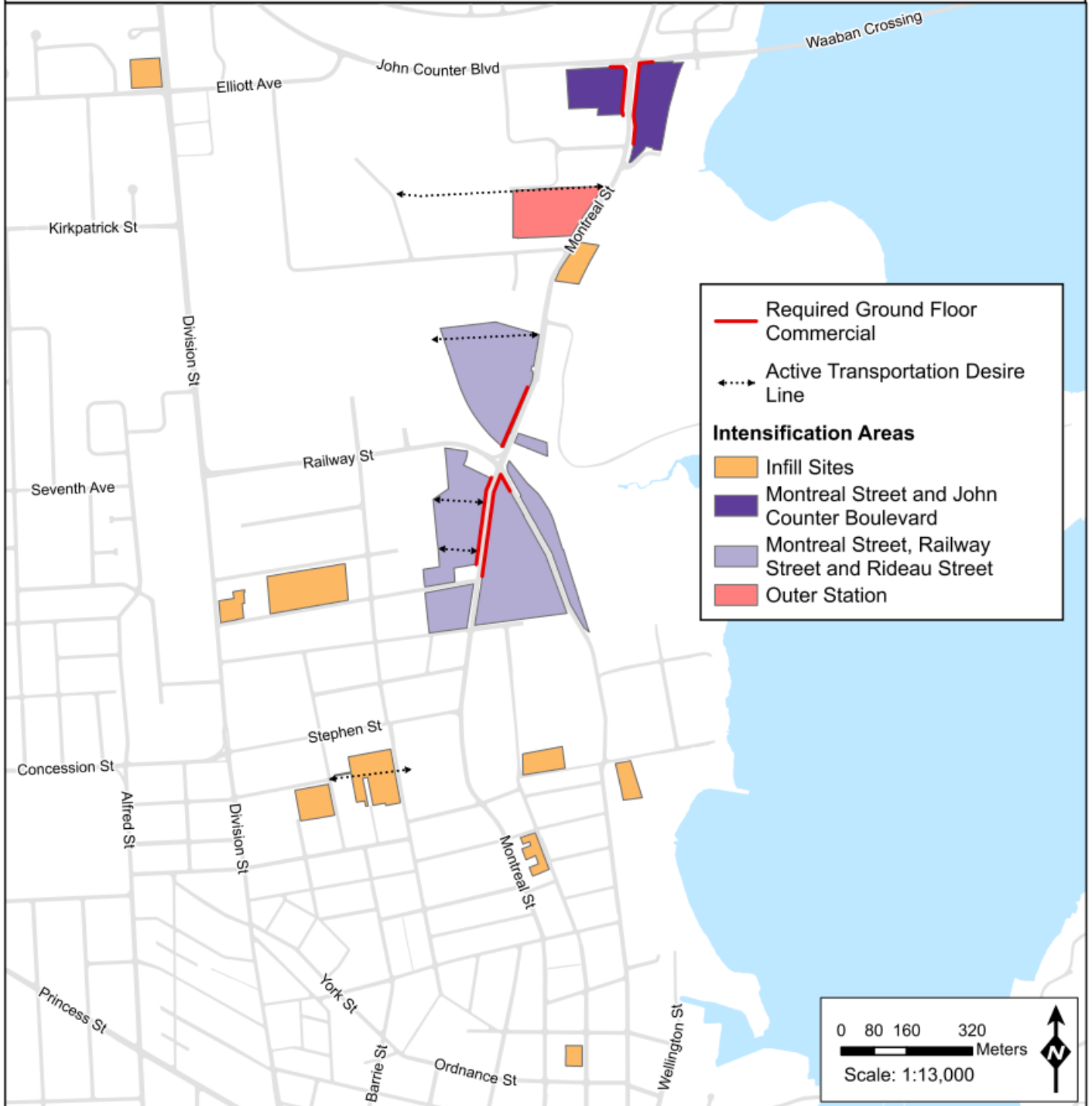
**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'I' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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# North King's Town Specific Policy Area

## Employment Land Removal Rationale

### North King's Town – Specific Policy Area

As part of the proposed Official Plan and Zoning By-Law amendment related to the North King's Town (NKT) project, a few properties are proposed to be redesignated from a General Industrial or a Business Park Industrial designation to a new Mixed Use designation to allow for intensification. The rationale for these changes is provided below.

### Provincial Planning Statement, 2024 (PPS, 2024)

The Provincial Planning Statement, 2024 (PPS, 2024) has refined the definition of “employment area” to focus on uses that cannot locate in mixed use areas, such as heavy industry, manufacturing and large-scale warehousing. The PPS, 2024 directs other industrial uses that can be located in proximity to sensitive uses without adverse effects to strategic growth areas and mixed use areas where frequent transit service is available, outside of “employment areas”. The new definition of an “employment area” is as follows.

“Employment area: means those areas designated in an official plan for clusters of business and economic activities including manufacturing, research and development in connection with manufacturing, warehousing, goods movement, associated retail and office, and ancillary facilities. An employment area also includes areas of land described by subsection 1(1.1) of the *Planning Act*. Uses that are excluded from employment areas are institutional and commercial, including retail and office not associated with the primary employment use listed above.”

PPS, 2024 now permits planning authorities to consider the removal of land from employment areas at any time and removes the requirement for a comprehensive review. The tests to be met include that:

- a) there is an identified need for the removal and the land is not required for employment area uses over the long term;
- b) the proposed uses would not negatively impact the overall viability of the employment area by:
  1. avoiding, or where avoidance is not possible, minimizing and mitigating potential impacts to existing or planned employment area uses in accordance with policy 3.5;

2. maintaining access to major goods movement facilities and corridors;
- c) existing or planned infrastructure and public service facilities are available to accommodate the proposed uses; and
- d) the municipality has sufficient employment lands to accommodate projected employment growth to the horizon of the approved official plan.

The application of these tests is anticipated to rely on targets contained in official plans, which is discussed further below.

As part of the protection of employment areas from incompatible uses, Section 2.8.1.3 of the PPS, 2024 directs development “within 300 metres of employment areas to avoid, or where avoidance is not possible, minimize and mitigate potential impacts on the long-term viability of employment uses within existing or planned employment areas”.

## Official Plan

Section 3.6.5 of the Official Plan provides direction on the redesignation of employment lands and indicates that these requests will only be considered at the time of a comprehensive review and will only be supported when Council is satisfied that the lands are not required for employment uses over the long term and that there is a need for the redesignation. This language was consistent with the direction of the Provincial Policy Statement, 2014 and has not been updated to be consistent with the revised direction provided by the PPS, 2024.

Section 3.6.5 of the Official Plan provides the following criteria to assess redesignation requests:

- a. the proposal will only be considered in conjunction with a review that addresses the following matters to the satisfaction of Council:
  - € the land is not required for employment purposes over the long-term;
  - € there is a need for the conversion;
  - € the intensity, characteristics and impacts associated with the proposed use will not detrimentally impact the viability, desirability, or the proper servicing of existing and future surrounding land uses;
  - € the site's physical and natural characteristics, development constraints, and location will justify the consideration of non-employment uses on the subject lands; and
  - € redesignation of employment areas abutting major transportation corridors, including railways, highways and major arterial roads is discouraged.
- b. in addition to subsection a. above, the proposed redesignation must meet the following conditions:



- € the amount of land affected is minor in area based on the projected employment area land requirements within the planning horizon of this Plan;
  - € the development of the subject lands is demonstrated to the satisfaction of Council not to be feasible for employment area uses within the planning horizon of this Plan;
  - € there are no alternative sites, designated and approved for the proposed use elsewhere in the City;
  - € the proposal will have a beneficial impact on the surrounding uses and broader community; and,
  - € the development of the subject lands for non-employment uses will meet a public need identified by Council resolution.
- c.** in addition to subsection a. and b., the site of the proposed conversion must meet the following criteria:
- € the site is located on the fringe of a designated Employment Area;
  - € the site is surrounded by non-employment uses on at least three sides;
  - € the conversion would be consistent/supportive of the City's objectives as described in this Plan;
  - € the conversion would not contravene any of the City's objectives as described in this Plan;
  - € the site offers limited market choice for industrial development due to size, configuration, physical conditions, and other constraints; and,
  - € the site does not offer practical or feasible potential for future expansion onto existing or neighbouring employment lands.
- d.** in addition to subsections a., b., and c. above, if the proposed re-designation is to a commercial designation, then the following applies:
- € a Commercial Inventory Assessment, as described in Section 3.4.9 of this Plan, must be undertaken by the applicant to the satisfaction of the City; and,
  - € if the proposed commercial development is greater than 5,000 square metres and is outside the Central Business District, the applicant must satisfy the provisions of Section 3.4.10 of this Plan.

## Proposal and Rationale

The NKT project proposes to remove approximately 18 hectares of land from an industrial land use designation, which represents a minor amount of employment land contemplated for development within the planning horizon of the current Official Plan. The majority of the lands proposed for removal from an industrial land use designation have been vacant or underutilized for a number of years. which suggests that development of the lands for employment purposes is not feasible. The lands are

located in proximity to existing residential uses and the conversion to non-employment uses may reduce existing and future land use compatibility conflicts. The proposed Zoning By-Law amendment would include the incorporation of requirements related to separation distances between sensitive uses and Class 1, 2 or 3 industrial uses as per the Province’s D-Series Guidelines.

While this amendment is proposed outside of a comprehensive review (which is no longer a requirement of PPS, 2024), it is supported by the recently completed Employment Area Lands Review, which is a supporting document for the City’s new Official Plan project, and represents the same analysis that would have been undertaken during a comprehensive review. Additional information on the findings of the Employment Area Lands Review is provided in the following section.

The employment lands proposed for redesignation are located on the southern fringe of the Old Industrial Area and are generally surrounded on three sides by non-employment uses. The proposed removal of these lands from an industrial land use designation would support the City’s objectives to provide mixed use intensification and increase the supply of housing within the urban area of the municipality, support transit ridership, promote active transportation, and contribute to more efficient delivery of services. Supporting amendments are proposed to the zoning by-law which would implement minimum separation distances between sensitive uses and Class 1, 2, or 3 industrial uses, thereby ensuring land use compatibility and ensuring no adverse effects for surrounding employment uses.

The removal of these lands from an industrial land use designation is being sought to promote mixed use development within the urban area of the city, along express transit and active transportation routes and within walking distance of open space and recreational facilities, which would contribute to a more efficient use of the land. The redevelopment of these lands for mixed uses would benefit the broader community by increasing housing options within the city, directly supporting one of Council’s strategic priorities.

Table 1 below includes a list of the properties proposed for removal from an industrial land use designation.

**Table 1 – List of properties proposed for removal from an industrial land use designation.**

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
856 Montreal Street	101104011017000	Automobile Repair Shop	General Industrial	Directly adjacent to sensitive land use and likely

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
				constrained for industrial uses.
810 Montreal Street	101104015034200	Vacant	General Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses. Property has been vacant for prolonged period. Contains protected heritage buildings.
688 Montreal Street	101104011005200	Residential	General Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
686 Montreal Street	101104011005100	Residential	General Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
684 Montreal Street	101104011005000	Residential	General Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
670 Montreal Street	101104011004700	Vacant	Business Park Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses. Property has been vacant for prolonged period.
662 - 664 Montreal Street	101104011004600	Retail Store and Residential	General Industrial	Property contains a retail store and residential uses, which are not permitted within an Employment Area.
604-606 Montreal Street	101104010003300	Residential	Business Park Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
600-602 Montreal Street	101104010003100	Vacant	Business Park Industrial	Property has been vacant for prolonged period. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
603 Montreal Street	101104001019500	Vacant	General Industrial	Property has been vacant for prolonged period. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
594-598 Montreal Street	101104010002900	Vacant	Business Park Industrial	Property has been vacant for prolonged period. Reduced

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
				parcel size is a constraint to industrial uses. Proximity to adjacent sensitive use.
576 Montreal Street	101104010002400	Vacant	Business Park Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses.
575 Montreal Street	101104001019400	Retail Store, Automobile Repair Shop	General Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses. Property contains a retail store, which may not be permitted within an Employment Area.
553 Montreal Street	101104001019300	Automobile Repair Shop	General Industrial	Reduced parcel size is a constraint to industrial uses.
541 - 549 Montreal Street	101104001019000	Light Industrial	General Industrial	Reduced parcel size is a constraint to industrial uses.
546 Montreal Street	101104010001500	Vacant	Business Park Industrial	Property has been vacant for prolonged period. Proximity to adjacent sensitive use.
537 Montreal Street	101104001019000	Unknown – potentially Residential	General Industrial	Property potentially contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses.
469 – 533	101104001018802	Institutional and Office	General Industrial	Institutional and Office uses are not

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
Montreal Street				permitted within an Employment Area.
445 Montreal Street	101104001018700	Contractor's yard	General Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses.
439 Montreal Street	101104001018600	Transportation Depot	General Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses.
425 Montreal Street	101104001018500	Automobile Repair Shop	General Industrial	Directly adjacent to sensitive land use and likely constrained for industrial uses.
000 Montreal Street	101104010003500	Vacant	Business Park Industrial	Reduced parcel size is a constraint to industrial uses. Proximity to sensitive land use.
000 Montreal Street	101104011009110	Open Space	General Industrial	Recreational facilities are not permitted within an Employment Area. Property does not have road frontage.
000 Montreal Street	101104011009100	Community Centre	General Industrial	Associated with the Royal Canadian Legion, which is not permitted within an Employment Area. Property does not have road frontage.
000 Railway	101104010003800	Vacant	Business Park Industrial	Reduced parcel size is a constraint to industrial uses. Adjacent to sensitive land use.
468 Rideau Street	101104001019800	Light Industrial	General Industrial	Directly adjacent to sensitive land use and likely

<b>Property Address</b>	<b>Roll Number</b>	<b>Existing Use</b>	<b>Existing Land Use Designation</b>	<b>Rationale for Removal</b>
				constrained for industrial uses.
40 – 44 Hickson Avenue	101104011008500 101104011008600	Residential	General Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.
15-21 McKenna Avenue	101104011008300 101104011008200	Residential	General Industrial	Property contains a residential use, which is not permitted within an Employment Area. Reduced parcel size is a constraint to industrial uses. Directly adjacent to sensitive land use.

It is staff’s understanding that the new PPS, 2024 policies related to the removal of lands from “employment areas” are limited to areas with traditional industrial land uses such as manufacturing, warehousing, goods movement and related uses. A number of the uses that are excluded from the new definition of “employment area” are permitted within the existing Business Park Industrial and General Industrial designations that apply to the lands contemplated for conversion, including, but not limited to, automobile sales and service, community facilities, offices and institutional uses.

Through the new Official Plan project, the City will be creating new protected industrial areas as per the new definition of “employment area” included in PPS, 2024 that are not constrained and are well separated from sensitive uses to maintain the long-term operational and economic viability of the planned uses and function of these areas.

In response to Official Plan policy 3.6.5.d, a separate market justification study and impact assessment for these conversion lands has not been prepared as the commercial uses associated with these lands are primarily intended to support the future residential uses within the mixed use developments and surrounding neighbourhoods. The commercial uses associated with these conversion lands are not intended to compete with the commercial uses within the Central Business District. As

discussed within Report Number 24-223, the City has recently undertaken a Commercial Land Review, which confirms that the City will require additional commercial lands to support future growth.

### **Employment Area Land Review**

As discussed within Report Number 24-221, Waston & Associates was retained to prepare the Employment Area Lands Review (EALR), which is a key background study for the Official Plan project. The EALR provides a comprehensive review of City-owned and privately owned employment land within the city to ensure that an adequate supply of employment land is designated to accommodate long-term demand.

Through this work, the EALR identified a series of lands where an employment designation would not be appropriate in the future, given the amended definition of “Employment Area”, surrounding land uses, site access constraints, proximity to Highway 401 and other factors. The EALR contemplated the removal of certain lands within NKT located within an industrial land use designation and determined that these vacant and underutilized lands were not well situated for employment uses and would be more appropriate within a non-employment designation.



## North King's Town Specific Policy Area

### Proposed Amendments to the Zoning By-Law

#### Kingston Zoning By-Law 2022-62

The following sections show the proposed amendments to Zoning By-Law 2022-62 to implement the North King's Town Specific Policy Area.

The following new section is proposed to be added:

#### **4.32 Separation Distances between Sensitive Uses and Industrial Facilities**

**4.32.1** Despite anything to the contrary in this By-law, the establishment of a new **sensitive use** must comply with the following minimum separation distances from a Class 1, Class 2 or Class 3 industrial facility, as applicable, located within an Employment Zone. Despite clause 3.19.7., this separation distance must be measured as per the province's D-series Environmental Land Use Compatibility Guidelines, or any successor thereof:

1. Minimum separation distance from a Class 1 industrial facility located within an Employment Zone: 20 metres;
2. Minimum separation distance from a Class 2 industrial facility located within an Employment Zone: 70 metres; and
3. Minimum separation distance from a Class 3 industrial facility located within an Employment Zone: 300 metres.

**4.32.2** Despite clause 4.32.1, any **sensitive use** within the required separation distance existing as of the date of passing of By-law Number 2025-XX is considered to comply with this provision and may be expanded as long as it does not further increase the extent or degree of non-compliance with clause 4.32.1.

The following section is proposed to be amended as shown in tracked changes.

## Section 10: Mixed Use Zones

### 10.1 All Mixed Use Zones

**10.1.1.** For the purposes of this By-law, Mixed Use Zones include Williamsville Zone 1 (WM1), Williamsville Zone 2 (WM2), Downtown Zone 1 (DT1), ~~and~~ Downtown Zone 2 (DT2), Mixed Zone 1 (MU1), Mixed Zone 2 (MU2) and Mixed Zone 3 (MU3).

**10.1.2.** **Uses** permitted in Mixed Use Zones are limited to the **uses** identified in Table 10.1.2., and are denoted by the symbol “●” in the column applicable to each Zone and corresponding with the row for a specific permitted **use**. Where the symbol “—” is identified in the table, the **use** is not permitted in that Zone.

**10.1.3.** Where a permitted **use** includes a reference number in superscript beside the “●” symbol in Table 10.1.2., the following corresponding provision applies:

1. Is required to operate within an enclosed building.

**Table 10.1.2. - Permitted Uses in the Mixed Use Zones**

Use	WM1, WM2	DT1, DT2	<u>MU1, MU2, MU3</u>
Residential apartment building	●	●	<u>●</u>
dwelling unit in a mixed use building	●	●	<u>●</u>
stacked townhouse	●	—	<u>●</u>
townhouse	●		<u>●</u>
Non-residential animal care	—	●	<u>—</u>
automobile sales establishment	—	●	<u>—</u>
banquet hall	●	●	<u>●</u>
building supply store	—	●	<u>—</u>
<u>catering service</u>	<u>—</u>	<u>—</u>	<u>●</u>
commercial parking lot	—	●	<u>—</u>
community centre	●	●	<u>●</u>
club	●	●	<u>●</u>
creativity centre	●	●	<u>●</u>
day care centre	●	●	<u>●</u>
department store	—	●	<u>—</u>

Use	WM1, WM2	DT1, DT2	<u>MU1, MU2, MU3</u>
entertainment establishment	•	•	<u>•</u>
financial institution	•	•	<u>•</u>
fitness centre	•	•	<u>•</u>
service station	—	•	<u>—</u>
grocery store	•	•	<u>•</u>
funeral establishment	•	•	<u>•</u>
hotel	•	•	<u>•</u>
laboratory	•	—	<u>•</u>
laundry store	•	•	<u>•</u>
library	•	•	<u>•</u>
museum	•	•	<u>•</u>
office	•	•	<u>•</u>
personal service shop	•	•	<u>•</u>
place of worship	•	•	<u>•</u>
<u>production studio</u>	<u>—</u>	<u>—</u>	<u>•</u>
public market	•	•	<u>•</u>
recreation facility	•	•	<u>•</u>
repair shop	—	•	<u>—</u>
<u>research establishment</u>	<u>—</u>	<u>—</u>	<u>•</u>
restaurant	•	•	<u>•</u>
retail store	•	•	<u>•</u>
special needs facility	•	—	<u>•</u>
training facility	—	•	<u>•<sup>1</sup></u>
transportation depot	—	•	<u>—</u>
transportation terminal	—	•	<u>—</u>
wellness clinic	•	•	<u>•</u>
<u>workshop</u>	<u>—</u>	<u>—</u>	<u>•<sup>1</sup></u>

The following sections are proposed to be added:

## 10.6 Mixed Zone 1 (MU1)

**10.6.1.** The **use** of any **lot** or **building** in the MU1 Zone must comply with the provisions of Table 10.6.1.

**Table 10.6.1 – MU1 Provisions**

Zoning Provision	All permitted uses
2. Minimum <b>lot area</b> (square metres)	—
3. Minimum <b>lot frontage</b> (metres)	—
4. Minimum <b>streetwall height</b> (metres)	10.5
5. Minimum floor to floor <b>height</b> of <b>first storey</b> (metres)	4.5
6. Maximum height	The lesser of: (a) 20 metres (b) 6 storeys
7. Minimum <b>front setback</b> (metres)	3
8. Minimum <b>rear setback</b> (metres)	10
9. Minimum <b>exterior setback</b> (metres)	3
10. Minimum <b>interior setback</b> (metres)	3
11. Minimum <b>stepbacks</b> (metres)	(a) Front lot line and exterior lot line: i. <b>storey</b> 5 to 6: minimum 3.0 metres from the exterior wall of the 4th <b>storey</b>
12. Minimum <b>landscaped open space</b>	15%
13. Maximum <b>lot coverage</b>	55%
14. Maximum residential <b>density</b>	175 <b>dwelling units</b> per net hectare

### Additional Provisions for Lots Zoned MU1

**10.6.2.** In addition to the provisions of Table 10.6.1., **uses** in the MU1 Zone must comply with the following provisions:

### Front Setback of Streetwall

1. A minimum of 75% of the **streetwall** of a building must be built to the minimum required front setback for the height of the streetwall.

### Ground Floor Commercial Uses

2. **Buildings** are required to have ground floor commercial **uses** on the **first storey** where any portion of the **lot** aligns with the area identified as "Required Ground Floor Commercial" on Schedule 3 of this By-law.
3. Where ground floor commercial **uses** are required by Subclause 2., the entire **streetwall** of the **first storey**, excluding areas devoted to a lobby or other shared entrances/exits for other permitted **uses**, must be occupied by commercial **uses**. Portions of the floor area of the **first storey** that do not have an exterior wall facing a **street line** may be occupied by **uses** that service the **building** such as **loading spaces**, waste management facilities and rooms, mechanical rooms, bike parking facilities and other similar **uses**.

### Main Pedestrian Entrance

4. The main pedestrian entrance to the **building** must be located on the **main wall** facing the **front lot line**.

### Projecting Balconies

5. **Balconies** are only permitted above the second **storey**, to a maximum projection of 1.5 metres from the **main wall**.

### Loading and Parking

6. **Parking structures** are not permitted to occupy any part of a **main wall** facing a **street line**.
7. **Loading spaces**, **parking spaces** and **parking lots** are not permitted in a **front yard** or **exterior yard**.

### Planting Strip

8. Where a **lot** is adjacent to a **lot** in a UR Zone, a Commercial Zone or an Employment Zone, within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

### Legally Existing Uses

9. **Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.

## 10.7 Mixed Zone 2 (MU2)

10.7.1. For the purpose of the MU2 Zone, the following definitions apply:

1. **Podium** means the base component of any **building** that is no greater than 20 meters in **height** (excluding mechanical penthouses) and only includes the **first storey** through sixth **storeys** of such **building**.
2. **Tower** means any portion of any **building** that is greater than 20 metres in **height**, excluding a **podium**, below grade **building** components and mechanical penthouses.

10.7.2. The **use** of any **lot** or **building** in the MU2 Zone must comply with the provisions of Table 10.7.1.

**Table 10.7.1 – MU2 Provisions**

Zoning Provision	All permitted uses
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Minimum <b>streetwall height</b> (metres)	10.5
4. Minimum floor to floor <b>height</b> of <b>first storey</b> (metres)	4.5
5. Maximum height	The lesser of: (a) 50 metres (b) 15 storeys
6. Minimum <b>front setback</b> (metres)	3
7. Minimum <b>rear setback</b> (metres)	10
8. Minimum <b>exterior setback</b> (metres)	3
9. Minimum <b>interior setback</b> (metres)	3
10. Minimum <b>stepbacks</b> (metres)	(b) Front lot line and exterior lot line: i. <b>storey</b> 5 to 6: minimum 3.0 metres

	from the exterior wall of the 4th <b>storey</b>
11. Minimum <b>landscaped open space</b>	15%
12. Maximum <b>lot coverage</b>	55%
13. Maximum residential <b>density</b>	300 <b>dwelling units</b> per net hectare

**Additional Provisions for Lots Zoned MU2**

**10.7.3.** In addition to the provisions of Table 10.7.1., **uses** in the MU2 Zone must comply with the following provisions:

**Front Setback of Streetwall**

1. A minimum of 75% of the **streetwall** of a building must be built to the minimum required front setback for the height of the streetwall.

**Ground Floor Commercial Uses**

2. **Buildings** are required to have ground floor commercial **uses** on the **first storey** where any portion of the lot aligns with the area identified as "Required Ground Floor Commercial" on Schedule 3 of this By-law.
3. Where ground floor commercial uses are required by Subclause 2., the entire **streetwall** of the **first storey**, excluding areas devoted to a lobby or other shared entrances/exits for other permitted **uses**, must be occupied by commercial **uses**. Portions of the floor area of the **first storey** that do not have an exterior wall facing a **street line** may be occupied by **uses** that service the **building** such as **loading spaces**, waste management facilities and rooms, mechanical rooms, bike parking facilities and other similar **uses**.

**Main Pedestrian Entrance**

4. The main pedestrian entrance to the **building** must be located on the **main wall** facing the **front lot line**.

**Projecting Balconies**

5. **Balconies** are only permitted above the second **storey**, to a maximum projection of 1.5 metres from the main wall.

### Loading and Parking

6. **Parking structures** are not permitted to occupy any part of a **main wall** facing a **street line**.
7. **Loading spaces, parking spaces** and **parking lots** are not permitted in a **front yard** or **exterior yard**.

### Tower Conditions

8. The maximum floor plate of a **tower** is 800 square metres. **Tower** floor plate includes all areas enclosed within exterior walls, including hallways, elevators, stairs, mechanical shafts, and all similar components.
9. The minimum **separation distance** between a **tower** and another **tower** is 25.0 metres.
10. The minimum **setback** from a **tower** to a **lot line** shared with an adjacent property is 12.5 metres.
11. Despite Subclause 10., where an adjacent property has already been developed with a **tower**, the **tower** is permitted to be located closer than 12.5 metres to the **lot line** shared with that adjacent property so long as the 25.0 metre **tower separation distance** is maintained.
12. The minimum **setback** from a **tower** to the exterior wall of the **podium** is 2.0 metres.

### Planting Strip

13. Where a **lot** is adjacent to a **lot** in a UR Zone, a Commercial Zone or an Employment Zone, within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

### Legally Existing Uses

14. **Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.
15. Commercial **uses** that legally existed prior to the passage of this By-law are deemed to be permitted uses and must comply with the provisions of the CN Zone.



## 10.8 Mixed Zone 3 (MU3)

**10.8.1.** The **use** of any **lot** or **building** in the MU3 Zone must comply with the provisions of Table 10.8.1.

**Table 10.8.1 – MU3 Provisions**

Zoning Provision	All permitted uses
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Minimum <b>height</b> (storeys)	2
4. Maximum height	The lesser of: a) 4 storeys b) 12.5 metres
5. Minimum <b>front setback</b> (metres)	3.0
6. Minimum <b>rear setback</b> (metres)	8.0
7. Minimum <b>exterior setback</b> (metres)	3.0
8. Minimum <b>interior setback</b> (metres)	a) non-residential buildings: equal to half the height of the building b) residential buildings up to 3 storeys: 1.2 metres c) each additional storey above 3: 1.2 metres d) Despite (b) and (c), for townhouses, where a common party wall is located along a lot line: 0 metres
9. Minimum <b>landscaped open space</b>	30%
10. Maximum <b>lot coverage</b>	45%
11. Maximum residential <b>density</b>	90 <b>dwelling units</b> per net hectare

### Additional Provisions for Lots Zoned MU3

**10.8.2.** In addition to the provisions of Table 10.8.1., **uses** in the MU3 Zone must comply with the following provisions:

#### Front Setback of Streetwall

1. A minimum of 75% of the **streetwall** of a building must be built to the minimum required front setback for the height of the streetwall.

### Loading and Parking

2. **Loading spaces, parking spaces** and **parking lots** are not permitted in a **front yard** or **exterior yard**.

### Planting Strip

3. Within the required **rear setback**, a minimum 2.0 metre wide **planting strip** must be provided along the full length of the **rear lot line** and must comply with Subclause 4.16.1.2.

### Legally Existing Uses

4. **Residential uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.

The following section is proposed to be amended as shown in tracked changes:

## Section 12: Urban Multi-Residential Zones

### 12.1 All Urban Multi-Residential Zones

12.1.1. For the purposes of this By-law, Urban Multi-Residential Zones include Urban Multi-Residential Zone 1 (URM1), Urban Multi-Residential Zone 2 (URM2), Urban Multi-Residential Zone 3 (URM3), Urban Multi-Residential Zone 4 (URM4), Urban Multi-Residential Zone 5 (URM5), Urban Multi-Residential Zone 6 (URM6), Urban Multi-Residential Zone 7 (URM7), Urban Multi-Residential Zone 8 (URM8), Urban Multi-Residential Zone 9 (URM9), ~~and~~ Urban Multi-Residential Zone 10 (URM10) and Urban Multi-Residential Zone 11 (URM11).

12.1.2. **Uses** permitted in Urban Multi-Residential Zones are limited to the **uses** identified in Table 12.1.2., and are denoted by the symbol "●" in the column applicable to each Zone and corresponding with the row for a specific permitted **use**. Where the symbol "—" is identified in the table, the **use** is not permitted in that Zone.

12.1.3. Where a permitted **use** includes a reference number in superscript beside the "●" symbol in Table 12.1.2., the following corresponding provisions apply:

1. May only contain **non-residential uses** that are permitted in the CN Zone as per Table 15.1.2., and the **non-residential uses** are only permitted on the **first storey**.

2. Is only permitted on a **lot** that has a **front lot line** and/or **exterior lot line** on a Collector Road or Arterial Road in accordance with the **street type** identified in Schedule 4.

**Table 12.1.2. - Permitted Uses in the Urban Multi-Residential Zones**

Use	URM1	URM2	URM3	URM4	URM5	URM6	URM7	URM8	URM9	URM10	URM11
Residential apartment building	●	●	●	●	●	●	●	●	●	●	●
dwelling unit in a mixed use building	—	—	—	—	—	—	—	● <sup>1</sup>	—	—	● <sup>1</sup>
house	●	—	●	●	●	—	—	—	—	—	—
semi-detached house	●	—	—	—	—	—	—	—	—	—	—
stacked townhouse	●	●	—	—	—	—	—	●	●	●	●
townhouse	●	●	—	●	●	—	—	—	—	—	●
Non-residential community centre	●	●	●	●	●	●	●	●	●	●	●
day care centre	●	●	●	●	●	●	●	●	●	●	●
elementary school	●	●	●	●	●	●	●	●	●	●	●
library	●	●	●	●	●	●	●	●	●	●	●
museum	●	●	●	●	●	●	●	●	●	●	●
place of worship	●	●	●	●	●	●	●	●	●	●	●
secondary school	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>	● <sup>2</sup>

The following section is proposed to be added:

## 12.12 Urban Multi-Residential 11 (URM11)

**12.12.1.** The **use** of any **lot** or **building** in the URM11 Zone must comply with the provisions of Table 12.12.1.

**Table 12.12.1 – URM11 Provisions**

Zoning Provision	All permitted uses
1. Minimum <b>lot area</b> (square metres)	—
2. Minimum <b>lot frontage</b> (metres)	—
3. Maximum height	The lesser of: (a) 13.5 metres (b) 4 storeys
4. Minimum <b>front setback</b> (metres)	4.5
5. Minimum <b>rear setback</b> (metres)	7.5
6. Minimum <b>exterior setback</b> (metres)	4.5
7. Minimum <b>interior setback</b> (metres)	1.2 metres for the first three storeys, plus an additional 1.2 metres for the fourth storey
8. Minimum <b>landscaped open space</b>	30%
9. Maximum <b>lot coverage</b>	45%
10. Maximum residential <b>density</b>	90 <b>dwelling units</b> per net hectare

### Additional Provisions for Lots Zoned URM11

**12.12.2.** In addition to the provisions of Table 12.12.1., **uses** in the URM11 Zone must comply with the following provisions:

1. **Loading spaces, parking spaces**, except for **accessible spaces**, and **parking lots** are not permitted in a **front yard** or **exterior yard**.
2. Residential **uses** that legally existed prior to the passage of this By-law are deemed to be permitted **uses** and must comply with the provisions of the UR5 Zone.

## Exceptions

The following Exceptions are proposed to be added:

### Montreal Street - Main Street Commercial (CN)

- E186.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) In addition to the **uses** permitted by the applicable Zone, the following **uses** are permitted
    - (i) All **uses** permitted in the UR5 Zone as per the provisions of the UR5 Zone.; and
    - (ii) **Dwelling unit in a mixed use building.**

### John Counter Boulevard/Montreal Street Area (MU1)

- E187.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **height** is 12 **storeys**.
  - (b) The maximum floorplate size above the sixth storey is 1,200 square metres.
  - (c) The maximum **density** is 200 **dwelling units** per net hectare; and
  - (d) Residential uses that legally existed on the date of passing of the site specific by-law are deemed to be permitted uses and must comply with the provisions of the UR5 Zone.

### 102 Fraser Street and 775-791 Montreal Street (URM11)

### 15 Joseph and the rear of 546 Montreal Street (MU3)

- E188.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **density** is 120 **dwelling units** per net hectare.

### 12-14 Cataraqui Street (MU3)

- E189.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The maximum **height** is 6 **storeys**.
  - (b) The maximum **density** is 175 **dwelling units** per net hectare.

- (c) The fifth and sixth **storeys** are required to **stepback** a minimum of 3.0 metres from the exterior wall of the 4<sup>th</sup> **storey** along the **front lot line**.

#### 900 Division Street (MU1)

**E190.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum number of **dwelling units** is 230.

#### 410-420 Bagot Street (URM8)

**E191.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum **density** is 175 **dwelling units** per net hectare.
- (b) The maximum **floor space index** does not apply.
- (c) The fifth and sixth **storeys** are required to **stepback** a minimum of 3.0 metres from the exterior wall of the 4<sup>th</sup> **storey** along the **front lot line**.

#### 158 Patrick Street (URM11)

**E192.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) The maximum number of **dwelling units** is 40.
- (b) The minimum **rear setback** is 55 metres.

#### 541 Division Street (URM11)

**E193.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:

- (a) In addition to the **uses** permitted by the applicable Zone, the following **uses** are permitted:
  - (i) **Special needs facility**; and
  - (ii) **Wellness clinic**.
- (b) The lands subject to this Exception are deemed to be one **lot** for the purposes of interpreting zoning provisions;
- (c) **Uses** existing as of the date of passing of this site specific by-law must comply with the provisions of the URM11 Zone;
- (d) Where a **lot** contains more than one **use**, the required number of **parking spaces** is the sum of all **parking spaces** required;

- (e) The maximum number of **dwelling units** is 50
- (f) The southernmost **lot line** is deemed to be the **front lot line**;
- (g) The minimum **front setback** is 3.0 metres;
- (h) The minimum **rear setback** is 4.5 metres;
- (i) The minimum **exterior setback** is 3.0 m;
- (j) The maximum **height** is the lesser of 16.5 metres or 4 **storeys**;
- (k) **Accessible spaces** are permitted in the **front yard**;
- (l) The minimum **drive aisle** width is 5 metres for **accessible spaces** located in the **front yard**;
- (m) A **planting strip** is not required;
- (n) A **canopy** may project a maximum of 4.5 metres into a required **sight triangle**, provided it has a minimum clearance of 2.8 metres above the elevation of the Division Street centreline; and
- (o) A **canopy** may project a maximum distance of 1.7 metres into a required **setback**, provided such **canopy** is **setback** a minimum of 0.5 metres to any lot line.

### 317 Montreal Street (CN)

- E194.** Despite anything to the contrary in this By-Law, the following provisions apply to the lands subject to this Exception:
- (a) The minimum **front setback** is 1.5 metres.
  - (b) The minimum **interior setback** is 0 metres on the north side and 0.6 metres on the south side.
  - (c) The minimum size of a standard **parking space** is 2.6 metres wide and 5.2 metres long.
  - (d) The minimum **driveway width** is 2.3 metres.
  - (e) Unenclosed front **porches** and below-grade steps are permitted to project 1.5 metres out from the main building wall, with no minimum setback from the **front lot line**. Minimum **interior setback** for an unenclosed front porch and below-grade steps is 0.5 metres (south) and 0 metres (north).
  - (f) An **accessory building** may be located not less than 0.9 metres from the **rear lot line** and not less than 0.3 metres from the north **interior lot line**.
  - (g) **Amenity areas** may be aggregated into spaces of not less than 43 square metres.

### Legacy Exceptions

The following changes are proposed to the Legacy Exceptions:

**317 Montreal Street (CN)**

**L102:** To be deleted.

**235 Montreal Street (CN)**

**L192:** To be deleted

**722,730,766 John Counter Boulevard (MU1)**

**L205.** Text to remain the same. The boundary is proposed to be adjusted so that L205 only applies to the portion of the property proposed to remain in the URM2 Zone. The remainder of the property proposed to be included in the MU1 Zone would be subject to E187.

## Holding Overlay

The following new Holding Overlay is proposed for the intensification areas:

### For intensification areas to be rezoned to the MU1, MU2, MU3, URM11, and URM8 Zones

- H233.** Prior to the removal of the Holding Overlay, the following conditions must be satisfied:
- (a) The City is satisfied that there is adequate servicing capacity (i.e., water, wastewater, natural gas, and electrical) for the proposed development;
  - (b) A Transportation Impact Study is completed to the satisfaction of the City; and
  - (c) Confirmation from the Chief Building Official that a Record of Site Condition has been completed to the satisfaction of the City, or that no Record of Site Condition is required for the proposed development.

## List of Mapping Changes

- **Schedule 1, Zoning Map** - Amendment to reflect the proposed zone change of properties located within the Intensification Areas (MU1, MU2, MU3, URM11, URM8 Zones), the Main Street Commercial designation (CN Zone), and the Outer Station property (DR Zone).
- **Schedule 2, Parking Areas** – Amendments to include the intensification sites in Parking Area 2.



- **Schedule 3, Required Ground Floor Commercial** - Amendment to show the required ground floor commercial on Montreal Street.
- **Schedule 4, Road Classification** – Amendment to change the road classification of Rideau Street and Waaban Crossing.
- **Schedule E, Exception Overlay** – Amendment to add Exception Overlays for some of the intensification sites, and to amend/delete some of the Legacy Exceptions
- **Schedule F, Holding Overlay** – Amendment to add a Holding Overlay for the intensification sites.
- **Schedule H, Express Transit Area Overlay** – Amendment to remove the inset and properties on Montreal Street and the Express Transit Area Overlay for the intensification sites.



Planning Services

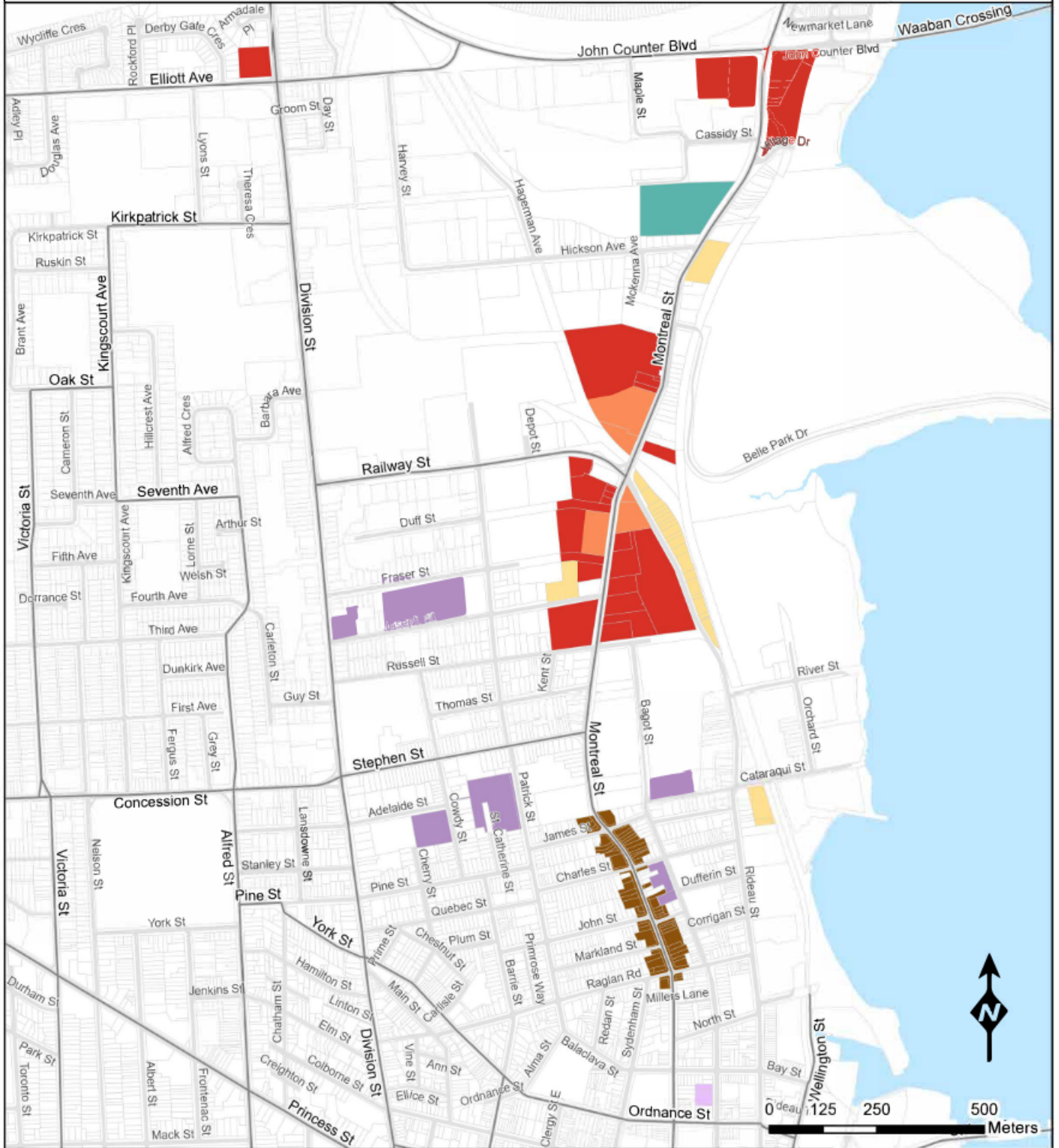
**Changes to Zoning By-Law 2022-62  
Schedule 1  
Zoning Map**

File Number: D35-001-2025

**Legend**

Lands to be rezoned from former zone to:

- Neighbourhood Commercial (CN)
- Mixed Zone 1 (MU1)
- Mixed Zone 2 (MU2)
- Mixed Zone 3 (MU3)
- Urban Multi-Residential Zone 8 (URM8)
- Urban Multi-Residential Zone 11 (URM11)
- Development Reserve (DR)





**Schedule 'A1'  
to By-Law Number**

File Number: D35-001-2025

Description: Zone Changes to MU1, OS2

**Certificate of Authentication**

This is Schedule 'A1' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

**Legend**

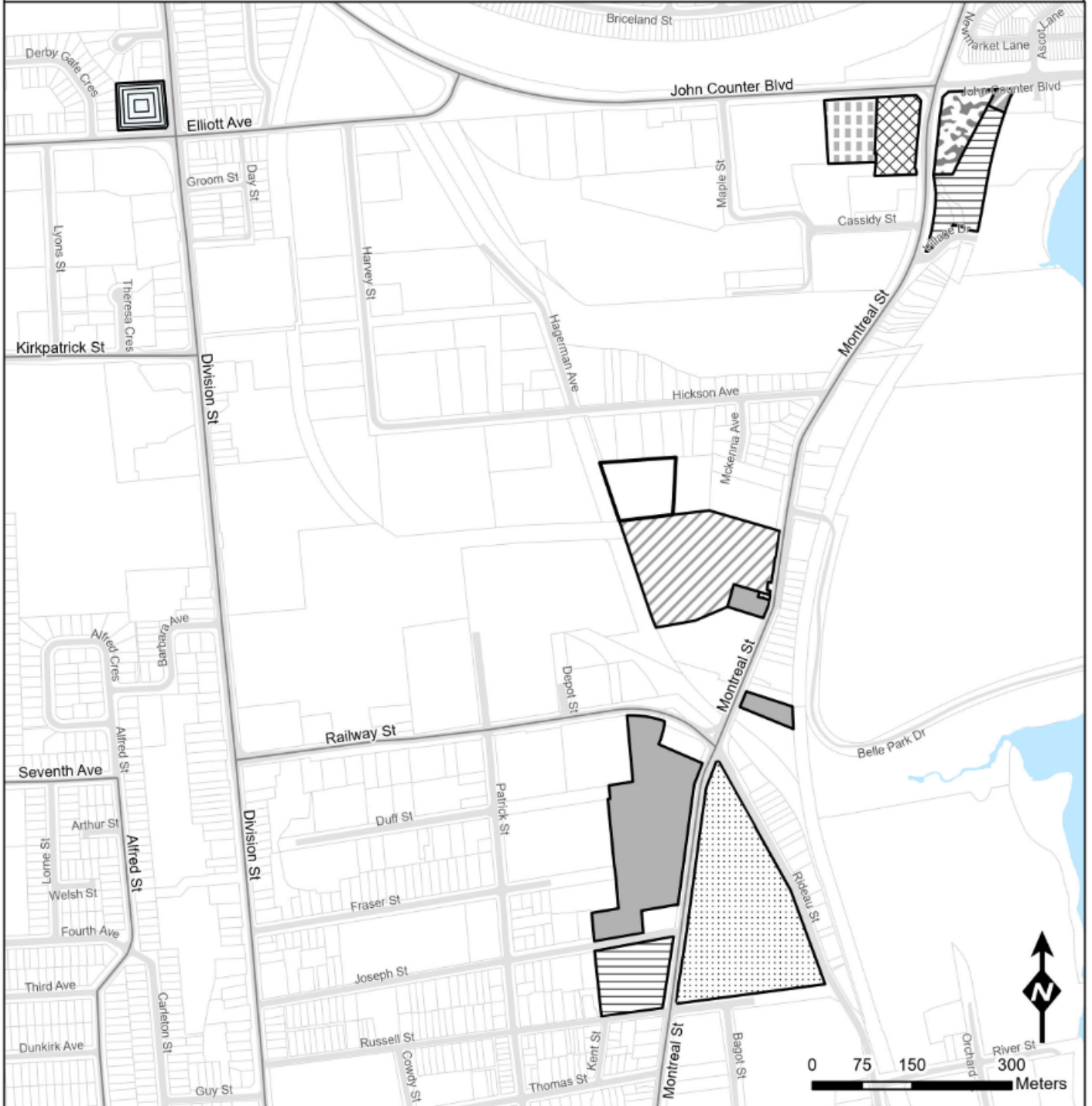
Lands to be rezoned from:

- |           |            |             |            |
|-----------|------------|-------------|------------|
| CA to MU1 | M2 to MU1  | UR5 to MU1  | N/A to OS2 |
| DR to MU1 | N/A to MU1 | UR6 to MU1  |            |
| M1 to MU1 | OS2 to MU1 | URM2 to MU1 |            |

**Planning  
Services**

\_\_\_\_\_  
Mayor

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Clerk





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### Schedule 'A2' to By-Law Number

File Number: D35-001-2025  
Description: Zone Changes to MU2

### Certificate of Authentication

This is Schedule 'A2' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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Mayor

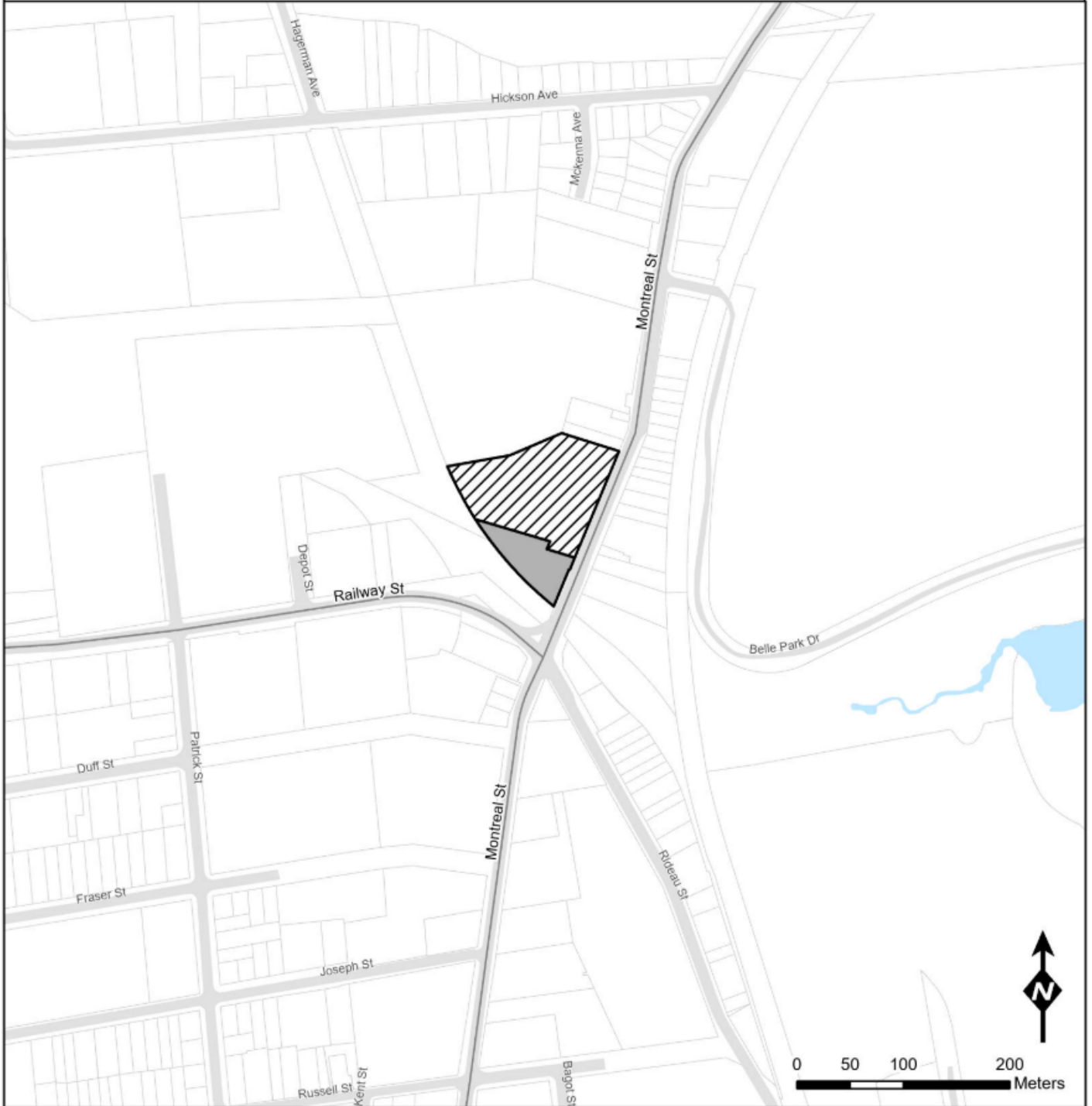
\_\_\_\_\_  
Clerk

### Zoning By-Law 2022-62 Schedule 1, Zone Map

#### Legend

Lands to be rezoned from:

-  M1 to MU2     M2 to MU2





**Schedule 'A3'  
to By-Law Number**

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

**Planning  
Services**

File Number: D35-001-2025  
Description: Zone Changes to MU3

**Legend**

Lands to be rezoned from:

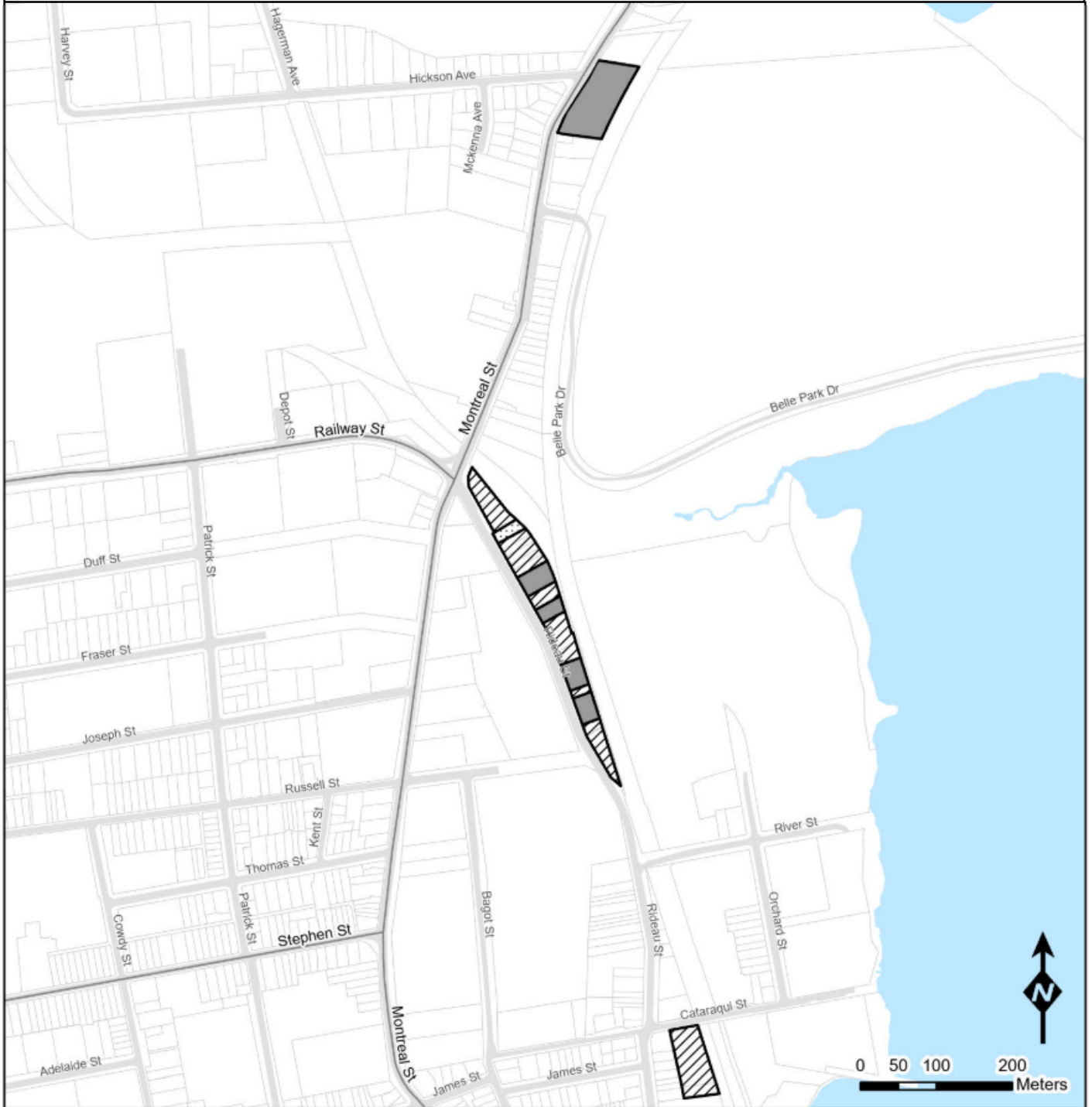
-  DR to MU3
-  N/A to MU3
-  UR5 to MU3

**Certificate of Authentication**

This is Schedule 'A3' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

\_\_\_\_\_  
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**Schedule 'A4'  
to By-Law Number**

File Number: D35-001-2025  
Description: Zone Changes to URM8

**Certificate of Authentication**

This is Schedule 'A4' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

**Legend**

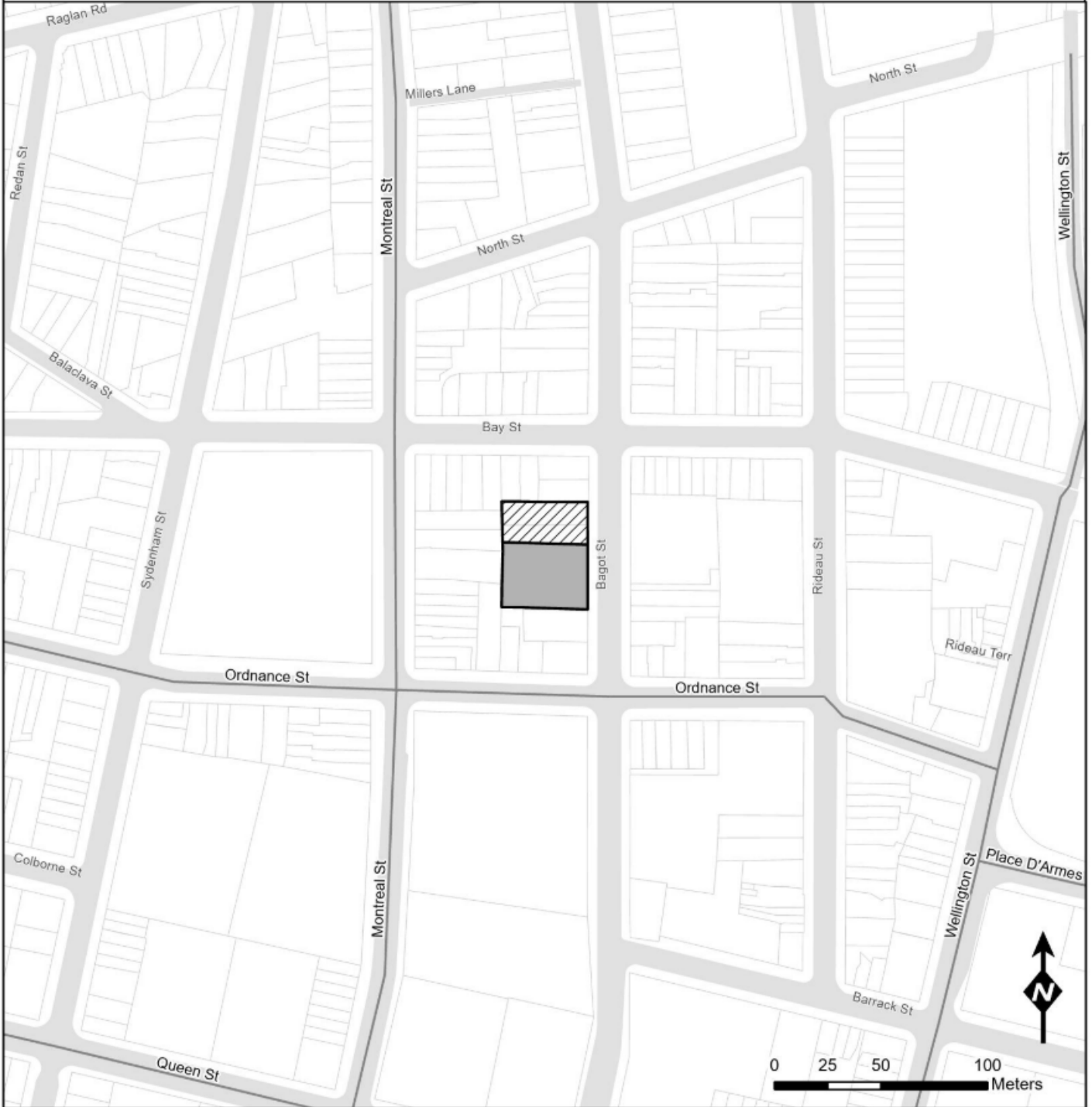
Lands to be rezoned from:

- UR5 to URM8
- N/A to URM8

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**Schedule 'A5'  
to By-Law Number**

File Number: D35-001-2025

Description: Zone Changes to URM11

**Certificate of Authentication**

This is Schedule 'A5' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

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**Zoning By-Law 2022-62  
Schedule 1, Zoning Map**

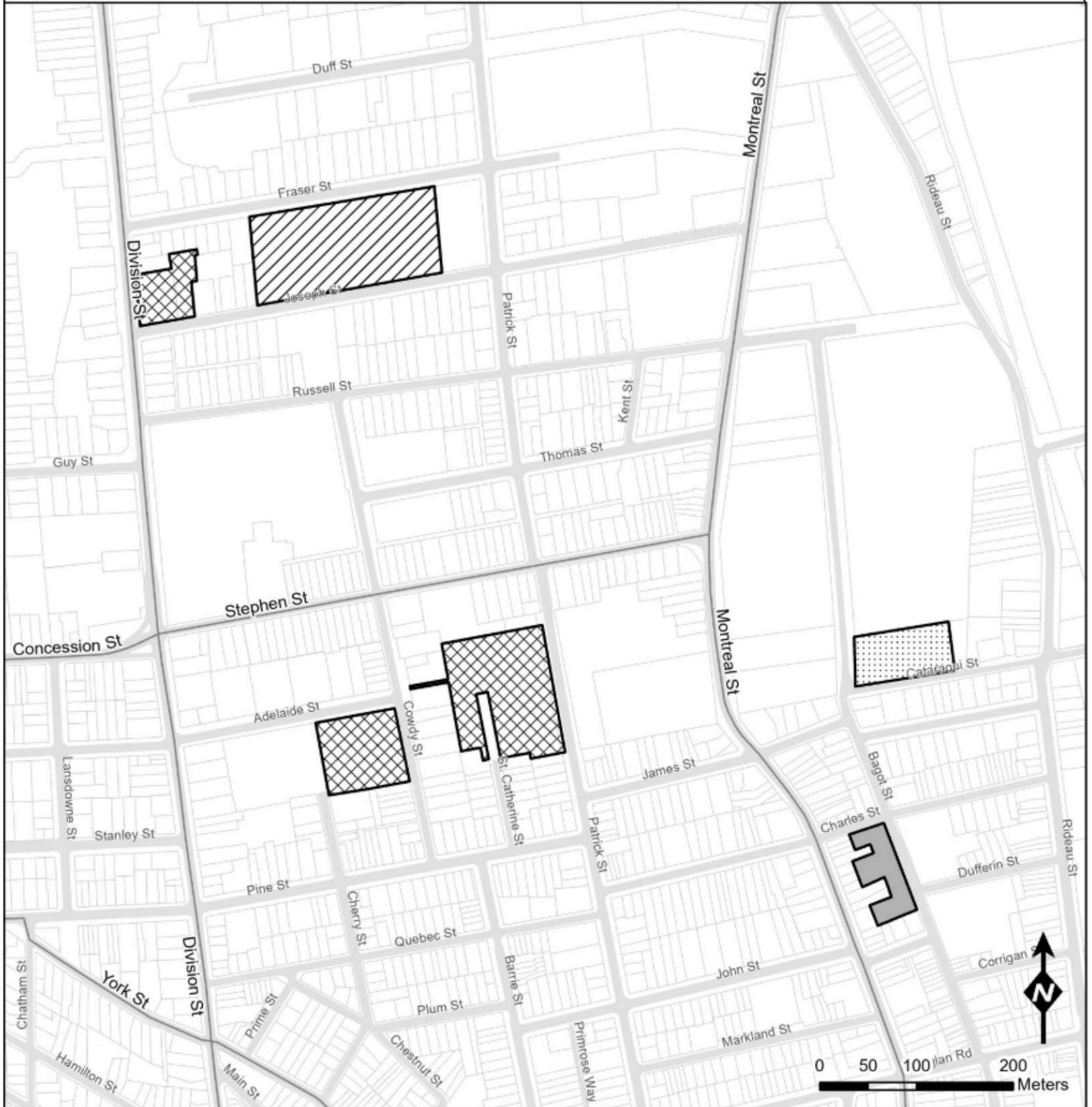
**Legend**

Lands to be rezoned from:

- |             |              |
|-------------|--------------|
| CD to URM11 | UR5 to URM11 |
| DR to URM11 | N/A to URM11 |

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### Schedule 'A6' to By-Law Number

File Number: D35-001-2025  
Description: Zone Changes to CN

### Certificate of Authentication

This is Schedule 'A6' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

### Zoning By-Law 2022-62 Schedule 1, Zone Map

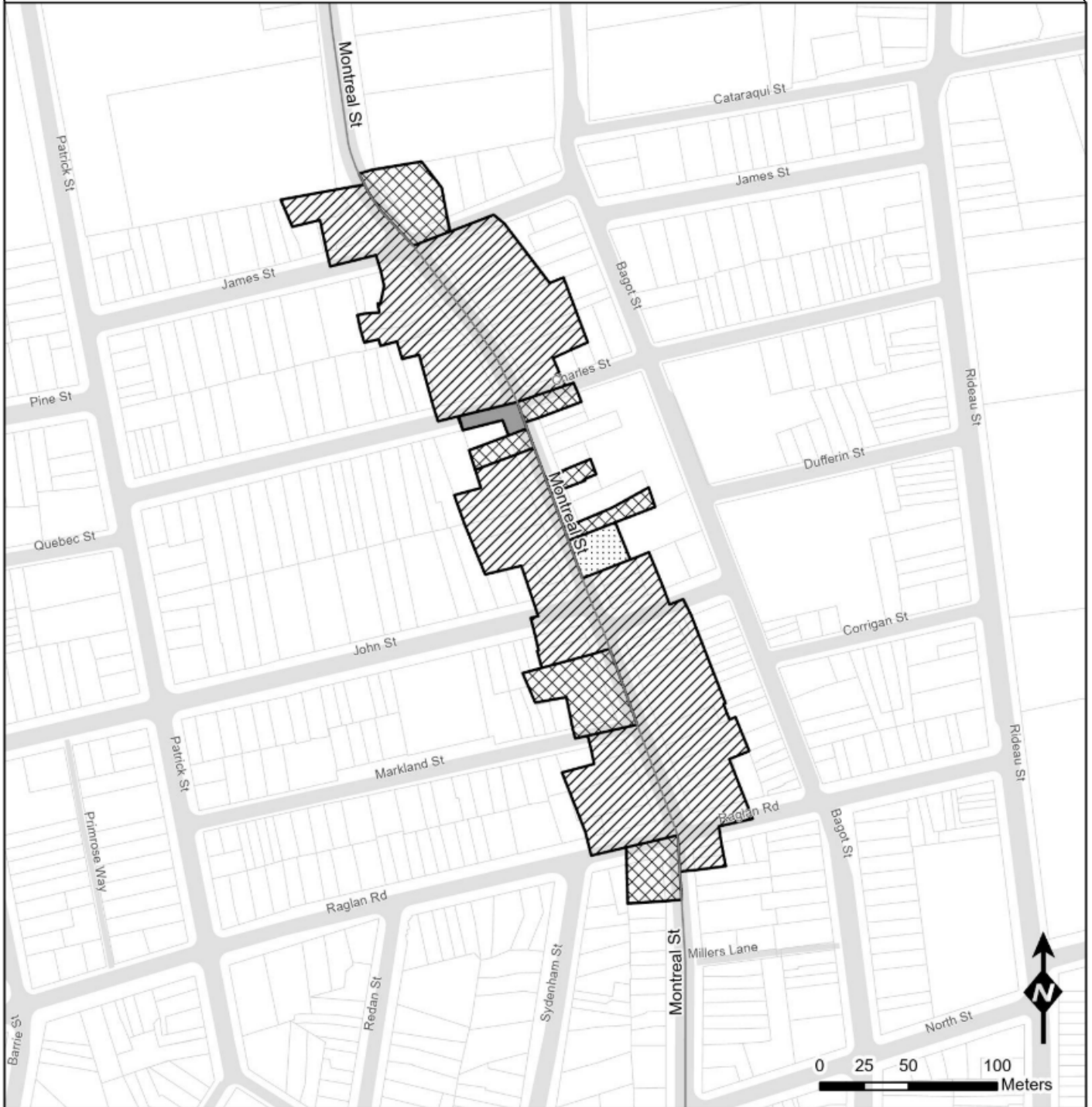
#### Legend

Lands to be rezoned from:

- |  |  |
|--|--|
|  CD to CN  |  URM3 to CN |
|  UR5 to CN |  N/A to CN  |

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**Schedule 'A7'  
to By-Law Number**

File Number: D35-001-2025  
Description: Zone Change to DR

**Certificate of Authentication**

This is Schedule 'A7' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule 1, Zone Map**

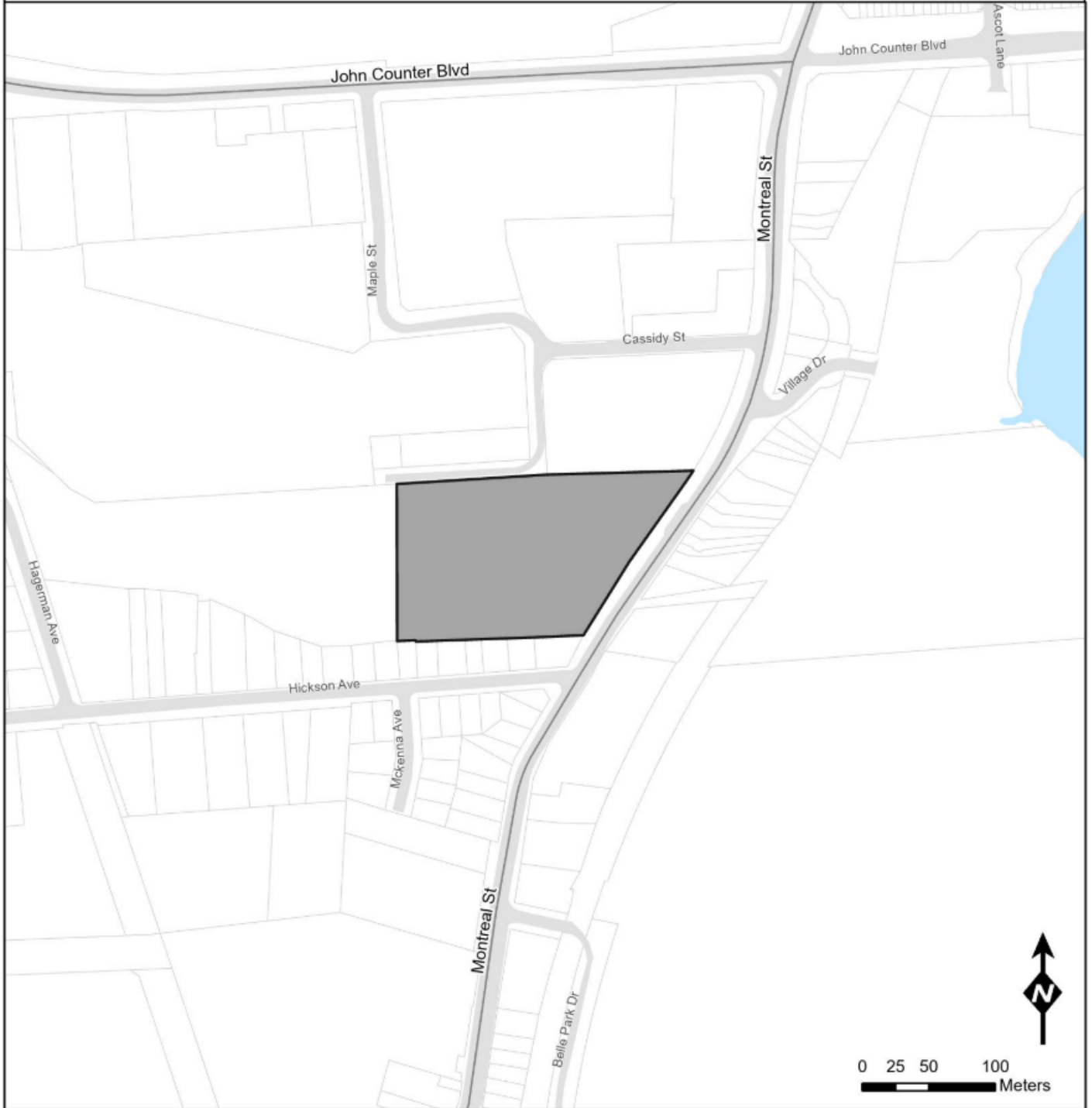
**Legend**

Lands to be rezoned from:

 N/A to DR

\_\_\_\_\_  
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### Schedule 'C' to By-Law Number

File Number: D35-001-2025

### Certificate of Authentication

This is Schedule 'C' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

### Zoning By-Law 2022-62

### Schedule 3, Required Ground Floor Commercial

#### Legend

— Required Ground Floor Commercial to be added

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**Schedule 'D'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'D' to By-Law Number \_\_\_\_, passed this \_\_\_\_ day of \_\_\_\_ 2025.

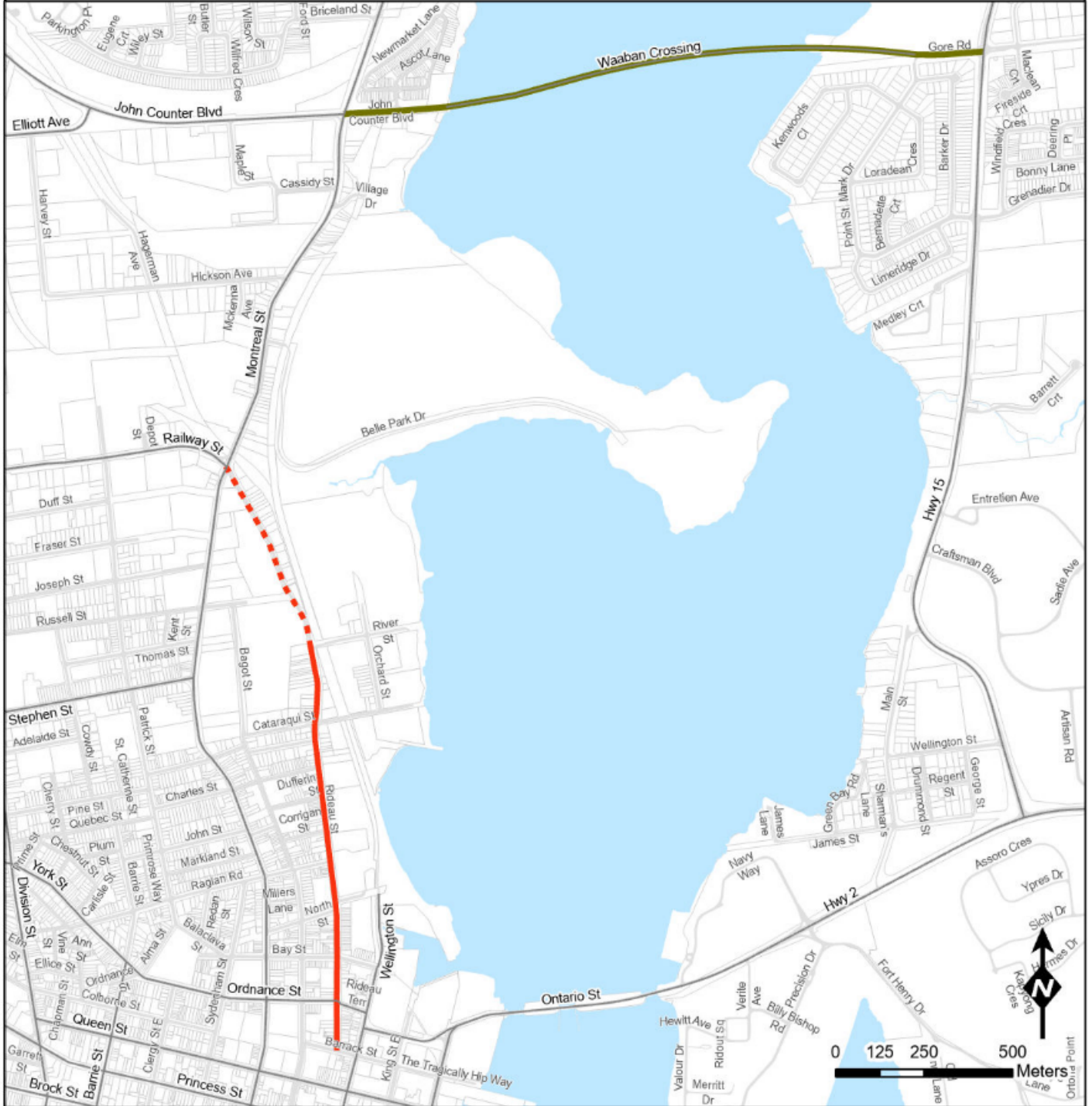
**Zoning By-Law 2022-62  
Schedule 4, Road Classification Urban**

**Legend**

- Road to be reclassified from Local to Arterial
- Road to be reclassified from Arterial to Collector
- Road to be reclassified from Local to Collector

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**Schedule 'E'  
to By-Law Number**




File Number: D35-001-2025

**Certificate of Authentication**

This is Schedule 'E' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule E, Exception Overlay**

**Legend**

-  Exception Overlay to be added (numbering as shown)
-  Legacy Exception Overlay to be removed
-  Legacy Exception Overlay to be adjusted as shown

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# Schedule 'F' to By-Law Number

File Number: D35-001-2025

Planning  
Services

## Certificate of Authentication

This is Schedule 'F' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

## Zoning By-Law 2022-62 Schedule F, Holding Overlay

### Legend



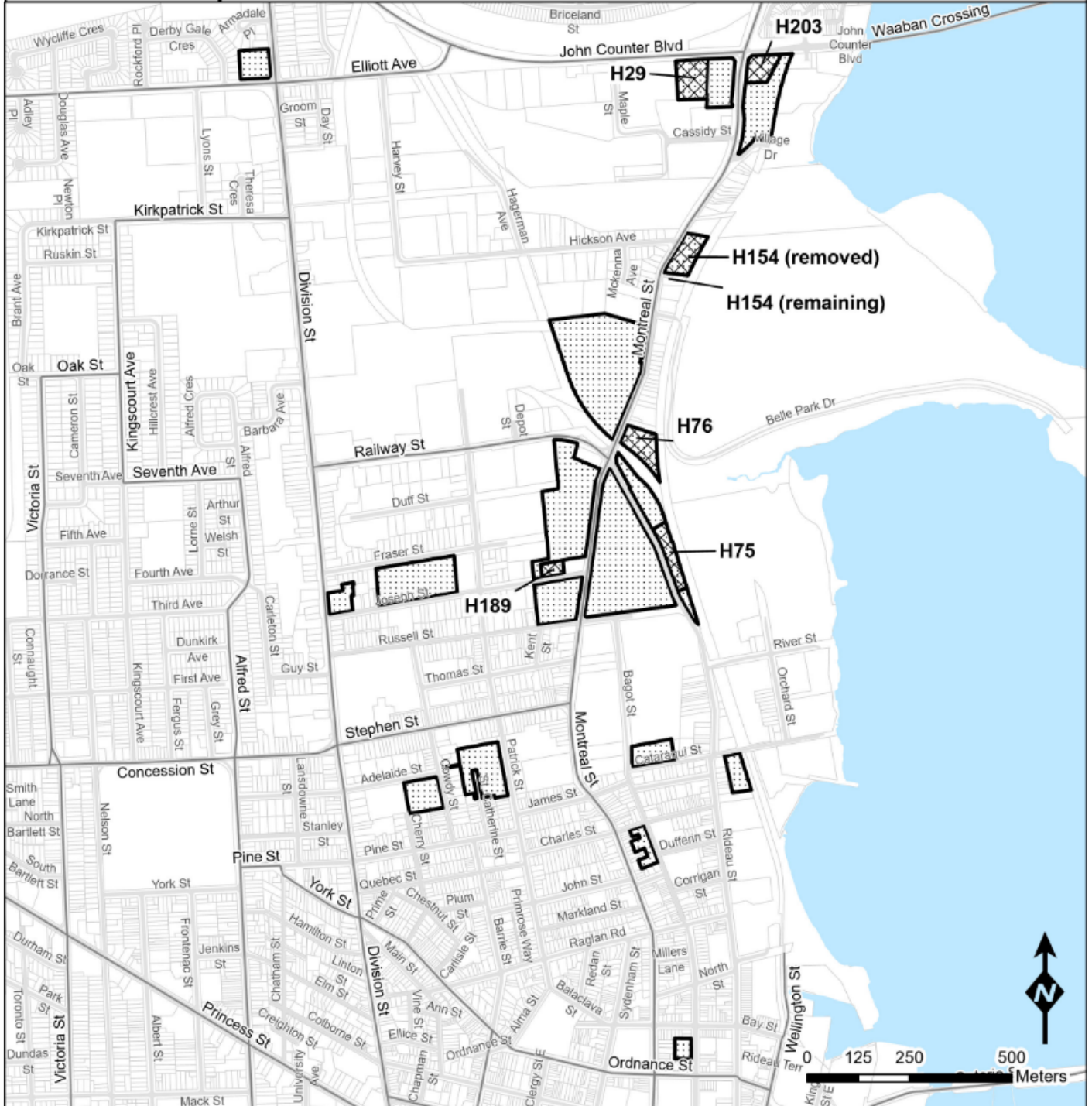
Lands to be added to  
Holding Overlay H243



Existing Holding Overlay to  
be removed (numbering as  
shown)

Mayor \_\_\_\_\_

Clerk \_\_\_\_\_





**Schedule 'G'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'G' to By-Law Number \_\_\_\_, passed this \_\_\_\_ day of \_\_\_\_\_ 2025.

**Zoning By-Law 2022-62  
Schedule H, Express Transit Area Overlay**

**Legend**

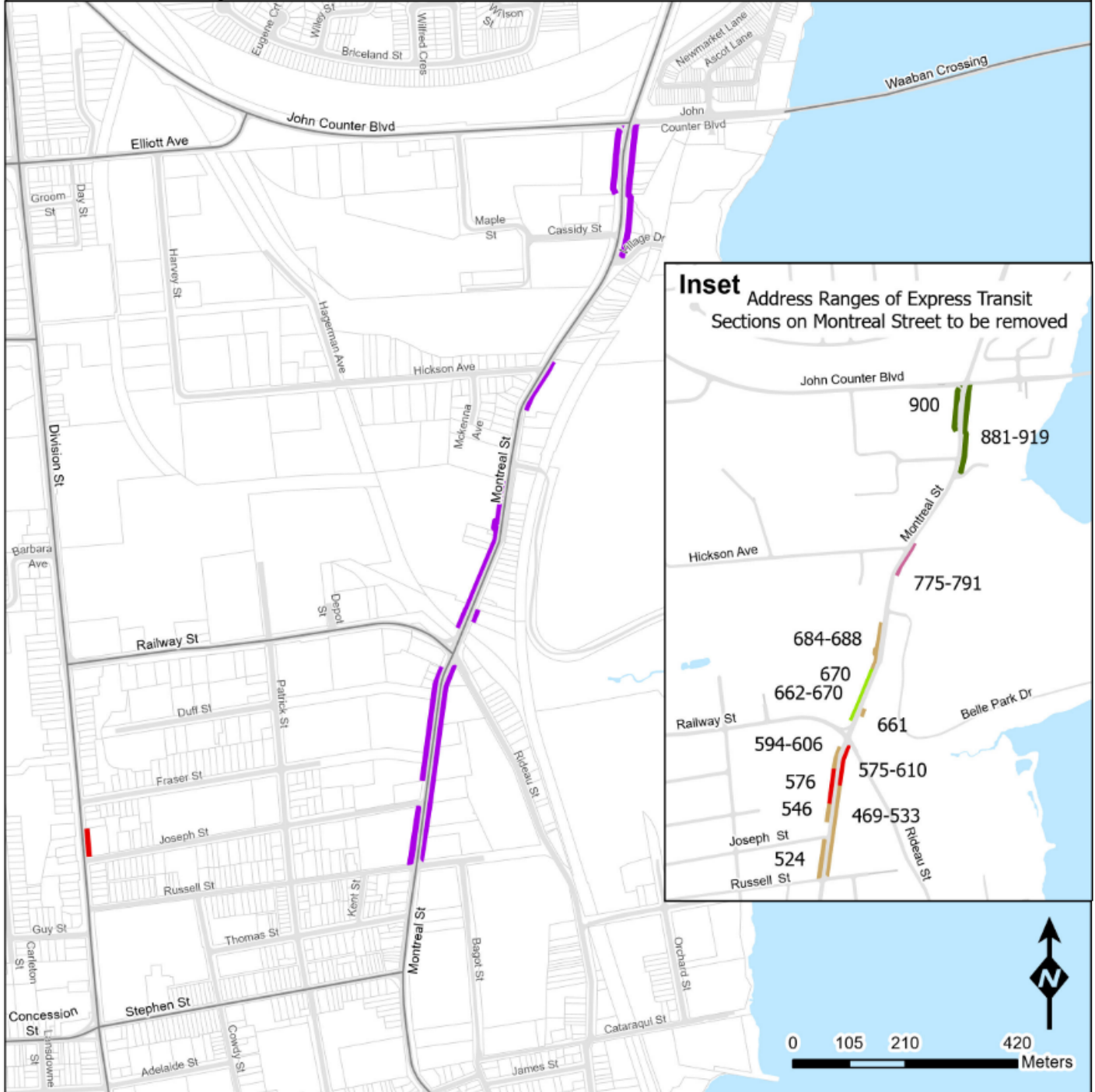
Express Transit Sections to be deleted from:

- Frontage of 541 Division Street
- Montreal Street

See Inset for detail

\_\_\_\_\_  
Mayor

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Clerk



**Zoning By-Law Number 8499**

Properties shown as 'N/A' or 'Not Subject to the Kingston Zoning By-Law' in Exhibit F (Existing Zoning) that are proposed to be rezoned to the MU1, MU2, MU3, URM8, URM11, or the CN Zone as part of the North King's Town project are proposed to be removed from Table 1 of Zoning By-Law Number 8499, titled, "Zones and Red Exceptions Subject to this By-law".





**Schedule 'A1'  
to By-Law Number**

File Number: D35-001-2025

**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'A1' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

**Kingston Zoning By-Law 8499  
Schedule A, Red Exceptions**

**Legend**

 Lands to be removed from Zoning By-Law Number 8499

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Mayor

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**Schedule 'A3'  
to By-Law Number**

File Number: D35-001-2025


**Planning  
Services**

**Certificate of Authentication**

This is Schedule 'A3' to By-Law Number \_\_\_\_\_, passed this \_\_\_\_\_ day of \_\_\_\_\_ 2025.

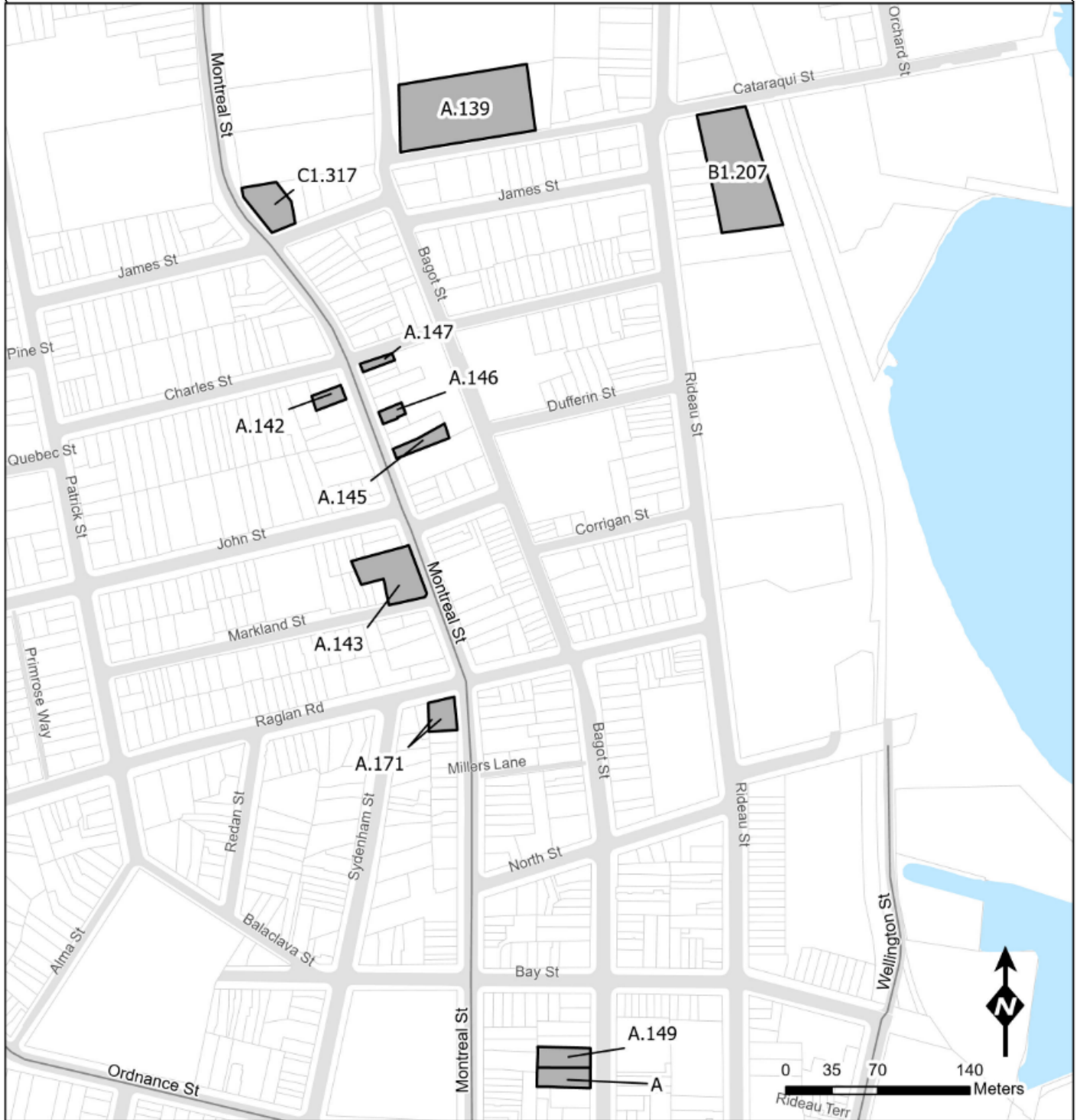
**Kingston Zoning By-Law 8499  
Schedule A, Red Exceptions**

Legend

 Lands to be removed from Zoning By-Law Number 8499

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Mayor

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Clerk



**North King's Town Specific Policy Area**  
**Conceptual Massing Models**



Figure 1 - Aerial view looking southeast towards the Montreal Street, Rideau Street and Railway Street intersection showing conceptual massing of intensification areas.



Figure 2 - Aerial view looking southeast towards the Montreal Street and John Counter Boulevard intersection showing conceptual massing of intensification areas.



Figure 3 – Aerial view looking south along Montreal Street, near the intersection with Rideau Street and Railway Street showing conceptual massing within intensification areas and use of building setbacks and stepbacks.



Figure 4 – Pedestrian view looking north along Montreal Street, near the intersection with Rideau Street and Railway Street showing conceptual massing within intensification areas and use of building setbacks and stepbacks.

## North King's Town Specific Policy Area

### Consistency with Provincial Planning Statement, 2024

The Provincial Planning Statement, 2024 (PPS, 2024) provides policy direction on matters of provincial interest related to land use planning and development, which are intended to be complemented by local policies addressing local interests. The *Planning Act* requires all municipal planning decisions to be consistent with the PPS, 2024.

Chapter 1 provides general information on how to read and use the PPS, 2024. Chapter 2 of PPS, 2024 provides several important policy directions related to planning for people and homes, housing, settlement areas, employment, and climate change. The proposed Official Plan and zoning by-law amendments for the North King's Town (NKT) project are consistent with this direction:

- Section 2.1 directs municipalities to provide an appropriate range and mix of housing options and densities to meet the current and future requirements. Further, an appropriate range and mix of land uses, housing options, transportation options, and parks and open house should be provided to foster complete communities. Municipalities are to plan for population and employment growth, as informed by provincial guidance. The NKT amendments seek to create a new Mixed Use designation, which would permit a wide range of residential, commercial and appropriate light industrial uses throughout much of NKT. This new designation will help to ensure an appropriate mix and range of uses can be provided to service and support the surrounding community. The identified intensification areas will permit a range of different residential uses to facilitate built form options for the community. The intensification areas create opportunities for a significant amount of residential and mixed use growth within the municipality.
- Section 2.2 focuses more specifically on ensuring an appropriate range and mix of housing options and densities are provided to meet the needs of current and future residents, including permitting and facilitating all types of residential intensification and redevelopment of underutilized sites. The NKT project promotes the redevelopment and intensification of large, vacant or underutilized parcels within the community for an appropriate range of residential uses, including townhouse, stacked townhouses and apartments. Other forms of ground-oriented residential uses, such as single-detached and semi-detached houses are permitted throughout much of the surrounding residential neighbourhoods. The densities included within the proposed zones have considered the potential for increased development costs due to remediation and infrastructure upgrades, however minor variances could be considered to support additional density, where appropriate.
- Section 2.3 contains policies intended to ensure settlement areas within municipalities are the focus of growth and development and are supported by

land use patterns that enable an efficient mix of uses, optimize existing and planned infrastructure, and support active, transit and freight transportation options. These policies broadly support intensification and redevelopment to support the creation of complete communities. Phasing should be considered to help align infrastructure improvements with growth pressures. The NKT amendments have identified opportunities to accommodate a significant amount of mixed use growth within the settlement area by redeveloping larger, vacant and underutilized properties. The policies promote an appropriate mix of uses to support the proposed redevelopment and surrounding community. All modes of transportation have been considered, with extensive focus on active transportation and transit opportunities.

- Section 2.8 promotes economic development and requires the protection of employment areas, especially those within proximity to major goods movement facilities or corridors. Municipalities are required to protect employment areas in settlement areas through a variety of measures, including the prohibition of residential uses, commercial uses and offices not associated with the primary employment use. Municipalities are to assess and update employment areas within Official Plans to ensure the designation is appropriate for the planned function of the area and may only remove lands from employment areas in limited circumstances. The NKT amendments propose to redesignate approximately 18 hectares of lands designated for industrial uses to the new Mixed Use designation, which is further discussed in the Employment Land Removal Rationale. The NKT amendments will protect the remaining employment areas by introducing a new zoning regulation to separate employment and sensitive land uses in accordance with the Province's D-series guidelines. Development applications would be required to submit appropriate technical studies to confirm land use compatibility between industrial and sensitive land uses.
- Section 2.9 includes the direction to reduce greenhouse gas emissions and prepare for the impacts of a changing climate by supporting compact, transit-supportive and complete communities, considering climate change in infrastructure projects, improving energy conservation and efficiency and promoting actions that protect the environment and improve air quality. The NKT amendments promote compact mixed use development in locations that are accessible to existing and planned transit and active transportation routes. The built form considered within these intensification areas is anticipated to promote energy efficiency due to modern building materials and the insulating value associated with shared interior walls common within apartments, townhouses and stacked townhouses. The NKT amendments promote the active transportation and transit opportunities and attempts to reduce automobile dependency through reduced minimum parking requirements.

Chapter 3 of PPS, 2024 focuses on infrastructure and facilities, including transportation, land use compatibility, utilities, and public spaces. The proposed amendments are consistent with this direction:

- Section 3.1 provides general policies relating to infrastructure and public service facilities, such as the need to provide efficient infrastructure and the requirement to optimize and re-use existing infrastructure prior to expanding infrastructure. The NKT Servicing Study identified several servicing infrastructure improvements necessary to support a full build out of NKT, which will assist in optimizing existing infrastructure within the urban boundary.
- Sections 3.2 and 3.3 indicate multi-modal transportation systems should be provided that are efficient and address the projected needs of the network and that municipalities should protect rights-of-way to meet current and projected needs. Further, the continuous linear characteristics of abandoned corridors should be maintained. The NKT Mobility Plan assessed the existing transportation network and provided a series of recommendations for each mode to help ensure an efficient future network. Recommendations have been provided to utilize portions of existing abandoned corridors to expand the active transportation network. Further, the NKT amendments seek to expand the protected road allowance width for portions of Montreal Street to ensure future development can be adequately serviced by a variety of transportation modes.
- Section 3.5 regards land use compatibility between major facilities and sensitive land uses to minimize and mitigate potential adverse effects from odour, noise and other contaminants, to ensure the long-term operational and economic viability of major facilities. The proposed amendments would implement separation distances in accordance with the Province's D-series guidelines. Development applications would be required to submit technical studies, such as noise assessments, to further confirm land use compatibility between industrial and sensitive land uses.
- Section 3.6 provides various policies relating to sewage, water and stormwater infrastructure and planning. The NKT project integrates servicing and land use considerations into all stages of the planning processes to ensure the Servicing Study identified the improvements required to support the contemplated growth within the identified intensification areas. As described below, the Servicing Study also provides recommendations for stormwater management, which are intended to be implemented through the City's development review process and supported through the proposed Official Plan amendments.
- Section 3.9 promotes healthy, active and inclusive communities by designing streets to be 'all ages and abilities', fostering connections for active transportation, providing access to public shorelines and minimizing negative impacts on natural areas. Most of the shoreline of the Great Cataragui River is held in public ownership, including several public waterfront parks and pathways. The NKT project has reassessed the Wellington Street Extension through Doug Fluhrer Park and recommends that this project be eliminated from the list of major road projects and instead be considered as a future active transportation project. The NKT Mobility Plan provides a series of recommendations to enhance the pedestrian and cycling networks, with several recommendations for 'all ages



and abilities' projects, which would be considered through the Integrated Mobility Plan.

Chapter 4 focuses on the wise use and management of resources, such as natural heritage, water and cultural heritage and archaeology. The proposed amendments are consistent with this direction:

- Section 4.1 provides direction on the protection of natural heritage features, including prohibiting development within specified features or adjacent lands, unless the development demonstrates no negative impacts. This direction is present within existing Section 6.1 of the Official Plan and would continue to apply to new development within the NKT intensification areas.
- Section 4.2 directs municipalities to protect, improve or restore the quality and quantity of water and restrict development near sensitive surface water features. The NKT Servicing Study provides a series of recommendations for innovative and enhanced stormwater management practices to improve the quality of runoff leaving development sites and reduce downstream flooding and erosion by controlling runoff rates and promoting increased infiltration. The proposed Official Plan amendment would reiterate the requirement for development applications to prepare stormwater management plans to the satisfaction of the municipality. Existing zoning regulations would continue to require new development to satisfy a minimum setback from waterbodies.
- Section 4.6 provides a series of policies that protect built heritage resources, cultural heritage landscapes, and archaeological resources. Municipalities cannot permit development or site alteration within lands containing these features, or adjacent lands, unless the resources will be conserved. The NKT Cultural Heritage Study provided analysis of the tangible and intangible cultural heritage within the area. The Official Plan already contains policies that protect designated built heritage, and adjacent lands, which would be reinforced by the proposed amendments. Further, the proposed amendment seeks to expand the St. Lawrence Ward Heritage Character Area to include lands with a greater concentration of potential built heritage resources and amended policies that seek to conserve the character of the area.

Chapter 5 focuses on protecting public health and safety, with focuses on natural and human-made hazards. The proposed amendments are consistent with this direction:

- The identified intensification areas are not located within any natural hazards and the proposed amendments do not modify existing Official Plan policies regarding prohibiting development within areas of natural hazards. Cataraqui Conservation has participated throughout the course of the NKT project and has no objections.
- Section 5.3.2 indicates that sites with contaminants in land or water should be assessed to ensure there will be no adverse effects. As it is anticipated that the identified intensification areas are brownfields, future development applications

will include appropriate studies to review previous land uses and assess the need for remediation prior to introducing sensitive land uses. The City can help support redevelopment and remediation of these lands through the Brownfield Community Improvement Plan.

Chapter 6 provides policies for implementation and interpretation of PPS, 2024, including coordination between various levels of government, agencies and community groups. The proposed amendments are consistent with this direction:

- Section 6.1 provides higher level guidance on implementation, such as the requirement to read the PPS, 2024 in its entirety to apply all relevant policies in each situation and the need for municipalities to identify matters of provincial interest within the Official Plans and update their zoning by-laws to be consistent with the Official Plan and PPS, 2024. The proposed NKT amendments assist in updating portions of the Official Plan and Kingston Zoning By-law to implement a variety of provincial interests relating to housing options and efficient delivery of services.
- Section 6.2 directs municipalities to undertake consultation with Indigenous communities and the broader public. NKT involved a series of engagement opportunities with the public and local Indigenous community, including an Indigenous representative on the NKT Community Working Group and hosting in-person and virtual consultation sessions in a variety of formats.

## North King's Town Specific Policy Area

### Conformity to the City of Kingston Official Plan

The following describes how the North King's Town (NKT) Official Plan and Zoning By-law amendments conform to the applicable policies of the Official Plan.

Section 2 of the Official Plan provides strategic policy direction, including guidance on sustainable development, climate change resiliency, appropriate locations for residential growth, land use compatibility principles, growth management and fundamental planning principles. The proposed NKT amendments conform to this section of the Official Plan:

- Policies relating to sustainable development are contained within Section 2.1. Most of the growth is anticipated to occur within the urban area of the municipality, which helps to conserve natural heritage features and reduce greenhouse gas emissions through reduced automobile use. This will be encouraged by allocating appropriate densities, identifying appropriate land use patterns, fostering transit and active transportation usage, maximizing investments in infrastructure and public amenities, and encouraging a mixture of land uses. These principles were considered throughout the NKT project and incorporated throughout the proposed amendments.
- Section 2.2 organizes the City into broad, structural elements identified on Schedule 2. NKT contains lands categorized as Business District, Housing District and Major Open Space. These categories reflect the broad use of these lands but are not land use designations. As discussed further in the Employment Land Removal Rationale, NKT proposes to redesignate certain lands from an employment designation to the Mixed Use designation. Corresponding amendments are proposed to Schedule 2 of the Official Plan to refine the boundary of the lands considered for Business District and those identified for Housing District in NKT.
- The principles of growth are outlined in Section 2.3, which include high-level policies to locate most of the growth within the urban area, promote intensification, protect employment uses, conserve cultural heritage, promote arts and culture, protect the Rideau Canal and natural heritage resources and promote active transportation. These principles were considered throughout the NKT project and incorporated throughout the proposed amendments.
- Policies relating to increasing residential density and promoting intensification are contained within Section 2.4. The NKT project has identified opportunities for a significant amount of residential and mixed use development to occur exclusive through intensification and redevelopment within the urban area. The proposed zoning amendments establish maximum residential densities that would serve to promote efficient delivery of services and infrastructure.

- Section 2.5 relates to phasing of municipal infrastructure and transportation. The recommendations contained within the NKT Mobility Plan and Servicing Study will be considered in the Integrated Mobility Plan and Water and Wastewater Master Plan, which will inform future municipal infrastructure investments within the NKT area. The NKT amendments include a holding overlay for all intensification areas, which will ensure appropriate servicing and transportation capacity.
- Section 2.6 identifies the appropriate locations for low-rise, mid-rise and high-rise residential development. These criteria were considered in the policies of Section 10H which will guide the development of NKT intensification areas.
- Land use compatibility considerations are outlined in Section 2.7, with development being required to demonstrate form, function and use being compatible with surrounding land uses. Various compatibility matters are identified, such as shadowing, loss of privacy, increased wind speed, and increased levels of light pollution, noise, odour, dust or vibration. Potential mitigation measures are identified, such as increased setbacks, building transitions, visual screening and appropriate location of driveways, service areas and activity areas. Section 2.7.5 aligns with the Province's D-series guidelines to identify spatial separation requirements between industrial uses and sensitive uses. The NKT amendments conform to these requirements by considering where mid-rise and high-rise buildings are permitted, identifying appropriate setbacks within zoning requirements, and establishing separation distances between industrial uses and sensitive uses within the zoning by-law. It is acknowledged that additional land use compatibility matters will be further addressed at the development application stage through the detailed design of the site and future Site Plan Control applications.
- Policies relating to climate change resiliency are found within Section 2.10 and indicate the City's intention to consider the potential impacts of climate change when planning for infrastructure and assessing new development and to support climate resilient architectural design of buildings. The NKT amendments encourage sustainable features, such as solar panels or green roof components, and the zoning regulations will regulate minimum landscape open space requirements and maximum lot coverage requirements. Stormwater management plans/studies submitted with development applications will be reviewed with climate change in mind.

Section 3 of the Official Plan identifies various land use designations within the City that form the basis of the intended function of those lands, including permitted uses and built form considerations. The proposed NKT amendments conform to this section of the Official Plan:

- Affordable housing policies are found within Section 3.2.18 and 3.2.19 and identifies a target of 25% of all new housing in the City be affordable to low- and moderate-income households. The City currently does not have the tools to

require development applications to include affordable housing units. The City encourages development applications to include affordable units by waiving various fees and reducing zoning regulations for this form of housing. It is anticipated that most of the mid-rise and high-rise development enabled through the NKT amendments would be subject to a Community Benefits Charge, which can be used to help support affordable housing programs within the City.

- The Residential designation permits various forms of low-rise, mid-rise and high-rise housing, in accordance with Sections 2.6 and 2.7, and smaller-scale neighbourhood commercial uses in accordance with Section 3.4.F. The policies and zoning regulations associated with the intensification areas within the Residential designation permit an appropriate range of residential uses and enable, but not require, neighbourhood commercial uses on the ground floor of developments. Building heights have been limited to four storeys, which is compatible with the surrounding low-rise residential neighbourhoods that currently permit three storeys.
- NKT proposes to create a new Mixed Use designation, which would be located under Section 3.4, which regards Commercial Uses. The majority of the lands to be redesignated in NKT will be placed within this new Mixed Use designation, which reflects the eclectic mix of uses that currently exists within NKT. The proposed designation would permit a range of residential uses, including mid-rise and high-rise residential, supporting commercial uses and the potential for compatible light industrial uses. This designation would provide greater flexibility to property owners and better enable future development applications to respond to market demands and fulfill community needs. The amendment proposes to redesignate certain lands from a commercial designation, in favour of the proposed Mixed Use designation, which permits a wider variety of uses and better aligns with the long-term intent of the intensification areas.
- Neighbourhood Commercial uses are outlined in Section 3.4.F and permit a variety of smaller-scale commercial uses intended to support surrounding residential neighbourhoods. These uses are not designated within the Official Plan but are intended to operate within the Residential designation subject to various criteria. The proposed zoning amendments for the intensification areas within the Residential designation enable ground floor neighbourhood commercial uses to create opportunities for development applications to better support surrounding residential neighbourhoods.
- Section 3.6 identifies the importance of employment lands, such as the General Industrial and Business Park Industrial designations, within the municipality. The Employment Land Removal Rationale provides further information on the proposed redesignation of approximately 18 hectares of employment land to the new Mixed Use designation within NKT. Several of the proposed intensification areas intended for future residential uses are in proximity to, or abut, employment lands. The proposed amendments will protect the industrial uses from incompatible uses by establishing new zoning regulations that require mi



Exhibit K -  
Consistency with PP

nimum separation between industrial uses and sensitive uses, in accordance with the Province's D-Series Land Use Compatibility Guidelines.

- The proposed amendments include minor expansions to the Open Space designation. These amendments seek to recognize existing, municipally-owned lands that are currently used for open space, including a baseball field and portions of the Waterfront Trail.
- Various intensification areas are in proximity to the Great Cataraqui River and associated natural heritage features. The proposed amendments would maintain all existing protections required under Section 3.9 of the Official Plan. Similarly, the proposed amendments maintain the existing boundary for the Environmental Protection Area designation and all existing protections required under Section 3.10 of the Official Plan.
- The proposed amendments seek to redesignate a small area of land from Harbour Area to Open Space. This should be considered a technical amendment as it serves only to recognize an existing portion of the Waterfront Trail and does not impact any active harbour operations.
- Section 3.17 of the Official Plan provides policies specific to certain lands. The proposed amendments remove certain lands from existing Site Specific Policies, to enable those lands to benefit from the proposed policies in Section 10H – North King's Town Specific Policy Area. The proposed policies enable a broader range of permitted uses and built form than the existing Site Specific Policies that apply to affected lands.

Section 4 of the Official Plan provides policies relating to physical infrastructure (such as water, wastewater, gas, electricity and stormwater) and transportation that must be considered to ensure efficient and fiscally sustainable services within the municipality. The proposed NKT amendments conform to this section of the Official Plan:

- Section 4.1 indicates that new development will only proceed if the City is satisfied that adequate servicing, roads and utilities are available, or can be made available to accommodate the development. Further, the timing of development may be constrained if adequate servicing or transportation capacity is not available. The NKT Mobility Plan and Servicing Study were prepared to assess the existing transportation and servicing infrastructure and identify potential improvements necessary to support the full build-out of the proposed intensification areas. The City will need to monitor development trends and advance corresponding capital budget requests to implement the recommendations from Mobility Plan and Servicing Study on an as-needed basis and in tandem with other planned capital works.

- Section 4.2 provides additional policies relating to the requirement for adequate municipal servicing for development within the urban boundary. Section 4.2.4 acknowledges development constraints for properties impacted by combined sanitary and storm sewers. The NKT Servicing Study acknowledges the presence of combined sewers and identifies several areas where separation will be required to accommodate full build-out of the intensification areas. The proposed zoning amendment would establish a holding overlay for the intensification areas, requiring development applications to demonstrate adequate servicing capacity prior to initiation.
- Section 4.3 identifies the requirement for development to manage the quantity and quality of stormwater runoff to a degree appropriate for the downstream receiving waterbody. The specific quantity and quality requirements that development applications would need to achieve are not provided within the Official Plan as it is best to identify these at the time of a development application to ensure current best practices and feasibility. The NKT Servicing Study provides a series of recommendations to ensure an enhanced level of treatment, acknowledging that runoff is received by the Great Cataraqui River, which includes fish habitat and a provincially significant wetland. The recommendations promote the use of innovative stormwater management techniques, such as Low Impact Development, which promotes greater infiltration to help slow and treat runoff.
- Section 4.6 provides the policies that guide transportation infrastructure and identifies the broad objective of fostering sustainability by reducing reliance on the automobile by promoting transit and active transportation opportunities before expanding the City's road infrastructure. These policies recognize that land use patterns play a role in transportation choices. Various policies speak to the requirement for sidewalks, active transportation connections, the benefits of pedestrian friendly streetscapes, the intention to develop cycling routes and pathways and intermodal improvements to better integrate active transportation and transit routes. This section also identifies the requirement to protect road allowances and secure road widenings along key roadways to protect for future needs.

The NKT Mobility Plan identifies a mode share that promotes increased participation in active transportation and transit to reduce automobile trips, supported by a series of recommendations. While many of the recommendations would be considered through the Integrated Mobility Plan, rather than the Official Plan, the current amendments seek to reduce automobile dependence through reduced minimum parking rates, promote active transportation by identifying 'Active Transportation Desire Lines' on Schedule NKT-1 and identifying other major pathways on Schedule 5 of the Official Plan. The proposed amendments increase the protected road allowance width along Montreal Street between Railway Street and Russell Street to ensure the intended development within this area is supported by all modes of transportation, including pedestrian sidewalks adjacent to the required ground floor commercial areas. Further, the zoning by-

law has established appropriate minimum front setbacks within this area to ensure ground floor uses, such as storefronts and patios, can be accommodated within the private realm and not impede pedestrian circulation.

This section of the Official Plan also identifies Major Projects (4.6.35) and includes two projects that together formed the 'Wellington Street Extension' (WSE), being a proposed two-lane road extending Wellington Street from Bay Street to John Counter Boulevard. As discussed within the NKT Mobility Plan, modelling has confirmed that the southern portion is no longer required as a roadway and should instead be protected for an active transportation corridor. The modelling indicates that the northern portion, corresponding to policy 4.6.35.g., provides mixed results and should be further analyzed through city-wide modelling associated with the Integrated Mobility Plan. The proposed amendments would remove the southern portion of the WSE (4.6.35.e) from the Major Roads Projects and from Schedule 4 of the Official Plan.

Section 5 of the Official Plan provides policies relating to protection of health and safety, which includes human-made hazards such as contaminated sites, and adverse effects created by land use incompatibility (noise, odour, vibration, air or water pollution). The proposed NKT amendments conform to this section of the Official Plan:

- Sections 5.11 through 5.14 relate to brownfield properties, which indicate that these former industrial lands must be appropriately remediated in accordance with Provincial requirements prior to establishing sensitive uses (such as residential). These policies will be applied to the future development of intensification areas within NKT.
- Various other policies within Section 5, such as 5.21 through 5.28, identify the requirement for studies to assess potential sources of incompatibility for sensitive uses such as noise, vibration, and other forms of pollution. These policies will be applied to future development of intensification areas within NKT.

Section 6.1 of the Official Plan provides policies relating to the protection of natural heritage features, such as waterbodies, wetlands and watercourses. The proposed NKT amendments conform to this section of the Official Plan:

- Section 6.1 identifies various natural heritage features, some of which are mapped on Schedules 7 and 8 of the Official Plan, while others cannot be mapped. The policies indicate whether development is prohibited within these areas or could be permitted subject to confirmation of no negative impacts. These policies will be applied to future development of intensification areas within NKT. The NKT amendments do not propose any amendments or modification to these existing policies.

Section 7 of the Official Plan provides guidance on built heritage resources, protected heritage properties and cultural heritage landscapes, including heritage character areas. This section also considers archaeological resources, arts and culture, public art,



museums and intangible cultural heritage. The proposed amendments conform to this section of the Official Plan:

- Sections 7.1 and 7.2 provide policies for the protection of built heritage resources and more specific guidance on properties protected under the *Ontario Heritage Act*. Generally, the City supports the conservation of built heritage resources and may require heritage impact assessments where construction, alteration, demolition to a property may impact a built heritage resource or is located adjacent to a protected built heritage resource. The NKT Cultural Heritage Study provided a series of recommendations relating to the protection of built heritage resources. The proposed NKT amendments acknowledge the presence of these resources and reinforce the need for heritage impact assessments or urban design studies, where appropriate, to ensure future development does not adversely impact these resources.
- Section 7.3.A provides policies to protect the Rideau Canal UNESCO World Heritage Site, which is adjacent to NKT study area along the Great Cataraqui River. These policies seek to conserve the cultural heritage landscape along the Canal by ensuring adjacent development satisfies minimum setbacks and appropriate viewscales. The City consulted with Parks Canada when considering locations of mid-rise and high-rise buildings. The existing policies within Section 7.3.A will be applied to future development of intensification areas in NKT.
- Section 7.3.C relates to heritage conservation districts, which are protected under Part V of the *Ontario Heritage Act*. There are no heritage conservation districts within NKT, however, the NKT amendments propose an expansion to the existing St. Lawrence Ward Heritage Character Area, identified within Section 7.3.D.5. The NKT amendments indicate the expanded St. Lawrence Ward Heritage Character Area may be appropriate for a future heritage conservation district study, which would assess whether all or a portion of these lands satisfy the necessary criteria to be designated as a heritage conservation district under the *Ontario Heritage Act*.
- Section 7.4 identifies the City's goal to recognize, protect and conserve archaeological resources. These existing policies will be applied to future development of intensification areas in NKT.
- Sections 7.5 through 7.7 relate to arts and culture, public art and museums and collections. The NKT amendments would serve to support arts and culture, public art and museums, by enabling uses such uses within the proposed designations and zones.
- Section 7.8 identifies the City's goal to partner with the community to develop strategies to conserve, develop and promote Kingston's unique stories and cultural experiences for residents and visitors. The NKT Cultural Heritage Study

provided recommendations on intangible cultural heritage that do not require Official Plan or zoning by-law amendments to be implemented.

Section 8 provides the City's policies on Urban Design to ensure the built environment is safe, efficient, accessible and harmonious. These policies recognize that the built environment contributes to an area's sense of place and significance within the community. The proposed amendments conform to this section of the Official Plan:

- NKT proposes to create a new Section 10H of the Official Plan that will provide policy guidance for the intensification areas identified on Schedule NKT-1 and include various policies to guide built form and urban design. The zoning regulations that would apply to these intensification areas are based on these built form and urban design policies, such as requiring appropriate front and rear setbacks, maximum building heights, the use of stepbacks above the fourth storey, and requirement for planting strips between intervening land uses.
- Section 8.8 identifies the protected views on Schedule 9 of the Official Plan. Development is required to maintain these views and are encouraged to incorporate facilities and amenities at locations where the Waterfront Pathway meets these protected views. The NKT amendments adjust the protected views on Schedule 9 by shifting the identified view along Raglan Road one block north to Corrigan Street, as the view along Raglan Road is already impacted by existing development.
- Section 8.10 relates to the City's review of development applications involving multiple buildings or multiple phases of development, such as requiring plans that demonstrate how the site will be developed cohesively. The existing policies will be applied to future development of intensification areas in NKT. The NKT amendments would include an additional policy relating to multi-phased projects, which would encourage the first phase should include buildings fronting onto a public street, which is intended to advance enhancements within the pedestrian realm.
- Section 8.11 recognizes Montreal Street as one of the 'Gateways' into the City, where development is intended to be characterized by a high standard of site design. The proposed policies within Section 10H are intended to supplement Section 8.11 by promoting an active pedestrian realm, supported by wide sidewalks and ground floor commercial uses, screening parking from prominent view, and locating development within mixed use nodes.

Section 9 includes various policies relating to administration and implementation of the Official Plan. The proposed amendments conform to this section of the Official Plan:

- Section 9.5.21 indicates the City may utilize a holding overlay within the zoning by-law where the future use of the land has been determined but where development would be premature until various conditions are met, such as technical studies confirming transportation or servicing capacity. The proposed

NKT amendment would utilize this section of the Official Plan and impose holding overlays for the various intensification areas relating to transportation, servicing or record of site condition requirements, as applicable.

- Sections 9.5.25 through 9.5.29 identify the City's ability to utilize Site Plan Control applications under the *Planning Act* to ensure safe and functional development. These policies will be applied to the future development of intensification areas within NKT.
- Section 9.8.8 identifies the Brownfield Community Improvement Area on Schedule 10. Recognizing that many of the intensification areas within NKT are likely to be brownfields, the NKT Financial and Implementation Plan recommends that the City extend the deadline of the Brownfield Community Improvement Plan by 10 years to help facilitate redevelopment of these intensification areas.
- Section 9.12 identifies consultation requirements for various development applications. The NKT project has involved an extensive public engagement and consultation program, as outlined in [Report Number PC-25-001](#). The Official Plan and Zoning By-law amendments were circulated in accordance with the *Planning Act* requirements.
- Section 9.12.3 identifies technical studies or plans that future development applications may be required to submit. These policies would continue to apply to future development applications within the NKT intensification areas. This will ensure the City is provided with sufficient information to assess and appropriately mitigate impacts resulting from future development.

**Oddie,Niall**

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**From:** Oddie,Niall  
**Sent:** December 6, 2024 12:02 PM  
**To:** [REDACTED]  
**Cc:** Osanic,Lisa; Agarwal,Sukriti; Van Vugt,Niki  
**Subject:** RE: Questions regarding the NKT Secondary Plan

Good morning Kerry,

Please see below for responses to your questions regarding the NKT project.

Please let me know if you have any further questions or comments.

Thanks  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
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The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** Ochej,Derek <[dochej@cityofkingston.ca](mailto:dochej@cityofkingston.ca)>  
**Sent:** December 5, 2024 12:54 PM  
**To:** Kerry Hill <[REDACTED]>  
**Subject:** RE: Questions regarding the NKT Secondary Plan

Hello,

The City of Kingston acknowledges and thanks you for your communication. Your correspondence will be included on the addendum for the Planning Committee meeting on Dec. 5 and has been shared with appropriate City staff. Please be advised that your correspondence may form part of the public agendas and minutes, and therefore will be made available to members of the public at the meetings, through requests, and through the website of the Corporation of the City of Kingston. Contact information, such as phone numbers and email addresses, will be redacted from documents shared with the public.

Thank you,



**Derek Ochej (he/him/his)**

(pronounced O'Shea)  
Deputy City Clerk  
City Clerk's Department

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The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** Kerry Hill <[REDACTED]>  
**Sent:** December 5, 2024 11:52 AM  
**To:** City Clerk <[CityClerk@cityofkingston.ca](mailto:CityClerk@cityofkingston.ca)>  
**Subject:** Questions regarding the NKT Secondary Plan

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Would you please submit my questions to the Planning Committee members and staff prior to this evening's meeting?

Thanks,

Kerry Hill  
K7L 4V1

**1. How will requests for zoning amendments be dealt with?**

Can the proposed zoning plan be made legally binding? Can the development fee be significantly increased for every storey above planned that the Planning Committee will have to assess? This is to deter developers from subverting the vision of the City and adding time to the assessment of an application.

The NKT project proposes various amendments to the Kingston Zoning By-law, which is a legal document. The *Planning Act* establishes processes for property owners to submit applications to amend the Official Plan and Zoning By-laws and requires municipalities to consider and issue a decision on these applications within a timely manner. The City of Kingston is permitted to charge application fees for these development applications, however the fees are intended to reflect the costs the municipality incurs in processing the applications – the fees are not intended to dissuade property owners from exercising rights established under the *Planning Act*.

**2. How will the buried or partially buried streams in the area be considered in the development of NKT?**

These are fundamental to the natural ecology and drainage of this low-lying area. If daylighted, these buried waterways would provide access to green space necessary for a truly vibrant community.

NKT did not consider the daylighting of buried watercourses as this was beyond the scope of the servicing study. The stormwater management chapter of the Servicing Study provides a series of lot-level recommendations to reduce run-off volume and improve the quality of run-off leaving development sites, which will help to improve the quality of water entering the Great Cataraqui River.

We currently do not have any accurate mapping identifying where buried watercourses would be located. I would be happy to pass along any project proposals to daylight any buried watercourses through municipally owned lands to our colleagues in Engineering for their consideration.

It is important to note that the daylighting of any watercourse would not necessarily transition that space into a public green space – if this were to occur on privately owned lands, it may remain as private property.

### **3. Why would the densest area be at an intersection we already have evidence is significantly impacted by traffic?**

The multiple towers planned for Mongeal St and John Counter are harbingers of traffic congestion and potentially ghettoization. The length of Montreal St could be home to more four to six story buildings with places for community gardens and active transportation paths. There seems to be no consideration of an outdoor life for anyone living in the cliffs of John Counter and Montreal.

The Mobility Plan identifies intersection improvements are planned for the Montreal Street and John Counter Boulevard intersection. The Mobility Plan also identifies several active transportation improvements (pedestrian and cycling) within this area that would improve connectivity to the K&P and Waterfront Trails and facilitate access to Belle Park, which would be redeveloped through the Belle Park Master Plan. Further, the proposed Official Plan identifies new lands being added to the Open Space designation on the north and south sides of the Waaban Crossing along the western shore.

The Planning Principles established within Phase 1 of NKT, which was approved by Council in 2017, included direction “to cluster new development to create hubs of activity and investment, and a compact, walkable built form” and “to plan for compact mixed-use intensification around nodes and corridors”. Intensification areas have been located along Montreal Street, however nodes of redevelopment have been identified at the Montreal Street / John Counter Boulevard and Montreal Street / Rideau Street / Railway Street intersections to contribute to efficient service delivery and cluster a mixture of uses in proximity to each other, including ground floor commercial uses, which will help to promote active transportation opportunities by reducing the need for residents to travel far distances for their daily needs.

### **4. The Plan states that affordable housing is theme 1.**

How will the Plan ensure that a fraction of developments actually have affordable housing in them? What percentage of units will be affordable?

Report Number PC-25-001 indicates that the NKT project aligns with the Council Strategic Plan objective to “Support Housing Affordability” and more specifically, the goal to “Promote increased supply and affordability of housing”. Supporting the affordability of housing is a broader objective to reduce the cost of all forms of housing and is different than Affordable housing – which generally relates to units where accommodation costs do not exceed 30% of annual household income for low and moderate income households.

The proposed NKT Official Plan policies do not speak to specific affordable housing targets as affordable housing is a City-wide concern, rather than focused within a small geographic area, such as NKT. Sections 3.2.18 and 3.2.19 of the Official Plan provides City-wide affordable housing policies and objectives. The City's new Official Plan project is investigating new tools and processes to help secure affordable housing units within the municipality – however, these are separate from the NKT project.

NKT seeks to contribute to housing affordability by identifying opportunities for a significant number of dwelling units that could be created within a wide variety of built form. NKT proposes to establish the principle of development for these housing opportunities through enabling Official Plan policies and Zoning By-law regulations, which means that development applications would only need to complete their detailed design studies. This process will help to create certainty for developers (which reduces risk) and greatly accelerate the development approvals process (which reduces cost). It is hoped that these initiatives will help incentivize development applications and the cost-savings associated with the more efficient process would translate to lower housing costs.

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** December 6, 2024 8:18 AM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: North King's Town - Community Meeting - December 5, 2024  
**Attachments:** Notice of Community Meeting.pdf

Good morning Lise,

Apologies for the location mix-up.

The November 12, 2024 email included the attached Notice of Community Meeting, which indicated the meeting would be hybrid – either in person at City Hall (216 Ontario Street) or virtually through a link from our Clerk's department. Additionally, the meeting was also live-streamed on the City's website.

A recording of the meeting will be added to the December 5, 2024 Planning Committee agenda package shortly. I would be happy to answer any questions or receive any feedback that you may have.

<https://www.cityofkingston.ca/council-and-city-administration/committees-and-boards/planning-committee/>

The project will be presented for a Public Meeting in early 2025 when formal Official Plan and Zoning By-Law amendments are submitted. That meeting will also be offered in a hybrid model with the in-person portion being held at City Hall.

Again, happy to answer any questions or receive feedback on the draft materials posted on DASH.  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
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---

**From:** Lise Melhorn-Boe <[REDACTED]>  
**Sent:** December 5, 2024 6:26 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** Re: North King's Town - Community Meeting - December 5, 2024

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Where was this meeting? I had assumed it was at the Legion so walked up there only to find no meeting. I am now home, and rereading this email, I cannot actually see a location.

Lise Melhorn-Boe  
Sent from my iPad

On Dec 2, 2024, at 8:14 AM, Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)> wrote:

Good morning,

Further to my November 12, 2024 email, the final drafts of the land use (including the proposed Official Plan and Zoning By-law amendments), mobility, cultural heritage and servicing studies for NKT will be presented for a Community Meeting at Planning Committee on **December 5, 2024 at 6pm**. The purpose of this Community Meeting is to seek feedback from the public and members of Planning Committee on the proposed amendments before a formal application for an Official Plan and Zoning By-Law Amendment is filed by the City.

Draft documents are available for review by accessing the [Development and Services Hub \(DASH\)](#) using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

A copy of the staff report, Report Number PC-25-001, is available on the Planning Committee agenda for December 5<sup>th</sup> and through the following link:

<https://pub-cityofkingston.escribemeetings.com/filestream.ashx?DocumentId=6145>

Should you have any questions or wish to be removed from this mailing list, please feel free to contact the undersigned or Sukriti Agarwal, Manager Policy Planning (copied).

Thanks  
Niall

<image001.png>

**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

<image002.png> <image003.png> <image004.png>

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**Oddie, Niall**

---

**From:** Oddie, Niall  
**Sent:** December 16, 2024 3:06 PM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti  
**Subject:** RE: Following up on NKT Community Meeting

Hi Paul,

Sorry for the delay in responding back to you, I was out of the office for much of last week.

While the areas around the Montreal Street/ John Counter Boulevard and Montreal Street/ Railway Street/ Rideau Street intersections are intended to be the main nodes of redevelopment, other intensification areas have also been identified based on their size, location, utilization and development interest. The intensification areas are shown on the proposed Schedule NKT-1, found within the 'Proposed Official Plan Modifications' document posted on DASH. Please visit our [DASH](#) platform to access the information using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

A copy of Staff Report PC-25-001, which provides an overview of the plan and process, and a recording of the presentation is available through the following link: <https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

I do not recall mentioning that this was the fifth iteration of the plan. The plan has evolved over time in response to public comments, development trends, lessons learned through other policy projects, and changes in provincial legislation. There have been various working groups meetings, public open houses and workshops along the way shaping the plan that was presented on December 5, 2024.

Please let me know if you have any further questions.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

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**From:** Paul D. Smith <[REDACTED]>  
**Sent:** December 10, 2024 9:54 AM  
**To:** Oddie, Niall <noddie@cityofkingston.ca>  
**Subject:** Re: Following up on NKT Community Meeting

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Hi Niall - following up on this message. I have a team mtg with Public Progress this afternoon, and I would appreciate the opportunity to speak to the questions I have asked.

Cheers,  
Paul

On Sat, Dec 7, 2024 at 1:24 PM Paul D. Smith <[REDACTED]> wrote:

Niall,

Great job with the Community Meeting on Tuesday. I have 2 follow-up questions for you.

1. On Tuesday, you focused on intensification in two areas along Montreal Street, at John Counter, and at Railway / Rideau. Are there other areas in the NKT Secondary Plan being considered for intensification?
2. Is there a consolidated report that contains a comprehensive discussion of all elements of this plan? Would that report be current? In the presentation you referenced that the outline you were sharing was a fifth iteration of the plan. Do I recall that correctly?

Cheers, and thank you.

Paul D. Smith  
[REDACTED]

[REDACTED]

December 16, 2024

Via Email

Ms. Sukriti Agarwal, Manager  
Policy Planning, Planning Services  
City of Kingston  
Kingston, ON

Mr. Joel Konrad, Manager  
Heritage Planning, Heritage Services  
City of Kingston  
Kingston, ON

Re: North King's Town: NKT Cultural Heritage Study, Reports PC-25-001  
and HP-25-002

Dear Sukriti and Joel

The Frontenac Heritage Foundation has monitored this project since 2016, and provided detailed comments that year, and in 2018 and 2019. We wish to provide comments on PC-25-001 as well as the information report containing the North King's Town Cultural Heritage Study under report HP-25-002. The Foundation has yet to review the two remaining reports - the financial and implementation plans, which are to be released in 2025.

The FHF supports the recommendations from the Cultural Heritage Study report that require redevelopment in proximity to the enlarged St. Lawrence Ward Heritage Character Area to undertake heritage impact assessments to demonstrate that no negative impacts will affect adjacent heritage resources. The Foundation also supports the recommendation to undertake a study under the OHA to determine whether a heritage district is warranted under the Act.

We also support the requirement to submit urban design studies to demonstrate how development, particularly infill and intensification, will incorporate identified heritage characteristics from the surrounding area into new proposals to blend the old and the new. Both of these requirements support the Foundation's mandate to preserve and protect heritage structures and the proposed enlarged Heritage Character Area.

The FHF is pleased to see the Outer Station is recognized as the historic cultural and economic nexus of the NKT area as a key factor in the overall 19th and early 20th

century growth of the city. While the Outer Station site is shown within an intensification area, we believe due to the archaeological past, and the cultural/industrial history of the site, its redevelopment should not be diminished by excessive on-site intensification.

While the existing main structure is in poor condition, adaptive reuse and recognition of its potential should be foremost in the minds of any redevelopment of the site. We recommend that Stage 1 and 2 archaeological assessments of the property be completed in the short term to ensure that significant archaeological resources on site will be appropriately conserved. The archaeological assessment should inform the required Heritage Impact Statement on the property. We support a Conservation Plan (as defined in draft S. 10H.3.2) that conserves/protects the built and cultural heritage assets of the Outer Station property. As noted in the proposed policy, the Foundation does not support moving of any of the structures off the site, nor do we support the relocation of the structure elsewhere on the site as part of any adaptive reuse strategy. We note that the Rockwood Conservation Area is a good example of ruins being repurposed for special events, and such a possibility could act as a focus for development in the area. (Here is a link to a related article: <https://publichistoryinpractice.ca/2023/03/24/rockwoods-hidden-gem-harris-woolen-mill-ruins/> )

The FHF supports further review, analysis and assessment of the Division Street node at John Counter Blvd., and the proposed node at Montreal and Railway Streets. Comments made at the Planning Committee meeting on December 5, 2024 suggested that the nodes are too dense for these two locations, and that they might resemble the Williamsville area if they are developed as proposed. This would be unfortunate.

Thank you for your time and effort on this project.

Sincerely,

*Original signed by Kristine Hebert*

Kristine Hebert  
President, Frontenac Heritage

Cc. Members of Kingston Heritage Properties Committee

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** December 19, 2024 9:54 AM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: deadline for North King's Town plan cooments

Good morning Vicki,

We anticipate submitting the formal Official Plan and Zoning By-law amendments in early 2025. To ensure that we have sufficient time to consider comments and make any necessary revisions to the draft materials presented at the Community Meeting, we would appreciate questions and comments by January 10, 2025.

This being said, you may submit questions and comments at any time before Council makes a decision on the formal applications.

Thanks,  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

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---

**From:** [REDACTED] <[REDACTED]>  
**Sent:** December 18, 2024 4:07 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** deadline for North King's Town plan cooments

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Hello Niall:

What is the timeframe for submitting questions and comments on the North King's Town report/

Thank you for letting me know.

Vicki

Vicki Schmolka



**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** December 20, 2024 10:55 AM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: City File number D01-011-2024 - 27 Bay Street

Hi Debbie,

Following up on our phone call this morning, you received this notice because your lands (27 Bay Street) are located within 120 metres of lands where changes are proposed - but no changes are directly proposed for your lands. As indicated, most of the changes within this area are related to a proposed expansion of the existing St Lawrence Ward Heritage Character Area. You can review the draft policies associated with the proposed expanded St Lawrence Ward Heritage Character Area on our DASH platform using the following steps:

<https://aca-prodca.accela.com/kingston/Welcome.aspx?TabName=Home>

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

Please let me know if you have additional questions. As mentioned, I'll be out of the office from 3 pm today and returning on January 6, 2025, but I would be happy to further discuss with you in the new year.

Thanks  
Niall

Niall Oddie M.PI, MCIP, RPP (he/him/his) Senior Planner Planning Services

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-----Original Message-----

From: Debbie Earl <[REDACTED]>  
Sent: December 20, 2024 9:49 AM  
To: Oddie,Niall <noddie@cityofkingston.ca>  
Subject: City File number D01-011-2024

Caution: This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.



Good morning Niall, I just left a voice mail fir you. My name is Debbie Earl and I own a townhouse at 27 Bay St. Because of the postal strike, I just received a notice about a community mtg that was to take place in Dec. 5 th , so , only having received the letter yesterday, obviously that mtg is over and I fid not attend. I want to clarify how my property, on the map as part of Lands to be redesignated, will be redesignated and what that means. All around me the lands seem to be designated to be added to the Heritage Character Area, but I am not sure where my property falls on this map. Can you clarify this gor me please? Thank you Niall.

Sent from my iPhone

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** December 20, 2024 2:47 PM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: North King's Town Project Meeting Notice

Hi Ed,

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

We are in the process of collecting feedback on the draft materials and will be submitting formal Official Plan and Zoning By-law amendments in early 2025. Once these formal applications are submitted, a Public Meeting will be held at Planning Committee, where the information will be presented, and opportunities to ask questions will be provided. Notice of the meeting will be distributed in accordance with the *Planning Act* (owners of land impacted by the changes and owners of lands within 120 metres of the changes) and to the project mailing list. Further, notice will also be placed within the Whig Standard, the project website and the City calendar

Several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

We anticipate submitting the formal Official Plan and Zoning By-law amendments in early 2025. To ensure that we have sufficient time to consider comments and make any necessary revisions to the draft materials presented at the Community Meeting, we would appreciate questions and comments by January 10, 2025. This being said, you may submit questions and comments at any time before Council makes a decision on the formal applications.

I will be on vacation until January 6, 2025, but I would be pleased to meet with you to discuss the project in greater detail and answer any further questions that you may have.

Thanks  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

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Planning Services

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---

**From:** Ed Horba <[REDACTED]>  
**Sent:** December 20, 2024 10:48 AM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** North King's Town Project Meeting Notice

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Niall

I received notice by mail on December 19, 2024 of a community meeting for the project. As my notice was received after the December 5th, 2024 meeting I trust that there will be another meeting so that all those that did not get the notice in time will be able to view the information and to comment. I would like to provide comments but have not seen any of the project details.

Can you please provide details regarding the public comment process and timelines since there has been a delay due to postal strike and any other information on the project. (I am not comfortable using DASH).

Thank you  
Ed Horba

**Oddie, Niall**

---

**From:** North King's Town Secondary Plan  
**Sent:** December 20, 2024 2:33 PM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti; Van Vugt, Niki  
**Subject:** RE: North King's Town Secondary Plan Notice of Dec 5, 2024 Meeting, received December 19, 2024 & Questions

Good afternoon Felix,

There were a limited number of residents at the December 5, 2024 Community Meeting for the North King's Town project. A copy of the staff report and a recording of the presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

We are in the process of collecting feedback on the draft materials and will be submitting formal Official Plan and Zoning By-law amendments in early 2025. Once these formal applications are submitted, a Public Meeting will be held at Planning Committee where the information will be presented and opportunities to ask questions will be provided. Notice of the meeting will be distributed in accordance with the *Planning Act* (owners of land impacted by the changes and owners of lands within 120 metres of the changes) and to the project mailing list. Further, notice will also be placed within the Whig Standard, the project website and the City calendar.

Several reports associated with the North King's Town project are available on our [DASH platform](#) for review – if you have not already done so. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

The document “Proposed Official Plan Modifications” contains proposed policies for the enlarged St Lawrence Ward Heritage Conservation Area – these proposed policies would replace the existing policies found within Section 7.3.D.5 of the Official Plan. These proposed policies would discourage the demolition of buildings within this area and encourage adaptive reuse of buildings to help conserve the character of the area. Further, development applications requiring *Planning Act* approval may be required to submit heritage impact assessments or urban design studies to mitigate the impacts of development on the character of the area. The proposed policies would also identify the intention for a future study to determine if the area satisfies the criteria to be designated as a heritage conservation district under the *Ontario Heritage Act*. To be clear, the proposed policies speak to a future study and are not recommending that the area be designated at this time.

I will be on vacation until January 6, 2025 but would be pleased to meet with you to discuss the project in greater detail and answer any further questions that you may have.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

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Planning Services

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---

**From:** Félix Lavoie <[REDACTED]>

**Sent:** December 19, 2024 3:01 PM

**To:** Agarwal,Sukriti <sagarwal@cityofkingston.ca>; North King's Town Secondary Plan <nktplan@cityofkingston.ca>; Van Vugt,Niki <nvanvugt@cityofkingston.ca>

**Subject:** North King's Town Secondary Plan Notice of Dec 5, 2024 Meeting, received December 19, 2024 & Questions

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Good afternoon Skuriti, Niall, and Niki

Given the Canada Post strike which just ended, we received this notice for the meeting two weeks ago, today. Could you advise if there will be another community meeting, or what the attendance was at this meeting given it is likely that no one in the proposed lands to be redesignated received a notice sent during or right before the Canada Post strike?

I am also curious as to whether you could provide additional details as to the meaning of being added to the St. Lawrence Ward Heritage Character area. A quick search of the official plan only references it at 7.3.D.5 and 10A.4.6, but it would be helpful to the public to be explained what these changes mean in lay-man's terms. I don't see how the public would be able to understand this notice from a substantive point of view.

Best,

--

Félix

**Oddie,Niall**

---

**From:** Jaynes,Janet  
**Sent:** December 20, 2024 12:43 PM  
**To:** [REDACTED]  
**Cc:** Ridge,Gregory; Oddie,Niall  
**Subject:** RE: North King's Town Project

Hello Laurel,

Thank you for your email.

Notice of the Community Meeting was sent by mail to all property owners (according to the latest Assessment Rolls) subject to the proposed Official Plan or Zoning By-Law amendments and to all property owners within 120 metres of these properties, as well as to all individuals on the project mailing list for the North King's Town (NKT) project and posted on the [NKT Get Involved Page](#).

In addition, a notice of the Community Meeting was provided by advertisement in The Kingston Whig Standard 20 days in advance of the Community Meeting.

A statutory Public Meeting will be scheduled at a later date and further notice regarding the Public Meeting will be provided in accordance with the Planning Act and to all individuals on the project mailing list for the NKT project.

Additional information about the proposal can be viewed by accessing the Development and Services Hub (DASH) at [https://www.cityofkingston.ca/DASH\(External](https://www.cityofkingston.ca/DASH(External) link).

I have copied Niall Oddie, Senior Planner, who is leading this work in case you have additional questions related to this project.

All the best,



**Janet Jaynes**

City Clerk  
Office of the City Clerk

City of Kingston  
City Hall  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 1262  
[jjaynes@cityofkingston.ca](mailto:jjaynes@cityofkingston.ca)

-----Original Message-----

From: Laurel Aziz <[REDACTED]>  
Sent: December 20, 2024 12:11 PM

To: Jaynes, Janet <jjaynes@cityofkingston.ca>  
Cc: Ridge, Gregory <gridge@cityofkingston.ca>  
Subject: North King's Town Project

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Hi Janet:  
Hope all is well at City Hall.

I am writing today about a letter I received in the mail yesterday regarding the North King's Town Project.

Following the Canada Post strike, the letter arrived in my mailbox on December 19. It is dated from your office on November 15. The Canada Post strike began, with at least a week's notice, on November 15. The Community Meeting promoted in the letter has since passed, as it was scheduled for December 5.

Can you please explain how stakeholders living in the affected area were expected to learn about the December 5 meeting when the letter was put in the mail on or after the first day of the Canada Post strike?

In addition, I would be interested in knowing if there are opportunities now to have input included as part of the December 5 meeting given that the City failed to inform the community in a fair and reasonable timeframe?

Sincerely,  
Laurel Aziz

**Oddie, Niall**

---

**From:** North King's Town Secondary Plan  
**Sent:** December 20, 2024 2:04 PM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti  
**Subject:** RE: North Kings Town Secondary Plan Updates

Hi Ryan,

I've added you to the distribution list for the North King's Town project.

177 Clergy Street East is not identified as 'unprotected properties of heritage interest' – but the properties to the north and to the south have been identified; I've marked up a copy of the mapping below, with the red arrow pointing at 177 Clergy Street East. These are not the same as heritage designations, just an indication of a property that may be further reviewed or considered in the future.

A copy of the Cultural Heritage Study is available on our [DASH platform](#) for review – if you have not already done so. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

The proposed text changes for the St Lawrence Ward Heritage Character Area are provided on pages 7 and 8 within the "Proposed Official Plan Modifications" document, also available on DASH.

I will be out of the office over the holidays but I would be happy to answer any further questions you may have on the materials when I return on January 6, 2025.

Thanks  
Niall





**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
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[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



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---

**From:** Ryan Low <[REDACTED]>  
**Sent:** December 19, 2024 3:01 PM  
**To:** North King's Town Secondary Plan <nktplan@cityofkingston.ca>  
**Subject:** North Kings Town Secondary Plan Updates

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Hi Niall,

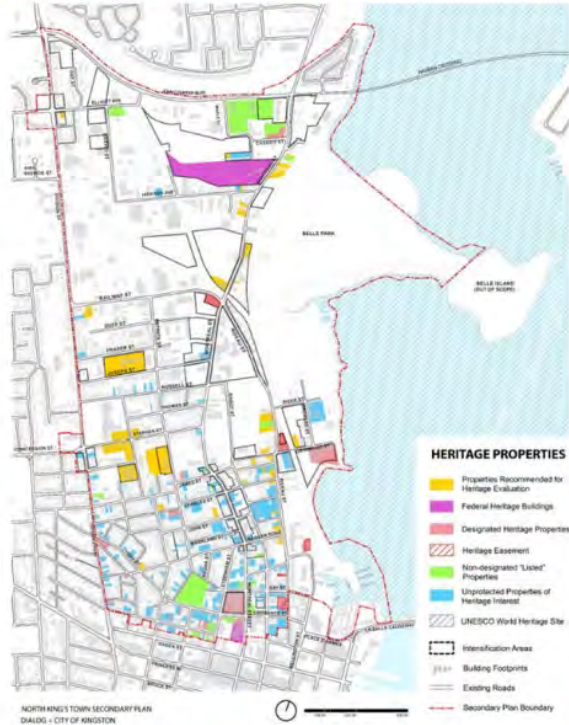
Can I please be added to the distribution list for future updates for the North Kings Town Secondary Plan?

I am also interested if there is more information available on the Heritage Area Conservation Studies as I live in the area of future expansion to the St Lawrence Heritage Area Conservation. I noted in some of the most recent material (image below for ref) my house might be identified as an Unprotected Property of Heritage interest, is there any way I can confirm this? My address is 177 Clergy Street East.

Cheers,

Ryan Low

**North King's Town** **SECONDARY PLAN** **Cultural Heritage Heritage Properties**



NORTH KING'S TOWN SECONDARY PLAN  
DRAFT - CITY OF KINGSTON

**Non-Designated "Listed" Properties**

Section 27 of the *Ontario Heritage Act* allows municipalities to "List" or inventory properties of cultural heritage value. These properties are not designated and therefore are not afforded the same level of protection as a designated property. While a Heritage Permit is not required for alterations to the property, owners are always welcome to seek staff advice. An owner of a "Listed" property is required to give Council 60 days notice of their intention to demolish a building, or portion of a building, on the property. The City of Kingston is currently reviewing the "Listed" properties throughout the municipality for potential designation.



**Unprotected Properties of Heritage Interest**

Properties located within this category are those which have been identified by the City of Kingston as being potential cultural heritage resources. These properties do not currently hold any protections against future redevelopment but should be reviewed for potential "Listing" or formal designation in the future. Designation can be pursued provided a formal *Planning Act* application has not been accepted by the municipality.



**Designated Heritage Properties**

The properties within this category have existing *Ontario Heritage Act* designations, which means that Council

**Federal Heritage Buildings**

There are two properties within NKT which hold federal heritage designations. Any proposed

**Properties Recommended for**

...

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** December 20, 2024 3:02 PM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: Meeting on December 5th

Hi Liza,

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link.

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

The North King's Town (NKT) project has identified a series of intensification areas where low, mid and high-rise residential and mixed use developments may be appropriate. The project also identifies lands appropriate for a new "Mixed Use" designation within the Official Plan, where a mixture of residential, commercial and limited light industrial uses may be appropriate. The project also contemplates an expansion to the existing St Lawrence Ward Heritage Character Area. Official Plan and Zoning By-law amendments are contemplated to implement these proposed changes. The NKT project is supported by a Cultural Heritage Study, Mobility Plan and Servicing Plan, while a Financial and Implementation Plan is in the process of being finalized.

Several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

We anticipate submitting the formal Official Plan and Zoning By-law amendments in early 2025. To ensure that we have sufficient time to consider comments and make any necessary revisions to the draft materials presented at the Community Meeting, we would appreciate questions and comments by January 10, 2025. This being said, you may submit questions and comments at any time before Council makes a decision on the formal applications.

I can confirm that 125 Rideau Street received notice of the project because this property is located within 120 metres of lands where changes to the Official Plan or Zoning By-law are proposed – however no changes are proposed for 125 Rideau Street directly.

I will be on vacation until January 6, 2025, but I would be pleased to meet with you to discuss the project in greater detail and answer any further questions that you may have.

Thanks  
Niall



**Niall Oddie M.Pl, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

---

**From:** Liza Lawson <[REDACTED]>  
**Sent:** December 19, 2024 8:26 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** Meeting on December 5th

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Good evening, Niall

We just received notice of this meeting to discuss file number D01-011-2024 for our area. I guess because of the postal strike we were not aware of the meeting. Can you please help us understand the amendments. Many thanks, Liza Lawson

125 Rideau St, Kingston, ON K7K 7B2

**Oddie, Niall**

---

**From:** North King's Town Secondary Plan  
**Sent:** January 7, 2025 9:09 AM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti  
**Subject:** RE: NKT Cultural Heritage Study request

Hi Andrea,

No worries – glad that question has been cleared up.

At this point, we are aiming for a public meeting date at the end of February or early March.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
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216 Ontario Street Kingston, ON K7L 2Z3  
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---

**From:** Andrea Gummo <[REDACTED]>  
**Sent:** January 7, 2025 8:12 AM  
**To:** North King's Town Secondary Plan <nktplan@cityofkingston.ca>  
**Subject:** Re: NKT Cultural Heritage Study request

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Hi Niall! Oh I see, thanks so much. I realized I was reading Schedule 9 wrong, appreciate the clarification and apologies for that.

Is there a rough timeline on when the amendments might be brought forward?

Thanks again  
Andrea

On Mon, Jan 6, 2025 at 2:08 PM North King's Town Secondary Plan <[nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca)> wrote:

Hi Andrea,

The CHS provides recommendations within subsections 5.1 (Sub-Area #1 Division Street Corridor), 5.4 (Sub-Area #3 Montreal Street Corridor), 5.5 (Sub-Area #4 Bagot Street Neighbourhood), 5.7 (Sub-Area #6 McBurney Park Neighbourhood), and 5.8 (Sub-Area #7 Patrick Street Neighbourhood) to undertake a heritage conservation district study within portions of these sub-areas to determine if the area satisfies the criteria for a formal HCD designation under the Ontario Heritage Act. The recommendations contained within these sub-sections also mention that in the interim the City should expand the boundary of the St Lawrence Ward Heritage Character Area to match the anticipated boundary of the future HCD study area.

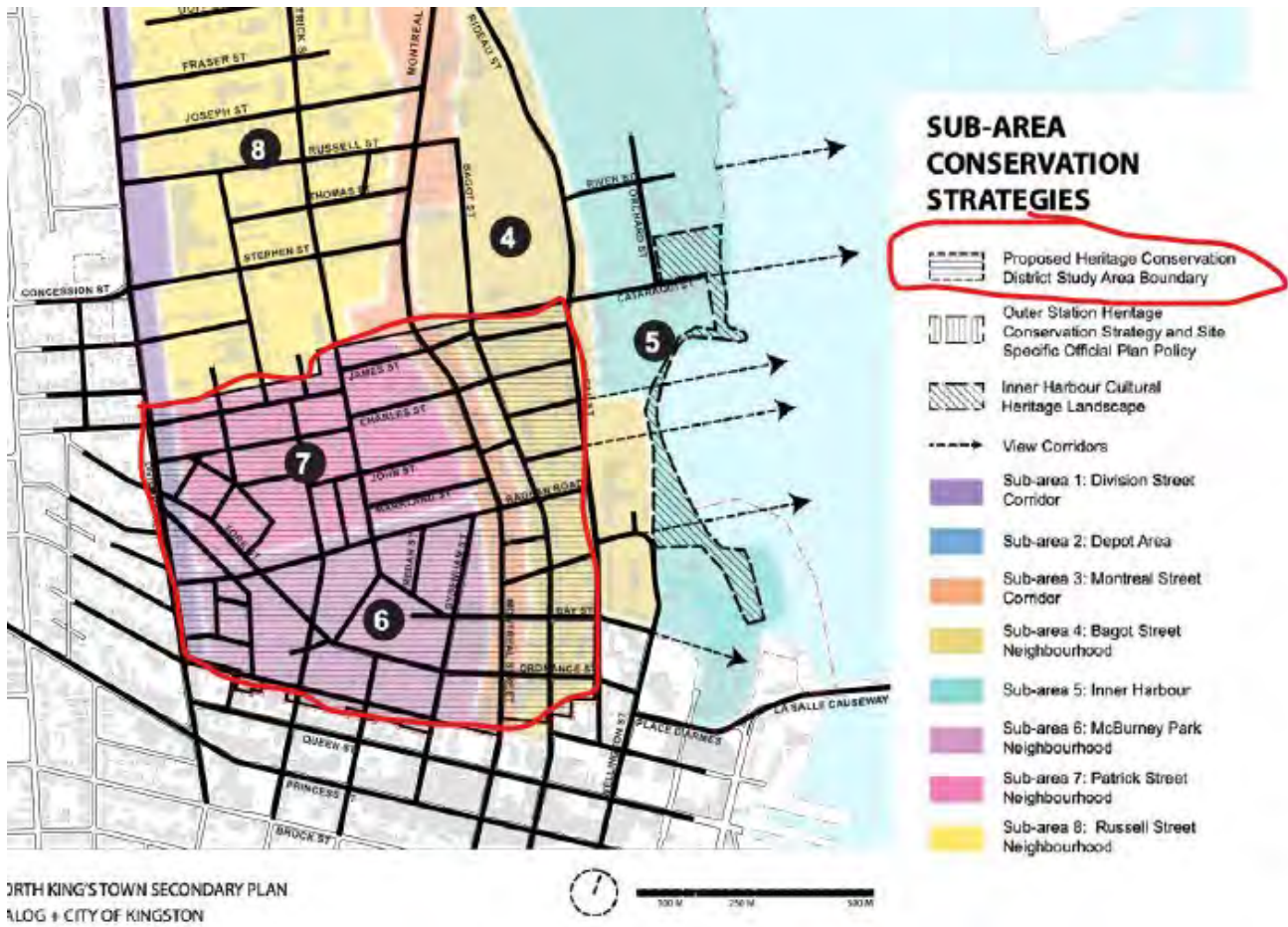
The “Conservation Strategies” figure shown on page 108 of the CHS identifies the recommended “Proposed Heritage Conservation District Study Area Boundary”. This same boundary has been carried forward into the proposed OPA to expand the boundary of the St Lawrence Ward Heritage Character Area (Schedule 9 modifications). McBurney Park and surrounding lands, northward to Pine and James Streets between Division and Bagot Streets, are included within these maps.

The lands immediately north of McBurney Park have not been identified for intensification. These lands are located within the existing St Lawrence Ward Heritage Character Area and are proposed to remain within the expanded St Lawrence Ward Heritage Character Area.

I hope this resolves your question – however, please feel free to give me a call to further discuss.

Thanks

Niall



**Niall Oddie M.Pl, MCIP, RPP (he/him/his)**

Senior Planner

Planning Services

City of Kingston

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216 Ontario Street Kingston, ON K7L 2Z3

613-546-4291 extension 3259

[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)





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---

**From:** Andrea Gummo <[REDACTED]>  
**Sent:** January 6, 2025 11:54 AM  
**To:** North King's Town Secondary Plan <[nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca)>  
**Subject:** Re: NKT Cultural Heritage Study request

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi Niall! Hope you're doing well. Thanks so much, I've found the study.

By chance, would you be able to point me to the best place to see the rationale for the St Lawrence Ward HCA new boundaries? Specifically I'm wondering about the north side of McBurney Park and why it was left out. I don't think it's been recommended for intensification, unless I missed something?

Thanks again

Andrea

On Mon, Jan 6, 2025 at 10:36 AM North King's Town Secondary Plan <[nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca)> wrote:

Hi Andrea,

Unfortunately, I cannot send copies of the NKT Cultural Heritage Study due to file size issues – but the most recent version is available for review on [DASH](#). You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

We are available to discuss any questions or comments that you may have.

Thanks!

Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner

Planning Services

City of Kingston

Located at: 1211 John Counter Boulevard



216 Ontario Street Kingston, ON K7L 2Z3

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---

**From:** Andrea Gummo <[REDACTED]>  
**Sent:** January 6, 2025 10:23 AM  
**To:** North King's Town Secondary Plan <[nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca)>  
**Subject:** NKT Cultural Heritage Study request

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello!

Could I please request the most recent version of the North King's Town Secondary Plan Cultural Heritage Study?

Thanks so much

Andrea

**Oddie, Niall**

---

**From:** Oddie, Niall  
**Sent:** January 7, 2025 9:22 AM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti  
**Subject:** North King's Town

Hi John,

I received your voicemail seeking information about the North King's Town project and the proposed expansion of the St Lawrence Ward Heritage Character Area. I left a voicemail at the number provided but thought that I'd send an email as well.

NKT is not recommending a heritage designation under the Ontario Heritage Act – rather, the proposed amendments would expand the St Lawrence Ward Heritage Character Area and include a policy that calls for a future study to determine if the area meets the criteria for a Heritage Conservation District. Additional community consultation would occur as part of that future study.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

Please give me a call to further discuss or respond to this email.

Thanks  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

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**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** January 7, 2025 11:03 AM  
**To:** [REDACTED]  
**Cc:** [REDACTED]; Agarwal,Sukriti  
**Subject:** RE: Fwd: North King's Town

Hi Julie,

I apologize for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

To answer your questions:

1. 190 Rideau Street would be located within the proposed expanded "St Lawrence Ward Heritage Character Area". The project also proposes revised policies for Section 7.3.D.5 of the Official Plan, which can be reviewed within the document titled "Proposed Official Plan Modifications" available on DASH through the above link. These policies would discourage demolition, encourage adaptive reuse of existing structures, and reiterate that *Planning Act* applications (such as zoning amendments) may be required to submit urban design studies or heritage impact assessments, where appropriate, to demonstrate future development is compatible with the characteristics of the area. Further, the proposed policies indicate further investigations will be undertaken to define appropriate boundaries for a future heritage conservation district study area.
2. The proposed amendments are NOT a heritage designation under the *Ontario Heritage Act*. These policies are intended to identify an area of the city with a unique character, but property owners would not be required to obtain heritage approval to alter buildings. Should the City undertake a heritage conservation district study in the future, and should that study determine the area satisfies the criteria for a heritage conservation district designation, then heritage approvals may be required in the future. However, it is important to reiterate that the current proposed amendments are not a heritage designation.

In terms of next steps, we are compiling comments received from the December 5 Community Meeting and considering any necessary modifications. As noted above, we intend to submit formal applications in

the near future and will be holding a Public Meeting at Planning Committee before seeking Council approval of the amendments.

3. We are actively seeking public comments on the draft materials and considering any necessary revisions. I would be happy to have a phone call or meet with you at 1211 John Counter Boulevard to further discuss the material. You are encouraged to provide any comments or recommended changes at your earliest convenience. You can provide comments at the public meeting and until Council makes a decision on the applications.

Please let me know if you'd like to have a phone call to further discuss.

Thanks  
Niall



**Niall Oddie** M.P.I, MCIP, RPP (he/him/his)

Senior Planner  
Planning Services

City of Kingston  
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613-546-4291 extension 3259

[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



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---

**From:** Julie [REDACTED]  
**Sent:** December 28, 2024 1:13 PM  
**To:** Oddie, Niall <noddie@cityofkingston.ca>  
**Cc:** Julie [REDACTED]; John Blasko [REDACTED]  
**Subject:** Fwd:

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my apologies for sending an incomplete email to you. I will start again!

Niall Oddie,  
My husband, John Blasko, and I own a house at 190 Rideau St. Kingston. I received the attached notice in the mail on December 24th 2024, stating that a community meeting will be held on December 5th, 2024. Given the strike by Canada Post we did not receive this information in time to respond.

The purpose of this email is to inform you of the above as well as to seek clarification on what this means for us. The map in the letter does not have sufficient streets identified for us to clearly understand which category in the legend applies to our property.

Could you please provide further information on the following:

- which category applies to our house at 190 Rideau St
- what this category means and proposed next steps by the City and / or the Heritage Society
- what our options are if we are not in agreement with the proposed changes.

thank you.  
Julie Blasko

----- Forwarded message -----

From: Julie <[REDACTED]>

Date: Sat, Dec 28, 2024 at 1:04 PM

Subject: Fwd:

To: <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>

Cc: John Blasko <[REDACTED]>, Julie <[REDACTED]>

Niall Oddie,

My husband, John Blasko, and I own a house at 190 Rideau St. Kingston. I received the attached notice in the mail on December 24th 2024, stating that a community meeting will be held on December 5th, 2024. Given the strike by Canada Post we did not receive this information in time to respond.

The purpose of this email is to inform you of the above as well as to seek clarification on what this means for us. The map in the letter does not have sufficient streets identified for us to clearly understand which category in the legend applies to our property. Could you please advise.

Furthermore, could you please provide further information on the following:



**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** January 8, 2025 2:21 PM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: 71 ordnance; owner janko lovcanin

Good afternoon,

The City has been working on a project called North King's Town Specific Policy area, which has identified a number of areas where intensification (residential with the potential for ground floor commercial uses) would be appropriate – these are the grey shaded areas shown on the back of the notice that you received. These intensification areas could contain buildings of varying heights. The project has also identified lands where a change in the land use designation or zoning is being proposed, primarily to encourage future residential or mixed use development – these are the areas shown in the diagonal lines on the map. The area shown in a cross-hatch pattern is a proposed expansion to the St Lawrence Ward Heritage Character Area, which recognizes the neighbourhood as having unique characteristics. The policies for this area would discourage demolition, encourage adaptive reuse of existing buildings and highlight that development applications may be required to submit urban design studies or heritage impact assessments to reduce impacts on the character of the area. Further, the policies of the expanded St Lawrence Ward Heritage Character Area indicate the future intention to undertake a heritage conservation district study – please note, the current amendments do not propose to establish any heritage designations, but rather, identify the potential for a future study.

71 Ordnance Street is already located within the existing St Lawrence Ward Heritage Character Area. The property is near one of the proposed intensification areas on Bagot Street between Ordnance and Bay Street, where building heights up to 6 storeys have been proposed.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

Please let me know if you'd like to arrange a call to further discuss the project or any of the materials available online for review. Alternatively, we could also meet in person at 1211 John Counter Boulevard.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

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---

**From:** John L <[REDACTED]>  
**Sent:** December 19, 2024 5:13 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** 71 ordnance; owner janko lovcanin

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

hello;  
this email is in regards to the letter I received today. It states there will be zoning amendments to the surrounding areas around my property.

If you can please explain what these three designations refer to. If you can explain what the city of Kingston actually is proposing.. It looks like a waterfront development project. Also what is this heritage and intensification designation?

regards

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** January 8, 2025 1:33 PM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: North King's Town

Good afternoon,

I apologize for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

We are currently receiving questions and comments from the public and considering potential revisions to the draft materials ahead of submitting the formal applications. I would be happy to arrange a call with you to discuss the project or answer any questions you may have. Alternatively, we could meet in person at 1211 John Counter Boulevard.

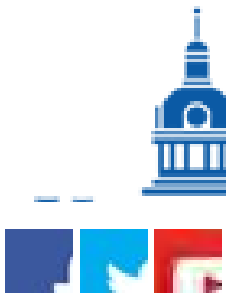
Thanks  
Niall

**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

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---

**From:** Marty Brule [REDACTED]  
**Sent:** December 22, 2024 1:00 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:**

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Mr. Noddie. I would like to mention that it is quite redundant to receive a notice about a public meeting to be held on December 05/24, and receive it on December 20/24.

The city should have made an extra effort to contact all citizens involved in this meeting because of the Canada Post strike.

Therefore I have missed this public meeting and could not voice my approval or disapproval.

I am disappointed in the actions of the planning committee.

**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** January 8, 2025 2:36 PM  
**To:** [REDACTED]  
**Cc:** Ridge,Gregory; Norwood,Malcolm; Agarwal,Sukriti  
**Subject:** RE: Follow up: North King's Town Secondary Plan

Hi Maureen,

Thanks for your email and apologies for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

We are currently collecting questions and comments from the public, and acknowledging many people were not aware of the December 5, 2024 meeting, I am offering to discuss the project over the phone or in person at 1211 John Counter Boulevard for anyone interested.

As Malcolm mentioned in his earlier email, a video recording of the presentation and a copy of the staff report are available through the link below.

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, copies of the draft materials are available on our [DASH platform](#) for review, using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

Please let me know if you’d like to arrange a call or meeting to discuss.

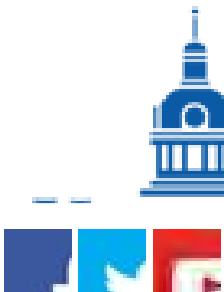
Thanks  
Niall

**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
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---

**From:** Maureen [REDACTED]  
**Sent:** December 23, 2024 5:09 PM  
**To:** Norwood, Malcolm <[mnorwood@cityofkingston.ca](mailto:mnorwood@cityofkingston.ca)>  
**Cc:** Ridge, Gregory <[gridge@cityofkingston.ca](mailto:gridge@cityofkingston.ca)>; Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Subject:** Re: Follow up: North King's Town Secondary Plan

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Malcolm

Thank you for the response and the information.

I just want to be clear I am not the only one who did not receive the notification of the Dec 5 Public meeting. No-one in the neighborhood received the notification, because as you know, there was a month long mail strike, hence the vast majority of people who have an interest and a stake in the amendments to the Official Plan as it relates to our area where not aware of the meeting.

I am very unclear as to why no-one will mention there was a mail strike and the notifications did not reach anyone.

Are you at all concerned that the data you collect, as you say for the draft report, will be incomplete because people did not receive the notifications?

That is my question.

Thank you

Maureen

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**From:** Norwood, Malcolm <[mnorwood@cityofkingston.ca](mailto:mnorwood@cityofkingston.ca)>  
**Sent:** December 23, 2024 2:39 PM  
**To:** [REDACTED] <[REDACTED]>  
**Cc:** Ridge, Gregory <[gridge@cityofkingston.ca](mailto:gridge@cityofkingston.ca)>; Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Subject:** Follow up: North King's Town Secondary Plan

Good Afternoon Maureen,

I am emailing to give a brief update on the North Kingstown Project as I understand the notice from the City for the December 5<sup>th</sup> meeting only recently reached your address.

The information from the December 5, 2024 Meeting is found online at the link below, which includes the staff report and supporting information in the agenda package and a video of the staff presentation and meeting for you should you wish to review it.

If you would like to get more information or submit comments on this project at any point in time, I suggest contacting Niall Oddie who is a primary contact at the City for the NKT project. His contact information is [noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca), or by phone 613-546-4291, ext. 3259, or he also manages the email for this project at [nktplan@cityofkingston.ca](mailto:nktplan@cityofkingston.ca).

Moving forward, I understand from the project team that they are in the process of collecting feedback on the draft materials and will be submitting formal Official Plan and Zoning By-law amendments in early 2025. Once these formal applications are submitted, a Public Meeting will be held at Planning Committee where the information will be presented and opportunities to ask questions will be provided. Notice of the meeting will be distributed in accordance with the *Planning Act* (owners of land impacted by the changes and owners of lands within 120 metres of the changes) and to the project mailing list. Further, notice will also be placed within the Whig Standard, the project website and the City calendar. This will be another opportunity to review the project's deliverables and provide more input to staff.

Several other reports associated with the North King's Town project are available on our DASH platform for review – if you have not already done so. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

Warm regards,

Malcolm



**Malcolm Norwood, RPP (he/him/his)**

Supervisor, Development Approvals

Planning Services

Growth and Development Services

City of Kingston

Located at 1211 John Counter Boulevard,

216 Ontario Street Kingston, ON K7L 2Z3

613-546-4291 ext. 3236

[mnorwood@cityofkingston.ca](mailto:mnorwood@cityofkingston.ca)

The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

**Oddie,Niall**

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**From:** Oddie,Niall  
**Sent:** January 8, 2025 1:39 PM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: Notices on proposed amendment

Good afternoon,

I apologize for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, several reports associated with the North King’s Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

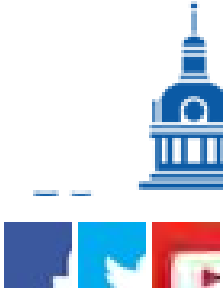
We are currently receiving questions and comments from the public and considering potential revisions to the draft materials ahead of submitting the formal applications.

If you can provide me with the address of your property, I would be happy to give you a call/email explaining how these proposed changes relate to those lands. Alternatively, we could meet in person at 1211 John Counter Boulevard.

I will add you to the project mailing list and you’ll receive future email notifications.

Thanks  
Niall





**Niall Oddie** M.Pl, MCIP, RPP (he/him/his)

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)

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**From:** Spencer Lanthier [REDACTED]  
**Sent:** December 19, 2024 4:35 PM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** Notices on proposed amendment

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi Niall,

I received something in the mail today about a community meeting set for December 5th, but today is the 19th. I assume the Canada post strike delayed this mail.

Can you send me any future notices on the st Lawrence ward heritage area?

Also, I am unsure if my property falls under this proposed change.

Thanks,  
Spencer

**Oddie, Niall**

---

**From:** Oddie, Niall  
**Sent:** January 8, 2025 1:20 PM  
**To:** [REDACTED]  
**Subject:** RE: results of Dec 5th meeting

Good afternoon,

I apologize for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, several reports associated with the North King’s Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

I would be happy to arrange a call with you to discuss the project or any questions you may have. Alternatively, we could meet in person at 1211 John Counter Boulevard.

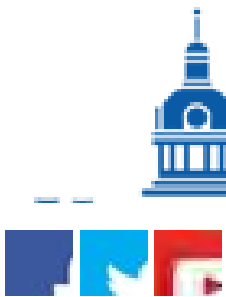
Thanks  
Niall

**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
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**From:** tracyandtimlyon <[REDACTED]>  
**Sent:** December 31, 2024 9:30 AM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** results of Dec 5th meeting

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi, following the postal strike we received a notice about infill in our neighbourhood (By-Law2022-62? and former 8499?), and a Dec 5th meeting. Please send results of the meeting.

Tim

--

Tim & Tracy

[REDACTED]

**Oddie, Niall**

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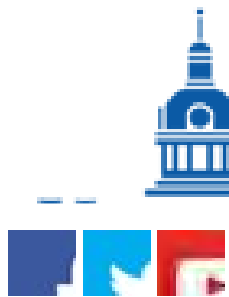
**From:** Oddie, Niall  
**Sent:** January 8, 2025 3:21 PM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti; Osanic, Lisa; Oosterhof, Gary; Cinanni, Vincent  
**Subject:** RE: North King's Town

Good afternoon Vicki,

Please see below for responses to your questions. Sukriti or I would be happy to discuss any further questions or comments that you may have.

We would appreciate comments on the draft materials by January 10, 2025 so that we can ensure they are considered in any revisions before the formal applications are submitted, however comments are accepted until Council makes a decision on the applications.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
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**From:** [REDACTED] <[REDACTED]>  
**Sent:** January 7, 2025 9:14 PM  
**To:** Agarwal, Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Cc:** Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>; Osanic, Lisa <[losanic@cityofkingston.ca](mailto:losanic@cityofkingston.ca)>; Oosterhof, Gary <[goosterhof@cityofkingston.ca](mailto:goosterhof@cityofkingston.ca)>; Cinanni, Vincent <[vcinanni@cityofkingston.ca](mailto:vcinanni@cityofkingston.ca)>  
**Subject:** North King's Town Secondary Planueen

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Sukriti:

Happy New Year!

I am trying to understand some things about the North King's Town Secondary Plan that was presented to Planning Committee on December 5, 2024.

Here are my questions.

### Urban design

The staff report (PC-25-001) mentions built form policies that can be used to minimize the impact of highrise buildings. I thought the city has no jurisdiction to impose design guidelines. How will the city be able to “ensure these buildings are appropriately designed?” (page 15) What measures do planning staff have in mind?

Changes to the *Planning Act* have reduced the City's ability to regulate exterior design of buildings through the Site Plan Control process (materiality, landscaping, etc.) however we retain the ability to regulate built form through the use of building setbacks, stepbacks, lot coverage, floor plate sizes, building length, articulation, etc. which can all be enforced through zoning regulations.

Also, could you please point to a reliable reference (scholarly analysis) that explains how a podium and tower design works to “reduce the visual mass of a building?” (page 20, section 10H.2.24) That does not seem to be the perception people have, for example, of the Homestead Tower on Queen Street between Ontario and King Streets.

The City does not have access to academic journals to complete a scholarly analysis, however the design concepts around podiums and towers are widely discussed in various design guidelines implemented by other large municipalities within Ontario, such as:

- Toronto: <https://www.toronto.ca/wp-content/uploads/2018/01/96ea-cityplanning-tall-buildings-may2013-final-AODA.pdf>
- Ottawa: [https://documents.ottawa.ca/sites/default/files/design\\_guide\\_tall\\_bldgs\\_en.pdf](https://documents.ottawa.ca/sites/default/files/design_guide_tall_bldgs_en.pdf)
- Milton: <https://www.milton.ca/en/business-and-development/resources/Tall-Building-Guidelines.pdf>
- Hamilton: <https://www.hamilton.ca/sites/default/files/2022-11/pedpolicies-tall-buildings-guidelines.pdf>
- Burlington: <https://www.burlington.ca/en/building-and-renovating/resources/Urban-Design-Guidelines/Tall%20Building%20Guidelines.pdf>

### Outer Station

Page 33 recognizes the cultural value of the Outer Station and the paragraph concludes – “As such, the CHS considered how appropriate future use might occur, including the potential for adaptive reuse or moving structures within the site.” [underlining added] How will moving the train station buildings preserve their cultural heritage significance? (section 10H.3.2 )

Section 5.3 of the Cultural Heritage Study provides recommendations for conservation and development for Sub-Area #2, which includes the Depot/Outer Station lands, with Section 5.3.2 providing a series of recommendations mainly focused on the Outer Station lands themselves, including the following:

“The City shall encourage adaptive re-use of the existing built heritage resources and cultural heritage landscape attributes in their original locations on site. In the absence of any feasible alternative to redevelopment, and as a last resort instead of demolition, the City may consider relocation of built heritage resources or cultural heritage landscape attributes to locations within the property provided that it has been demonstrated to the satisfaction of the City that the cultural heritage values of the heritage attributes will be conserved and that relocation would enhance public access to the cultural heritage resources (off-site relocation is not acceptable).”

It is acknowledged that the Outer Station buildings are in (and have been for quite some time) a deteriorated state and conservation will require significant expenditure. Redevelopment of the lands may be a feasible method of supporting such a significant expenditure, however the placement of the buildings within the site may constrain

redevelopment. As a last resort, relocating the buildings within the Outer Station site may be a feasible option to facilitate the redevelopment needed to fund restoration of the buildings.

### Electricity Capacity

“There were no specific upgrades identified for the electrical or natural gas networks at this time,” (page 4) and then on page 34 “It is anticipated that significant upgrades to the electrical infrastructure within NKT and surrounding network will be required to accommodate the full build-out of NKT.” Do I understand correctly that the studies have not been done to determine electricity needs and that developments are going to be approved on a first come, first served basis until the need for an upgrade is reached?

The Servicing Study was completed by J.L. Richards and reviewed by Utilities Kingston. Each of the utilities (wastewater, potable water, stormwater, electrical and natural gas) within NKT were analyzed to assess the impacts of the potential growth within NKT and provide recommendations for necessary modifications. Modelling was completed by Utilities Kingston for the wastewater and potable water networks and determined where specific upgrades within NKT would be required to support a full-build out scenario. Utilities Kingston did not complete the same modelling for the natural gas or electrical utilities as these networks function as a scale beyond the NKT study area and require more specific phasing information to determine where upgrades will be required.

Development within the identified intensification areas will be subject to a holding symbol and required to provide servicing reports to confirm adequate servicing capacity.

### Parkland

Both Councillors Osanic and Oosterhof raised the issue of parkland during the December 5<sup>th</sup> meeting. The staff answer was that based on the Williamsville experience, people wanted more sidewalks, wider sidewalks, vegetation planters, and trees. This may be an accurate summary for the student-development centred Williamsville. For the Montreal Street/ Counter Boulevard it can be assumed that much of the housing will be for families, especially young families.

Don't people in intensely developed neighbourhoods need nearby park space, not just parkettes than add to the pedestrian experience? (10H.2.11, 10H.2.42)

How does the lack of parkland close to the intensification area match Principle # 1 – “To create a welcoming and inclusive setting for people to gather, recreate, work, and live?” Is there a policy calculation cities use to determine the amount of parkland relative to population? (beyond the 5% or cash-in-lieu requirement in the Planning Act)

Is staff saying that people living in the area can use the paths to get to Belle Park for their parkland and recreation? Is the area at the footing of the Waaban Crossing appropriate park space? Where are the plans for parks (not parkettes) for people to use around Counter and Montreal Streets?

I note too that the city has already spent well over \$1 million for small parkette spaces in Williamsville. Is there a way to identify the park space area in the Secondary Plan for North Kingston's intensification areas? What spaces are identified for people to gather and recreate around Montreal and Counter Street that do not require crossing either street – the majority of the buildings are on the west side of Montreal Street.

New development will be required to provide parkland in accordance with the City's parkland dedication by-law and *Planning Act* requirements. We have not identified the locations of the parklands within the intensification areas at this time as the location and orientation of the park space needs to be considered in relation to the development application, servicing requirements, grade changes, etc. which are determined through detailed

design of the development applications. The City further utilizes the Parks and Recreation Master Plan to determine if additional parkland, outside of the development process, is necessary to support neighbourhoods and includes consideration of population density within the surrounding areas.

<https://www.cityofkingston.ca/council-and-city-administration/plans-reports-and-studies/parks-and-recreation-master-plan/>

The Montreal Street and John Counter Boulevard intersection is in proximity to Belle Park, which has an approved master plan to guide the redevelopment and programming within the park. Further, the City has been working to secure additional segments of the Waterfront Trail along the Great Cataraqui River south of the Waaban Crossing, which contributes to recreation opportunities within this area. The lands on the immediate north and south sides of the Waaban Crossing are City-owned lands and will be designated Open Space through the proposed Official Plan amendments, however detailed plans for these lands are not yet available.

Residents are also able to use the Newmarket Lane Parkette along the shoreline of the Great Cataraqui River. I also understand that several of the buildings around this intersection have programmed outdoor amenity space for their residents, such as picnic tables and climbing gyms for children.

### **Parking**

Given the reduction in parking spaces requirements, could the secondary plan require the majority of building parking to be underground? Otherwise, won't this level of intensification create an asphalt wasteland that holds heat in summer and is contrary to the city's sustainability goals?

The proposed policies encourage underground and structured parking, but would permit surface parking in the rear or interior side yards as well. We know through previous work that underground parking garages are costly to construct and can challenge the financial feasibility of a project or increase unit costs to recover the increased construction costs. This approach has been selected to help reduce construction costs, which can assist with housing affordability. Further, we heard through consultations with the development community that underground parking garages may be prohibited within brownfield locations, depending on the type of contamination encountered (e.g. groundwater contamination).

Proposed Section 10G.2.39 encourage the surface parking areas to include planting strips, landscaped traffic islands to define smaller parking courts, which help to create opportunities for shade, infiltration and surfaces with higher albedo to reflect sunlight and reduce heat build up. The zoning by-law would also regulate lot coverage and landscape open space requirements. The zoning by-law also established parking maximums, which serve to reduce the amount of asphalt included within development applications.

### **Active transportation**

Doesn't the Official Plan include transit as a form of active transportation? The answer to a question at the December 5<sup>th</sup> meeting about the active transportation corridor proposed for Bay Street to Montreal Street (page 7) said "active transportation corridor" referred to pedestrians and cyclists. How does the proposed zoning make this explicit? Doesn't it need to specify that buses are not considered an active transportation mode in the context of the Wellington Street Extension?

(Added by By-Law Number 2017-57, OPA Number 50)

**Active Transportation**

Means human-powered travel, including but not limited to, walking, cycling, inline skating and accessible travel with the use of mobility aids, including motorized wheelchairs and other power-assisted devices moving at a comparable speed.

(Added by By-Law Number 2017-57, OPA Number 50)

**Additional Residential Unit**

The definition of 'Active Transportation' within the Official Plan does not include transit – it means “human-powered travel”. The City’s Active Transportation Master Plan includes a similar definition that also omits transit. Buses need to operate on a road and a road is not being constructed through these lands.

The proposed amendments remove the southern portion of the WSE from the Schedule 4 – Roads of the Official Plan, while being retained on Schedule 5 - Major Pathways.

**20-storey building location**

Does the zoning to allow 20-storey buildings have a limit on the total number? If I am reading the proposed Schedule J correctly, they could be along Montreal Street near Railway. Is staff foreseeing a possibility of three 20-storey buildings or more?

The proposed Official Plan policies would enable building heights up to 20 storeys in the area north of the Montreal Street, Rideau Street and Railway Street intersection through a minor variance application. The zoning for this area is proposed for the MU3 zone, which permits building heights of up to 15 storeys as of right; the minor variance application could be used to increase up to 20 storeys in accordance with the proposed Official Plan policies.

The proposed zoning currently does not establish a maximum number of towers that could be constructed within the zones that permit high-rise development, but does limit development through density, setbacks, tower separation distances, lot coverage, etc.

Thank you for your answers to these questions.

I understand that the public has until January 10<sup>th</sup> to provide comments on the December 5<sup>th</sup> staff report and I hope to provide some comments by then.

Vicki

Vicki Schmolka





**Oddie,Niall**

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**From:** Agarwal,Sukriti  
**Sent:** January 13, 2025 4:36 PM  
**To:** [REDACTED]  
**Cc:** Oddie,Niall  
**Subject:** RE: A few comments on PC-25-001

Hello Vicki,

Acknowledging receipt of your email. Thank you for taking the time to submit comments. The comments are similar to the ones we received previously, to which responses were provided on January 8<sup>th</sup>.

Thanks,

Sukriti

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**From:** [REDACTED] <[REDACTED]>  
**Sent:** January 10, 2025 6:47 PM  
**To:** Agarwal,Sukriti <sagarwal@cityofkingston.ca>  
**Cc:** Oddie,Niall <noddie@cityofkingston.ca>  
**Subject:** A few comments on PC-25-001

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Please find attached a few comments on the North King's Town report that was at Planning Committee on December 5<sup>th</sup>.

Thank you.

Vicki

Vicki Schmolka  
[REDACTED]

## **Comments on the North King's Town Plan presented at the Community Meeting of Planning Committee, December 5, 2024**

### **Planning Report PC-25-001**

Here are issues of pressing concern that I believe need to be addressed before the next iteration of the Plan comes to Planning Committee with a staff recommendation:

#### **1. The failure to identify neighbourhood park locations in the areas of intensification.**

As Councillor Osanic said at the meeting on December 5<sup>th</sup>, no one wants to repeat the mistakes with recent developments in Williamsville. One mistake is the after-the-fact creation of park space. This not only means that the park spaces are not necessarily in appropriate locations but also that it became the city's responsibility to buy the land for them.

In the context of the two areas identified for the highest level of intensification in North King's Town, the need for outdoor park space is critical. Residents need nearby neighbourhood parks. Parkettes and private amenity spaces associated with each building are not enough. Suggesting that almost 13,000 new residents can make do with Belle Park, the area next to the Waaban Crossing, and existing park space is not acceptable.

Please figure out the areas for parks and include them in the Secondary Plan. To do otherwise is to leave it to chance and after-the fact purchases by the city. Leaving it to chance is not good planning, it is not even planning.

#### **2. The failure to account for electricity needs.**

Each area of intensification needs an electricity plan as part of the Secondary Plan. Not having a plan means that some developments will not be able to go ahead until after a service upgrade. This is arbitrary and not good planning.

#### **3. The failure to address any climate change pressures and show respect for the city's stated sustainability goals.**

This includes continuing to allow natural gas servicing to new buildings and the failure to require underground parking to avoid asphalt heat sink parking lots and excessive storm water runoff. The lack of underground parking also disadvantages people with mobility challenges as it is very difficult for them to clear snow from their vehicles. In what ways does the proposed Secondary Plan address climate change? How does it meet Council's Strategic direction to "lead environmental stewardship and climate action?"

**4. The continuation of the myth that a podium and tower design “reduces the visual mass of a building.”**

This building design approach may have been recommended by Mr. Toderian. It has never been substantiated with reference to academic journals or accepted by Council as city policy. A better city model would be four to eight storey buildings which create a more neighbourly streetscape. Really, the most important question is what building height and urban design is best from a climate perspective?

**5. The failure to protect the Outer Station buildings and grounds.**

While the Secondary Plan promotes protecting the property, it also opens the door to moving the buildings within the site. This is premature policy and should not be part of the current amendments. The policy needs to be to develop the site with the buildings where they are because of their historical context. Please do not give up on this yet.

**Other comments**

After all the community meetings and work that has gone on in the past, the plans for North King’s Town are less than expected. They do not reflect some of the dynamic input the city received during consultations nor do they have the design excitement offered by consultants during earlier stages of the project. I find there is nothing remarkable or interesting in this Plan for such a large area of the inner city that has enormous potential.

How does the plan meet the #1 principle for this work (page 7, PC-25-001 “To create a welcoming and inclusive setting for people to gather, recreate, work, and live?”

It is sad that Parks Canada is not defending the views from the Rideau River and speaking up for the UNESCO World Heritage Site inscription. The unlimited number of 20-storey towers contemplated in one location not far from the water will be very visible and an unwelcome intrusion on the landscape.

When I read the staff report, I didn’t see any consideration of the assets the city needs to protect, with the exception of the proposals relating to the St. Lawrence Ward Heritage Character Area. For example, the UNESCO inscription is a valuable community asset. I do not feel it is being given sufficient consideration.

In conclusion, the Secondary Plan proposal is not an inspiring or climate-appropriate plan for North King’s Town’s future.

Vicki Schmolka  


**Oddie,Niall**

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**From:** Oddie,Niall  
**Sent:** January 13, 2025 11:21 AM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** RE: Canada Post strike delayed delivery of this Community Meeting Notice...North Kings Town Area FILE # D01 011 2024

Hi Shona,

Thanks for your email and apologies for the delay in receiving the notice of the Community Meeting. We tried to get the notice out before the Canada Post strike. We also distributed notice through the project email list and included two advertisements within the Whig. This being said, the December 5 meeting was not the final opportunity for public comment on the project – December 5 was a Community Meeting, which is where a project is introduced for initial comment. A formal Public Meeting will be held in the near future once the formal Official Plan and Zoning By-law Amendments are submitted. Residents will receive another notice in the mail ahead of this meeting and have an opportunity to review and comment on the formal submission materials.

We are currently collecting questions and comments from the public, and acknowledging many people were not aware of the December 5, 2024 meeting, I am offering to discuss the project over the phone or in person at 1211 John Counter Boulevard for anyone interested.

A video recording of the presentation and a copy of the staff report are available through the link below.

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

Further, copies of the draft materials are available on our [DASH platform](#) for review, using the following steps:

1. Use the search bar near the top right-hand corner to search for application “D01-011-2024”.
2. Select “Record Info”
3. Select “Supporting Information” from the drop-down menu.

I will add your email to the project email list, so you’ll receive email updates as well as physical mailings. I’ll make a note to send the notices to the address provided.

Please let me know if you’d like to arrange a call or meeting to discuss.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** Shona YGK <[REDACTED]>  
**Sent:** January 13, 2025 10:54 AM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>; Contactus <Contactus@cityofkingston.ca>  
**Subject:** Canada Post strike delayed delivery of this Community Meeting Notice...North Kings Town Area FILE # D01 011 2024

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RE: FILE # D01 011 2024

Dear Niall Oddie

As the owners of 117 James Street (North Kings Town Area) on 20 December 2024 we received a notice that public consultation /community meeting would be held 5 Dec 2024.

As you can note from the dates provided above unfortunately Canada Post strike delayed delivery of this Community Meeting Notice. As a result, it was unfortunate that we did not schedule nor participate in this hybrid meeting.

The purpose of this email is to make a formal written request to receive future notices regarding the amendments.

Please mail these future notices to our home address:  
804 Sterling Ave  
Kingston Ontario  
K7L 4V4

Thank you  
kind regards  
Shona L. van Zijll de Jong



**Oddie, Niall**

---

**From:** Oddie, Niall  
**Sent:** January 17, 2025 10:42 AM  
**To:** [REDACTED]  
**Cc:** Agarwal, Sukriti; Van Vugt, Niki  
**Subject:** RE: Notice of Community Meeting - D01-011-2024 - North King's Town Project (City of Kingston)

Hi Mike,

Thank you for these comments on the draft NKT Servicing Study.

We have contacted the consultant to correct the minor, but important typo relating to the existing SWM system.

We appreciate the suggested wording relating to draft OP wording for Section 10H.2.47. We had considered an approach with stronger wording when drafting the policies. However, we elected for the more general approach as we did not want to require development applications to go through an OPA if they could not satisfy a particular target identified within the SWM chapter. The City does not typically include technical specifications within OP policies for this reason. Alternatively, the City typically identifies study requirements/methodologies through the pre-application process and can tailor the requirements to ensure they are applicable and feasible for the project. The intent is to use the NKT Servicing Study as a guide when reviewing development applications.

Thanks again for the comments,  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259

[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

---

**From:** Mike Dakin <[REDACTED]>  
**Sent:** November 28, 2024 4:07 PM  
**To:** Van Vugt, Niki <nvanvugt@cityofkingston.ca>  
**Cc:** Oddie, Niall <noddie@cityofkingston.ca>; Agarwal, Sukriti <sagarwal@cityofkingston.ca>  
**Subject:** RE: Notice of Community Meeting - D01-011-2024 - North King's Town Project (City of Kingston)

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Hi Niki, Niall and Sukriti:

Thanks for keeping me in the loop on the NKT project. As mentioned in my note to Niki, I've prepared a comment letter to serve two purposes: input as a member of the Working Group and formal comments from CRCA based on our *Planning Act* commenting agency role.

Please see attached letter. Hopefully it's of some assistance. Just reach out if there are any questions.

Feel free to add to DASH as appropriate.

Thanks,  
Mike

**Michael Dakin** RPP, MCIP  
Supervisor, Development Review



**Phone:** [REDACTED]

**Toll-Free:** [REDACTED]

**Web:** [www.CataraqiConservation.ca](http://www.CataraqiConservation.ca)

**Please note individuals should schedule an appointment with Cataraqi Conservation staff for any in-person inquiries relating to planning and permitting matters. Contact me or visit <https://crca.ca/property-inquiry-form/> to do so.**

---

**From:** Van Vugt, Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>

**Sent:** November 15, 2024 10:44 AM

**Cc:** Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>; Agarwal, Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>

**Subject:** Notice of Community Meeting - D01-011-2024 - North King's Town Project (City of Kingston)

Good morning,

Please see attached Notice of Community Meeting on Thursday December 5, 2024, for the North King's Town Project (City File Number D01-011-2024).

The City of Kingston is proposing amendments to the Official Plan, Kingston Zoning By-Law 2022- 62 and former City of Kingston Zoning By-Law 8499 to implement the recommendations of the North King's Town (NKT) project. The proposed amendments include, but are not limited to, the addition of a new specific policy area in the Official Plan to guide future development within areas identified for infill and mixed-use intensification in the North King's Town area and associated zoning regulations, redesignation and rezoning of properties located on Montreal Street generally between James Street and Ragland Road to Main Street Commercial, and amendments to implement planning-related transportation and cultural heritage recommendations including a proposed expansion to the boundary of the St. Lawrence Ward Heritage Character Area.



Additional information including supporting documents for the application can be viewed by accessing the Development and Services Hub (DASH) at <https://www.cityofkingston.ca/DASH>.

Please contact us, should you have any enquiries regarding the application following your review of the attached.

Sincerely,

Niki Van Vugt



**Niki Van Vugt (she/her/hers)**

Intermediate Planner

Planning Services, Growth & Development

City of Kingston

Located at: 1211 John Counter Boulevard

216 Ontario Street Kingston, ON K7L 2Z3

613-546-4291 extension 3253

[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

**Want more information from Planning Services? [Sign up for our email newsletter!](#) Select "Planning Project Updates" when you sign up.**

**Oddie, Niall**

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**From:** Agarwal, Sukriti  
**Sent:** January 17, 2025 2:32 PM  
**To:** [REDACTED]  
**Cc:** Oddie, Niall  
**Subject:** RE: A few comments on PC-25-001

Hello Vicki,

Thank you for your further comments.

The North King's Town project aims to address climate change in several ways, for example:

- Identifying opportunities for a significant amount of infill, intensification and redevelopment within the existing urban area of the City. This approach reduces pressure on natural heritage systems, conserves resources, contributes to more efficient infrastructure and service delivery, and reduces greenhouse gas emissions by reducing distances residents would need to travel to access places of employment, commerce and recreation.
- Creating a new Mixed Use designation and enabling mixed use development within all identified intensification areas, which can promote active transportation and reduce greenhouse gas emissions by locating residential uses in close proximity to supporting commercial uses.
- Establishing densities to facilitate mid- and high-rise development, which contributes to a more efficient use of land and resources.
- Promoting active transportation and transit use by locating intensification areas along (or within walking distance to) express transit routes and reducing minimum parking requirements for residential developments (while preserving a maximum parking requirement within zoning), which all serve to reduce greenhouse gas emissions.
- Encouraging developments to plant "little forests" within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and contribute to greater biodiversity, which can assist in mitigating urban heat island effects by increasing tree canopy, habitat loss and promote infiltration.
- Encouraging development applications to include fruit trees, nut trees and other edible plantings within their planting plans to promote food security and local food options, which can reduce greenhouse gas emissions associated with food transportation, while also serving to establish tree canopy within the urban area.
- Establishing minimum landscaped open space requirements for the intensification sites.

The North King's Town Servicing Report includes a detailed chapter on stormwater management, with innovative approaches and targets. Staff will be utilizing the NKT Servicing Report to identify study requirements for future development applications on the proposed intensification sites. Parkland dedication requirements for future development will be as per the Planning Act and the City's Parkland Conveyance By-Law.

Thanks,

Sukriti

---

**From:** [REDACTED] <[REDACTED]>  
**Sent:** January 14, 2025 12:44 PM  
**To:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Subject:** RE: A few comments on PC-25-001

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Hello Sukriti:

What worries me the most as a resident is the lack of integration of any measures that address local ways to mitigate climate “change.” Do you feel Planning has no mandate to do this?

For instance, Montreal is creating [sponge parks](#) to soak up water from excessive rain events. The North King’s Town plan shows no measures for rainwater management beyond the usual.

How did staff account for climate change in this vision for North King’s Town?

Vicki

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**From:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Sent:** January 13, 2025 4:36 PM  
**To:** [REDACTED] <[REDACTED]>  
**Cc:** Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Subject:** RE: A few comments on PC-25-001

Hello Vicki,

Acknowledging receipt of your email. Thank you for taking the time to submit comments. The comments are similar to the ones we received previously, to which responses were provided on January 8<sup>th</sup>.

Thanks,

Sukriti

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**From:** [REDACTED] <[REDACTED]>  
**Sent:** January 10, 2025 6:47 PM  
**To:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Cc:** Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Subject:** A few comments on PC-25-001

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Please find attached a few comments on the North King’s Town report that was at Planning Committee on December 5<sup>th</sup>.

Thank you.

Vicki

Vicki Schmolka



January 20, 2025

**Mr. Niall Oddie**

Senior Planner  
Planning Services, City of Kingston  
1211 John Counter Blvd.

**RE: 5-7 Cataraqui Street, Kingston**  
**North King's Town Secondary Plan**

Dear Mr. Oddie,

Fotenn Planning + Design is pleased to submit this letter on behalf of our client, Rose Kove Development Inc., regarding their property at 5-7 Cataraqui Street ("the site"), within the North King's Town Secondary Plan ("NKTSP") area. The City of Kingston is developing a secondary plan for the North King's Town area, the Inner Harbour and Old Industrial areas extending north from the central business district to John Counter Boulevard and west to Division Street. The NKTSP will support and promote a sustainable, vibrant and livable community while supporting the inclusion of additional housing opportunities for a significant portion of Kingston's urban core.

**Previous Development Applications**

On September 19<sup>th</sup>, 2023 City Council approved a zoning by-Law amendment and draft plan of subdivision for the site. The applications sought to permit a total of 76 residential townhouses, 1 block for a road reserve, 1 block for common elements, and 2 new roadways under private ownership of a future condominium. The zoning by-law amendment rezoned the lands to the Urban Residential Type 3 (UR3) Zone and introduced Exception Number E123 to capture site-specific provisions.

Due to detailed environmental site assessment results received after receiving Council approval, a revised proposal for the south portion of the site, Phase 2, is being developed. The first phase, consisting of 43 townhouses, will be constructed as approved and the detailed subdivision design work for these lands, including the required final plan of subdivision and condominium applications, is underway. The Phase 2 lands do not achieve sufficient density with townhouses to offset the costs of redeveloping that area. The owner is pursuing additional development applications on this portion of the site which will be submitted in early 2025.

**NKTSP Comments**

The most recent draft NKTSP documents released in November 2024 set out the proposed OP policies needed to establish the NKTSP, along with relevant amendments to OP schedules and to the zoning by-law. Among the changes proposed is the removal of certain properties from Site Specific Policy (SSP) areas in the OP due to how these historical SSPs conflict with the intensification objectives of the PPS, the OP, and the NKTSP.

SSP Number 8 in the OP applies to this site and limits net residential density to a maximum of 62.5 residential units per net hectare (50 units per net hectare plus 12.5 units per net hectare due to previous land conveyance to the City). This density would allow up to 40 units on the Phase 2 lands, which is insufficient given the extent of environmental contamination present. Leaving the site within SSP Number 8 would result in the need for a technical official plan amendment solely to adjust the maximum density on the site, which is necessary to achieve an economically viable unit count.

The 2024 Provincial Planning Statement (PPS) sets a goal of building at least 1.5 million homes by 2031. The vision for the 2024 PPS includes increasing the supply and mix of housing options in the province and building homes that respond to market needs and local demand to support a growing population and workforce. The PPS provides a policy framework that promotes growth and development within settlement areas, the protection of natural and



cultural heritage resources, and the mitigation of potential risks to public health or safety. The PPS includes policies encouraging municipalities to review their planning policies through the lens of how they will contribute to the supply of housing. Further to this, the NKTSP is seeking to facilitate intensification in an area of the city which will need to accommodate significant intensification, as demonstrated through the recent Population, Housing and Employment Growth Analysis Study completed by Watson & Associates Economists Ltd.

**We respectfully request that the site be removed from the SSP Number 8 designation in the OP through the implementation of the NKTSP.**

Please contact the undersigned with any additional comments or questions at [REDACTED] or [REDACTED] and [REDACTED] to advise on the status of any decisions regarding the NKTSP.

Respectfully submitted,



Youko Leclerc-Desjardins, MCIP RPP  
Principal  
Fotenn Planning + Design



Alexandra Cleave  
Planner  
Fotenn Planning + Design

**Oddie,Niall**

---

**From:** Agarwal,Sukriti  
**Sent:** February 6, 2025 1:01 PM  
**To:** [REDACTED] Oddie,Niall  
**Cc:** Van Vugt,Niki; [REDACTED] Agnew,Paige  
**Subject:** RE: NKT - Comments on Draft Materials

Hi Kelsey,

Thank you for your detailed comments on the draft NKT materials.

The following responses are provided in the same numerical order as your requested modifications:

1. Staff undertook additional internal consultation and came to the same conclusion regarding proposed Schedule NKT-2 (maximum building heights). The formal Official Plan Amendment will not include Schedule NKT-2 and instead, provide a more general policy description of the maximum building heights permitted within the intensification areas. The zoning by-law will regulate building height using the direction from the Official Plan.
2. We agree that policies 10H.2.3 and 10H.2.4 require modification to reflect the removal of Schedule NKT-2. Our preferred approach would be to provide a more general policy description of maximum building heights within Sections 10H.3.1 (Montreal Street and John Counter Boulevard) and 10H.3.2 (Montreal Street, Rideau Street and Railway Street). We will give consideration to incorporating some of the criteria you provided within a modified version of Section 10H.3.3.c, serving to guide minor variance applications seeking additional building height.
3. Staff do not support the removal of maximum building height or densities from the MU1, MU2 or MU3 zones. While the above approach enables additional building height to be considered through a minor variance application (whereas previously an Official Plan Amendment would have been required), it is important to include maximum building height regulations within a zoning by-law to provide clarity for property owners (and residents) on the expected built form for those areas. Further, it important to include maximum density regulations within these zones so that the City's Planning, Transportation and Servicing plans are in alignment. Density may be able to be increased through a minor variance application, where supported through technical studies and appropriate technical review. Maximum building height and density regulations are commonly included within zones throughout the City.
4. Staff do not support the removal of ground floor commercial areas within the proposed OP and ZBA schedules. The plans identify the opportunity for a significant number of new residential units to be added to these areas and the ground floor commercial requirements are intended to ensure that these future residents have access to commercial supports and amenities within walking distance. Further, the existing residential units within these areas would benefit from new commercial spaces being added to the area as it will reduce the distance they need to travel to access commercial amenities. The analysis comparing the amount of commercial space associated with these ground floor commercial areas to the recent Watson study is based on the assumption that every linear metre along these frontages would be constructed with a commercial unit, which is not accurate. The proposed ground floor commercial

requirements would not require every linear metre of these areas to be constructed with commercial uses – there will be gaps within buildings, there may be parkettes, there may be new internal roads and mid-block connections established, development will need to comply with maximum lot coverages – these have not been considered within Fotenn’s analysis. Further, the proposed zoning includes a number of areas excluded from this regulation (such as building lobbies and other shared entrances/exits).

5. Staff do not support the removal of “Active Transportation Desire Lines” from Schedule NKT-1. These areas have been identified as important future connections for the active transportation networks identified within the NKT Mobility Plan. Including the mapped desire lines is important for providing a clear visual reference and reflects the input we received in building out more of grid network within the northern portion of the study area, providing viable access to frequent transit along Division and Montreal Streets. Proposed policy 10H.2.36 indicates that “the City will work collaboratively with the property owner”, however staff will include additional clarification within this policy to note that the desire lines shown on Schedule NKT-1 are intended to be approximate/flexible and not intended to hinder redevelopment opportunities. Staff will work with the development community when locating these future connections, however it is important to show them on the schedule for transparency purposes.
6. As noted above, we are considering modifying 10H.2.36.
7. The current wording of 4.32.1 includes “this separation distance must be measured as per the province’s D-series Environmental Land Use Compatibility Guidelines”. We feel this language would enable Staff to consider the same nuances for determining and identifying separation distances. For example, Section 4.4.4 of D-6 enables parking lots to be included within the separation distance when a sensitive use is being added in proximity to an industrial use – the current wording would allow staff to measure from the proposed building to the industrial use, allowing the parking lot to be located within the required separation distance. Further, by including the setbacks within the zoning by-law enables those numbers to be reduced through a minor variance, where appropriate. If we were to amend the provision as suggested, we could not utilize minor variance applications to resolve minor issues. We would be happy to consider any examples of nuances you are aware of within the D-series guidelines.

We would be pleased to discuss the revised policies references above with you in the near future and are willing to meet with you to further discuss any remaining concerns.

Thanks,

Sukriti



**Sukriti Agarwal, MCIP, RPP, AICP (she/her/hers)**

Manager, Policy Planning  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 ext. 3217  
[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

---

**From:** Kelsey Jones <[REDACTED]>  
**Sent:** January 21, 2025 4:30 PM  
**To:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Cc:** Van Vugt,Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>  
**Subject:** RE: NKT - Comments on Draft Materials

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Much appreciated. Thank you, Sukriti.

**Kelsey Jones, MCIP RPP**  
Senior Planner  
T [REDACTED]

---

**From:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Sent:** January 21, 2025 4:11 PM  
**To:** Kelsey Jones <[REDACTED]>; Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Cc:** Van Vugt,Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>  
**Subject:** RE: NKT - Comments on Draft Materials

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Hi Kelsey,

Confirming receipt of the comment letter.

Thanks,

Sukriti

---

**From:** Kelsey Jones <[REDACTED]>  
**Sent:** January 21, 2025 4:05 PM  
**To:** Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Cc:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Van Vugt,Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>  
**Subject:** RE: NKT - Comments on Draft Materials

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Good afternoon Niall,

Please find attached our comment letter related to NKT.

Can you please confirm receipt?

Thank you,  
Kelsey

**Kelsey Jones, MCIP RPP**

Senior Planner

T [REDACTED]

---

**From:** Kelsey Jones

**Sent:** January 20, 2025 4:36 PM

**To:** Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>

**Cc:** Agarwal, Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Van Vugt, Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>

**Subject:** RE: NKT - Comments on Draft Materials

Hi Niall,

We wanted to give you the heads up, that we are preparing a comment letter on behalf of BPE Developments. It's currently under review, but we may not receive sign off to be able to submit today.

It will be submitted by tomorrow at the latest.

Thanks,  
Kelsey

**Kelsey Jones, MCIP RPP**

Senior Planner

T [REDACTED]

---

**From:** Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>

**Sent:** January 15, 2025 9:51 AM

**To:** Kelsey Jones <[REDACTED]>

**Cc:** Agarwal, Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Van Vugt, Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>

**Subject:** RE: NKT - Comments on Draft Materials

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Hi Kelsey,

Comments would need to be submitted by January 20<sup>th</sup> in order for us to consider and implement any changes to the draft materials.

You can continue to submit comments until Council makes a decision on the applications. However, it would be up to the Council to make those changes.

Thanks  
Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** Kelsey Jones <[REDACTED]>  
**Sent:** January 14, 2025 4:03 PM  
**To:** Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>; Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>  
**Cc:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Van Vugt,Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>  
**Subject:** RE: NKT - Comments on Draft Materials

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Hi Niall,

Thank you for the follow up, and apologies for not getting back to you before the holidays.

We obviously have passed your initial request to provide comments by January 10. Can you please give us a sense of your internal timelines so we can work towards a new date?

Thanks,  
Kelsey

**Kelsey Jones, MCIP RPP**

Senior Planner

T [REDACTED]

---

**From:** Oddie,Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Sent:** December 11, 2024 2:42 PM  
**To:** Benjamin Pilon <[REDACTED]>; Youko Leclerc-Desjardins <[REDACTED]>; Kelsey Jones <[REDACTED]>  
**Cc:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>; Van Vugt,Niki <[nvanvugt@cityofkingston.ca](mailto:nvanvugt@cityofkingston.ca)>  
**Subject:** NKT - Comments on Draft Materials

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Good morning,

Thank you for meeting with us on December 4, 2024 to discuss the draft NKT materials that were presented for Community Meeting on December 5, 2024. It was noted that we were not able to get through all of the comments on the draft materials within the time allotment and that written comments may be submitted or an additional meeting could be requested.

We are happy to meet with you to further discuss the materials or review any written comments that you may have. We anticipate submitting formal Official Plan and Zoning By-law amendments early in 2025 and would appreciate the opportunity to review your comments prior to making formal applications. To this end, would it be possible to please receive your comments by January 10, 2025?

Thank you,  
Niall



**Niall Oddie M.Pl, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

January 21, 2025

**Sukriti Agarwal**  
Planning Services  
City of Kingston

**RE: Draft North King's Town Secondary Plan documents released November 13, 2024**

Dear Ms. Agarwal,

Fotenn Planning + Design has been retained by BPE Developments to review the proposed North King's Town (NKT) Secondary Plan documents released on November 13, 2024. The draft documents include proposed amendments to the City of Kingston Official Plan (OP) and the Kingston Zoning By-law 2022-62 (ZBL) to establish a new secondary plan area together with official plan policies and zoning by-law provisions to guide development.

The purpose of this letter is to express our comments regarding the proposed amendments to the Kingston OP and ZBL. Our comments are provided through the lens of how these proposed amendments will affect properties owned by our client, BPE Developments.

BPE Developments is a local Kingston developer with experience in residential, commercial and industrial uses. BPE strives to have a long-lasting, positive influence on the communities where they operate. BPE is committed to being a responsible developer of properties and communities where individuals feel at home. BPE owns several properties within the future North King's Town Secondary Plan area and has a keen interest in ensuring the secondary plan supports growth and development while minimizing risks.

The stated intent of these policy changes is to facilitate intensification in appropriate locations in a manner that fulfills the vision for NKT. We are supportive of the expressed intent to intensify in the areas identified through the draft documents, however we expect that a number of policies and provisions as drafted will not achieve the desired effect and will not result in the much-needed residential development in NKT that is being sought.

For example, as currently proposed a number of key policies are overly prescriptive and limit the ability of developers to undertake "shovel-ready" development without pursuing costly applications to amend the policy and regulatory framework. In the spirit of pursuing a successful implementation to the NKT Secondary Plan, there are several changes we recommend be made to the draft OP policies and ZBL provisions, as discussed below.

#### **Building Height + Design**

The draft documents propose to regulate building height through both the OP and ZBL. The draft OP schedule NKT-1 (North King's Town Specific Policy Area) specifies "Intensification Areas" throughout NKT, as well as areas with mandatory ground floor commercial requirements. This schedule is supported by generally enabling policies (10H.2) that support the development of mid- and high-rise buildings, subject to facilitating a suitable transition to the existing community. Detailed policies regarding site design and configuration are provided in this section as well. Of particular concern are policies that impose highly prescriptive requirements at the OP level. For example, requiring a stepback above the fourth storey facing a streetline (10H.2.24). An important lesson learned from Williamsville and the Central Kingston Growth Strategy (CKGS) is that stepbacks can significantly increase building costs as a result of numerous construction implications. Stepbacks often require transfer slabs, for example, which increase cost and complexity, while also often resulting in an increase in height. Stepbacks also complicate mechanical design and insulating requirements. While there are strategies for alleviating some of these complications, these are generally limited to large sites where greater design flexibility is present. As will be discussed below, the proposed policy framework significantly limits flexibility. While we have highlighted one specific policy and its resulting implication, we note that the theme of maximizing flexibility will be repeated in our brief commentary on the draft NKT material.



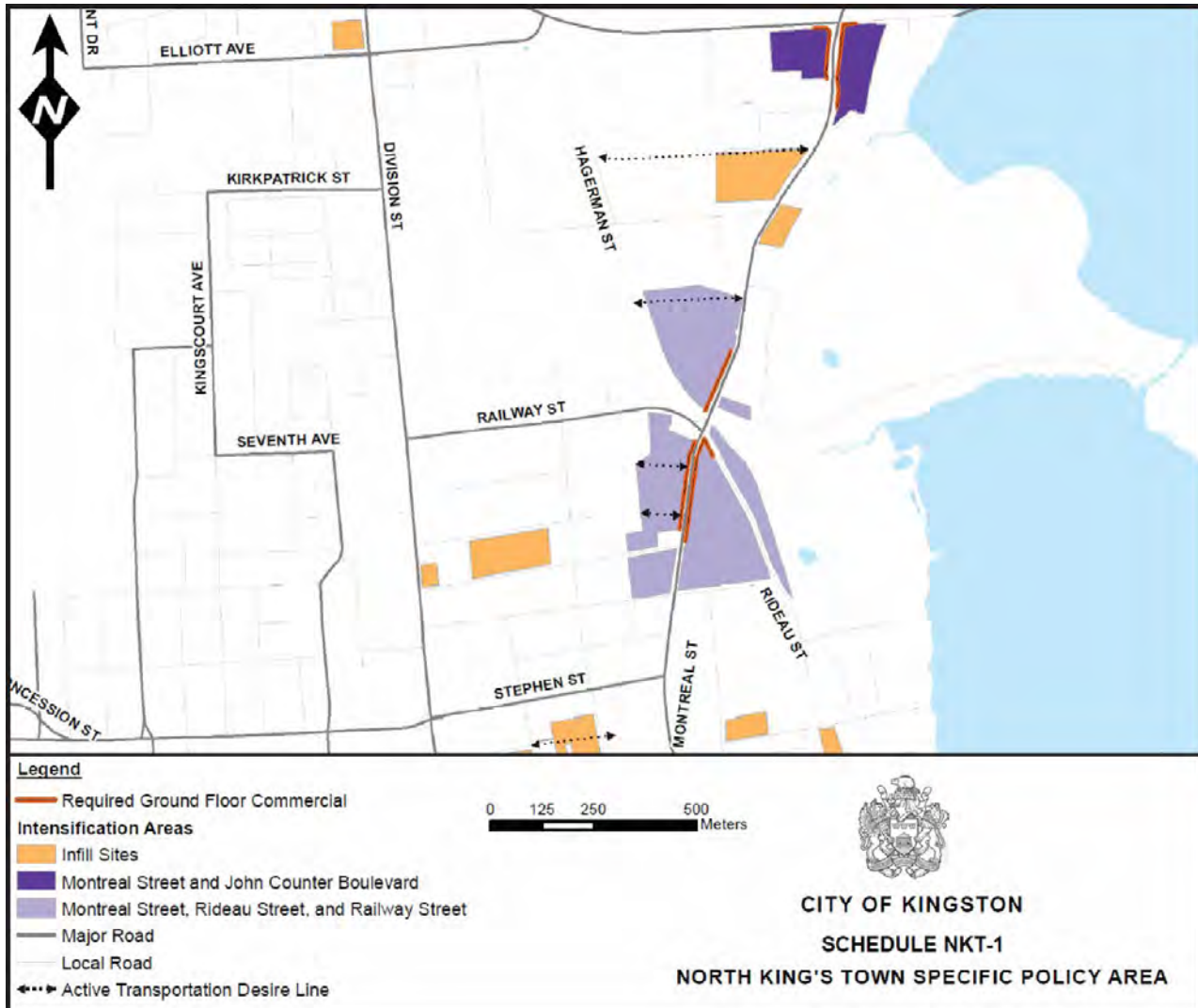


Figure 1: Excerpt of Draft OP Schedule NKT-1 (North King's Town Specific Policy Area)

OP schedule NKT-2 (North King's Town Specific Policy Area Height Map) specifies building heights, including the location for potential height up to 20 storeys subject to a minor variance application. The draft zoning for the intensification areas is proposed to consist of three new zones, Mixed (MU1) Zone 1, Mixed (MU2) 2, and Mixed (MU3) Zone 3, which each include a maximum building height and density, among other zoning provisions.

While it is understood that regulating building height through the ZBL is a standard expectation, building height limits in the OP create unnecessary regulation and will often trigger technical official plan amendments in order to facilitate the intended built form. This approach does not support “shovel-ready” development. The zoning by-law should be the primary planning tool regulating built form. The use of an OP schedule to specify locations where a given building height results in significant constraint to site design, which imposes artificial barriers to site design that significantly increase the risk to applicants, in that they must seek amendments to the OP and ZBL to proceed with development, while undermining the intended feasibility for as-of-right development.

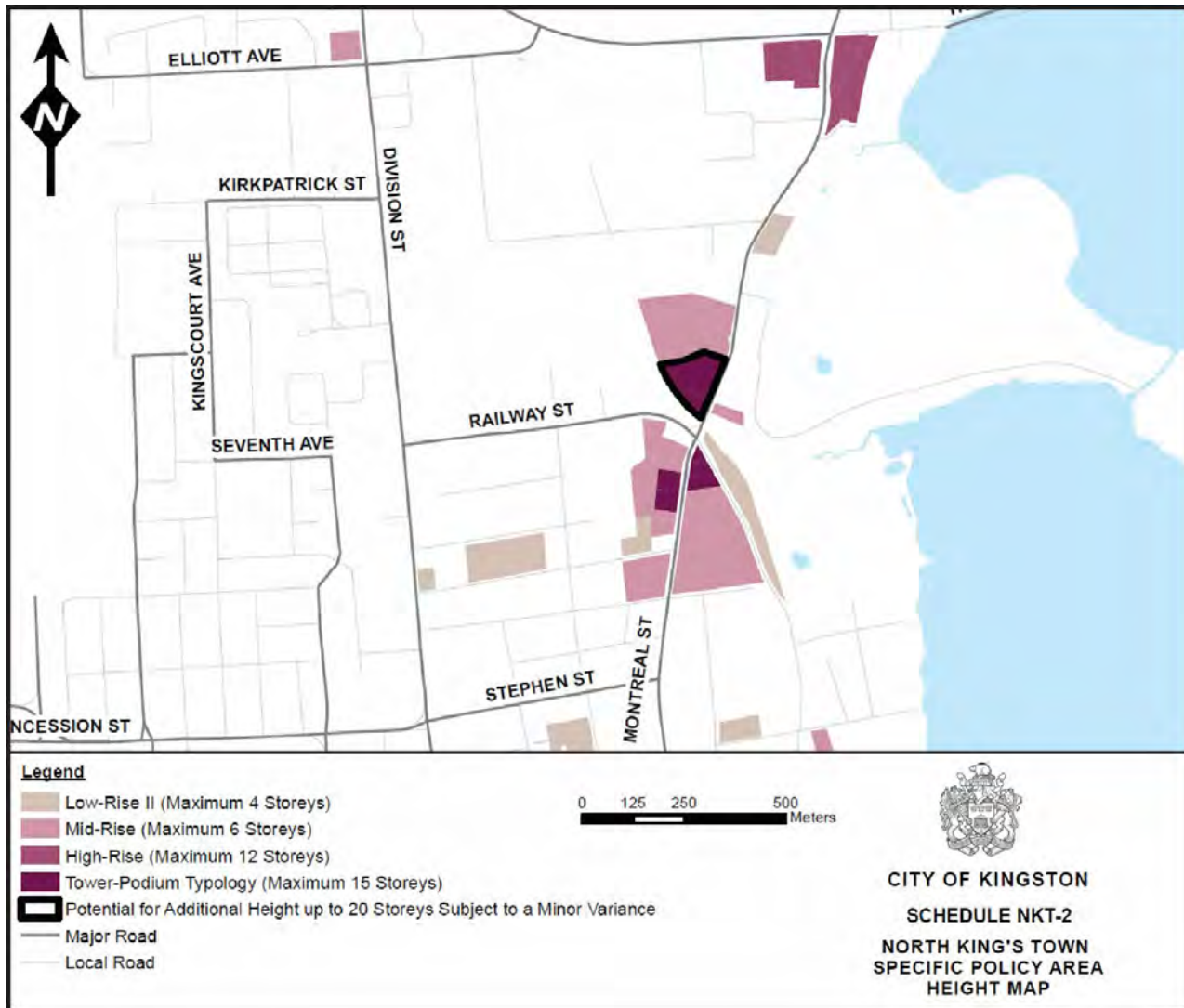


Figure 2: Excerpt of Draft OP Schedule NKT-2 (North King's Town Specific Policy Area Height Map)

As currently proposed, there is no indication the policies and regulations relating to building height, density or massing will result in economically viable development. It is our understanding that a financial analysis is being pursued by staff through a third-party consultant though it has not yet been made available. Our previous concerns expressed through the CKGS appear to have largely been correct thus far, as the economic analyses undertaken in support of that policy framework found that projects would only have economic viability in an extremely narrow set of circumstances, which could not be realized by the majority of developers in the area. We are similarly concerned and expect that any financial modeling for NKT should be based on a broad and realistic threshold for development. We note further than should an analysis be presented that demonstrates a limited set of circumstances for financial viability, this does not mean that the parameters used in the modelling will guarantee that a developer will arrive at the same modeling. We strongly recommend that the City take a broader approach to regulating built form, density and massing and allow for appropriate development to be considered through individual planning applications to allow for greater opportunity to achieve the intended intensification in NKT.

The prescriptive locations for building height and towers will create challenges for applicants when designing individual developments and would trigger official plan amendment, zoning by-law amendment and/or minor variance applications, slowing down and deterring development due to the increased risk and cost.

We recommend that schedule NKT-2 be removed altogether and that maximum building height and density provisions be removed from proposed Mixed (MU1) Zone 1, Mixed (MU2) 2, and Mixed (MU3) Zone 3. Instead of such restrictive provisions, the Secondary Plan should rely on principles for land use and built form compatibility set out in OP policy, including many of the policies already proposed for NKT under section 10H.2. The removal of maximum height provisions in those zones does not result in a carte blanche for building height, as is commonly feared by removing height restrictions. Other performance standards such as parking and amenity area requirements, combined with technical limitations such as servicing capacity, stormwater management, bedrock depth and hardness, and land use compatibility impacts such as traffic impact, shadowing, noise, wind, etc. all combine to impose real limitations on building height and massing. Reliance on these factors, rather than arbitrary limits on building height or density, ensures that good site design takes priority over trying to achieve as much as possible within the prescribed limits. We see this again and again in our practice, where the prescribed limits in zoning and sometimes even in the OP must be amended as they do not facilitate economically viable development, and so these provisions and policies must be amended prior to committing a full array of resources to site design. In support of the comments above, we recommend proposed OP sections 10H.2.3 and 10H.2.4 be reworded as follows:

*10H.2.3 Schedule NKT-1 identifies the intensification areas.*

*10H.2.4. The greatest building height in North King's Town is permitted within 400 metres of the Montreal Street and John Counter Boulevard area and the Montreal Street, Rideau Street, and Railway Street area, and can be permitted through a minor variance application if zoning compliance cannot be achieved. Development shall consider and incorporate transition to minimize the impact of shadow and maximize access to sunlight, sky view, and privacy on neighbouring properties. One or more of the following methods to achieve transition may be incorporated:*

- a. Increased yard setbacks;*
- b. Building stepbacks;*
- c. Generally limiting floorplate size to 850 square metres for buildings with a tower height of 12 storeys or greater;*
- d. Introduction of intervening ground-oriented dwelling or a lower built form that includes ground-oriented units;*
- e. Other approaches informed by good urban design and the policies in Sections 2 and 10H of this Plan.*

These amendments, along with the inclusion of a Holding Overlay that requires applicants to demonstrate suitable transportation and servicing capacity, will ensure the technical aspects of a development are supported along with the proposed design including built form, massing and density.

### **Required Ground Floor Commercial Area**

The extent of Required Commercial Ground Floor as shown on Schedule NKT-1 of the OPA (see Figure 1 above) and Schedule 3 of the ZBA (see Figure 3 below) is very high and in an area where there is currently very limited residential density. While difficult to confirm, there appears to be upwards of 550 metres of linear commercial frontage required along Montreal Street in proximity to the Rideau Street/Railway Street intersection, including a small amount of commercial frontage required along Rideau Street. The City of Toronto's Retail Design Manual recommends a minimum store width-to-depth ratio of 1:3. Anecdotally, ground floor commercial units in mixed use buildings in Kingston appear to commonly fall in the 8-10 metres in width at a minimum, meaning that unit depths should typically fall in the 24-30-metre range to achieve functional commercial units. This translates to 13,200-16,500 square metres (~142,000-177,600 square feet) of commercial floor space. Viewed against the Commercial Land Needs Assessment completed by urbanMetrics for the recently completed municipal comprehensive review, this represents between 10% and 15% of the total commercial floor area warranted for all of Kingston to 2051 (between 1,182,700-1,473,400 square feet). This percentage is even more stark when considering that urbanMetrics allocated 556,600 square feet for the existing urban boundary, which means that the commercial ground floor in NKT would make up between 25% and 32% of the commercial land warranted in Kingston's current urban boundary. Note that these figures do not even include the proposed "Main Street Commercial" designation further south on Montreal Street.



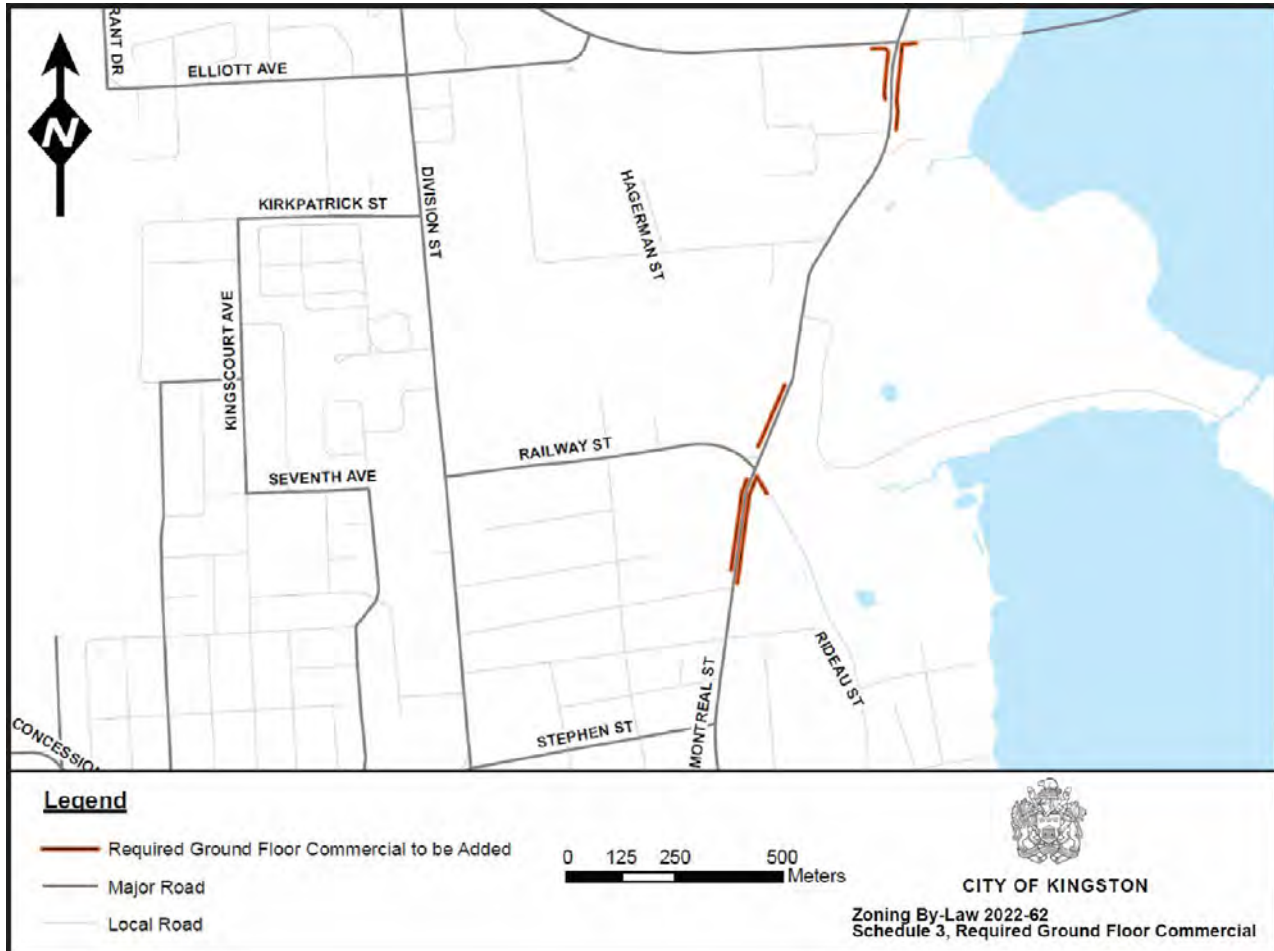


Figure 3: Excerpt of Draft ZBA Schedule 3 (Required Ground Floor Commercial)

While it is likely that site design would ultimately constrain the commercial floor areas to less than those indicated above, the scale of commercial land being required in this portion of NKT is simply too significant to proceed without detailed market analysis. The existing density of development in NKT is insufficient to support the amount of required ground floor commercial mandated in the secondary plan area. Future residential development of NKT will take years to obtain site-specific entitlements followed by development approvals, permits, construction, and occupancy. Commercial tenants will not move into a space until they are confident that there is a sufficient market already present to support them. This means that full commercial occupancy in this area is likely to lag behind full residential build out. In other words, store fronts will remain vacant for many years before being filled, assuming that residential build out in fact occurs as intended which, in our opinion, is unlikely without significant changes to the proposed policy and regulatory framework.

We understand the intent is to support and encourage additional commercial area in NKT, to create a complete and walkable community. In an area such as this, where there is not already a customer base that meets the critical mass needed to support a minimal degree of ground floor commercial uses, it is much more appropriate to simply require a minimum ground floor building height that would allow commercial uses, typically 4.5 metres. This is already proposed as part of the NKT Mixed (MU1, MU2, MU3) Zones. We therefore recommend keeping this minimum ground floor height provision while removing the requirement for new ground floor commercial. This will support the flexibility for conversion of ground floor spaces should the future market conditions of the area support the additional commercial area without unnecessarily limiting the viability of new development by ensuring that a

substantial portion of the ground floor areas would remain unable to generate revenues for potentially extended periods.

### **Active Transportation Desire Lines**

The Active Transportation Desire Lines identified on proposed Schedule NKT-1 (see Figure 1 above) are a source of concern as well. While we understand the intent of these Desire Lines is to provide pedestrian permeability and connectivity in NKT, they do not need to be located where they are shown on proposed Schedule NKT-1. The desire lines as shown appear to be aligned with locations of existing desire lines, many of which align with current or former unopened road allowances that have since been acquired by private property owners.

We understand the intent of these items is to secure mid-block connections across future development sites, however the language and intent are misaligned. "Active Transportation Desire Line" is not a term currently used anywhere in the City's OP and this terminology is generally used to represent a location where a path of travel exists despite site or public realm design. Instead, and more appropriately, the term "Mid-Block Connection" should be used to reference these intended pedestrian connections as that is ultimately the intent of these symbols on the plan.

Mid-block connections are most appropriately negotiated between a property owner and the City through a development application to assess potential and most appropriate locations. As currently proposed, showing the desire lines on NKT-1 could unnecessarily trigger official plan amendment should alternative locations be determined through negotiations with property owners.

We recommend the Active Transportation Desire Lines be removed from proposed Schedule NKT-1 and instead, proposed OP section 10H.2.36 should be reworded as follows:

*10H.2.36. The City will seek to secure Mid-Block Connections through future development applications, where appropriate. Where a Mid-Block Connection is determined to be appropriate, the City will work collaboratively with property owners to ensure future multi-use paths are integrated into a development proposal and provide necessary connections to the existing and planned active transportation network.*

At a minimum, we recommend the Active Transportation Desire Lines shown on proposed Schedule NKT-1 in the area south of Railway Street and North of Stephen Street, be consolidated given their proximity. We will also note these two Active Transportation Desire Line are shown on closed and sold municipal road allowances, which suggests that a previous municipal decision was made to eliminate public use of these through-connections.

### **Industrial Separation**

The proposed ZBA seeks to introduce a new general provision related to industrial separation distances. The proposed provision references both the D-series guidelines, their applicability, and explicitly states the required separation distances. The D-series guidelines provide nuances for determining and identifying separation distances which are not captured by proposed section 4.32.1 as written. By explicitly stating the separation distances in the provision, rather than simply referencing the D-series guidelines, this will lead to unnecessary zoning by-law amendments. We do not have concerns with the introduction of an OP policy referencing the D-series guideline, but zoning provisions that specify the numerical separation distances are highly problematic. A similar example is the Minimum Distance Separation (MDS) formulae required for separation from livestock and manure storage facilities. The zoning by-law does not specify separation numbers for MDS formulae nor does it include the factor tables that could be used to arrive at the calculated separations. It instead simply references the formulae. This is a much more appropriate approach for the D-series guidelines. We recommend the general provision be removed altogether and OP policy be left on its own. As a secondary approach, we would recommend re-wording the general zoning provision as follows, at a minimum:

*4.32.1. Despite anything to the contrary in this By-law, the establishment of a new sensitive use must comply with minimum separation distances from a Class 1, Class 2 or Class 3 industrial facility, as applicable, located within an Employment Zone. Despite clause 3.19.7., this separation distance must be established as per the province's D-series Environmental Land Use Compatibility Guidelines, or any successor thereof.*

This request is particularly significant as the province has once-again signaled its intention to revisit the D-series guidelines. Should they do so, then future housekeeping amendments will inevitably be required each time the province amends the guidelines.

### Summary

We request the following changes be made to the draft OPA and ZBA to support the intended outcome of growth and development in NKT:

1. Remove proposed OP Schedule NKT-2;
2. Reword proposed OP sections 10H.2.3 and 10H.2.4 as follows:
  - 10H.2.3 Schedule NKT-1 identifies the intensification areas.*
  - 10H.2.4. The greatest building height in North King's Town is permitted within 400 metres of the Montreal Street and John Counter Boulevard area and the Montreal Street, Rideau Street, and Railway Street area, and can be permitted through a minor variance application if zoning compliance cannot be achieved. Development shall consider and incorporate transition to minimize the impact of shadow and maximize access to sunlight, sky view, and privacy on neighbouring properties. One or more of the following methods to achieve transition may be incorporated:*
    - a. *Increased yard setbacks;*
    - b. *Building setbacks;*
    - c. *Generally limiting floorplate size to 850 square metres for buildings with a tower height of 12 storeys or greater;*
    - d. *Introduction of intervening ground-oriented dwelling or a lower built form that includes ground-oriented units;*
    - e. *Other approaches informed by good urban design and the policies in Sections 2 and 10H of this Plan.;*
3. Remove the maximum building height and maximum density provisions from proposed zones MU1, MU2, and MU3;
4. Remove Require Ground Floor Commercial on proposed OP Schedule NKT-1 and proposed ZBL Schedule 3;
5. Remove the "Active Transportation Desire Lines" on proposed Schedule NKT-1;
6. Reword proposed OP section 10H.2.36. as follows:
  - 10H.2.36. The City will seek to secure Mid-Block Connections through future development applications, where appropriate. Where a Mid-Block connection is determined to be appropriate, the City will work collaboratively with property owners to ensure the future multi-use paths are integrated into a development proposal and provide necessary connections to the existing and planned active transportation network.;*
7. Reword proposed ZBL section 4.32.1 as follows:
  - 4.32.1. Despite anything to the contrary in this By-law, the establishment of a new sensitive use must comply with minimum separation distances from a Class 1, Class 2 or Class 3 industrial facility, as applicable, located within an Employment Zone. Despite clause 3.19.7., this separation distance must be established as per the province's D-series Environmental Land Use Compatibility Guidelines, or any successor thereof.*

We would be pleased to meet with City staff to discuss our comments further. Should you have any questions or comments, please do not hesitate to contact the undersigned at [REDACTED]

Respectfully,



Kelsey Jones, RPP MCIP  
Senior Planner  
Fotenn Planning + Design



Youko Leclerc-Desjardins, RPP MCIP  
Principal  
Fotenn Planning + Design

Cc: Ben Pilon, BPE Developments

**From:** [Agarwal,Sukriti](#)  
**To:** ["Kerry Hill"](#)  
**Cc:** [Oddie,Niall](#)  
**Subject:** RE: Staff on addressing climate change in North King's Town plan  
**Date:** January 21, 2025 10:25:45 AM

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Hello Kerry,

I am doing well and hope the same for you.

Thank you for your email. We don't have these points included yet in an official document the way they are presented in the email below, however we will be including them in our forthcoming report on NKT. In the meantime, you could refer to the draft Official Plan policies included under the urban and sustainable design section of Exhibit C of this NKT report (starting on PDF page 51):

<https://pub-cityofkingston.escribemeetings.com//filestream.ashx?DocumentId=6486>

In the forthcoming report, we would be happy to include a similar statement about community gardens.

Thanks,

Sukriti

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**From:** Kerry Hill [REDACTED]  
**Sent:** January 19, 2025 3:03 PM  
**To:** Agarwal,Sukriti <sagarwal@cityofkingston.ca>  
**Subject:** Fwd: Staff on addressing climate change in North King's Town plan

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi Sukriti,

I hope that this finds you well. Vicki Schmolka forwarded to me your answer to her question regarding how climate change mitigation would be incorporated into the NKT Secondary Plan.

I am hoping that you could let me know if these points are published in an official document anywhere? I've looked on Get Involved, but no luck.

I am hoping to direct my newsletter readers to the information encouraging developers to plant Miyawaki forests on their properties. This is a huge step forward for the climate! I would like to have seen a similar statement for provision of space for community gardens, also, especially in view of the food insecurity many citizens face and the recent recognition of this crisis by Council.

Thanks for your help,

Kerry Hill  
[REDACTED]

----- Forwarded message -----

From: <[REDACTED]>  
Date: Fri, Jan 17, 2025 at 3:00 PM  
Subject: Staff on addressing climate change in North King's Town plan  
To: Chris Gusen [REDACTED], Gavin Hutchison  
[REDACTED], Kerry Hill [REDACTED], Joan Jardin  
[REDACTED], mary farrar [REDACTED] Nathan  
Nesdoly [REDACTED]

This is from Sukriti Agarwal in answer to my email, below, asking about how the proposal for North King's Town addresses climate change. I have her permission to share it with you.

The North King's Town project aims to address climate change in several ways, for example:

- Identifying opportunities for a significant amount of infill, intensification and redevelopment within the existing urban area of the City. This approach reduces pressure on natural heritage systems, conserves resources, contributes to more efficient infrastructure and service delivery, and reduces greenhouse gas emissions by reducing distances residents would need to travel to access places of employment, commerce and recreation.
- Creating a new Mixed Use designation and enabling mixed use development within all identified intensification areas, which can promote active transportation and reduce greenhouse gas emissions by locating residential uses in close proximity to supporting commercial uses.
- Establishing densities to facilitate mid- and high-rise development, which contributes to a more efficient use of land and resources.
- Promoting active transportation and transit use by locating intensification areas along (or within walking distance to) express transit routes and reducing minimum parking requirements for residential developments (while preserving a maximum parking requirement within zoning), which all serve to reduce greenhouse gas emissions.
- Encouraging developments to plant "little forests" within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and contribute to greater biodiversity, which can assist in mitigating urban heat island effects by increasing tree canopy, habitat loss and promote infiltration.
- Encouraging development applications to include fruit trees, nut trees and other edible plantings within their planting plans to promote food security and local

food options, which can reduce greenhouse gas emissions associated with food transportation, while also serving to establish tree canopy within the urban area.

- Establishing minimum landscaped open space requirements for the intensification sites.

The North King's Town Servicing Report includes a detailed chapter on stormwater management, with innovative approaches and targets. Staff will be utilizing the NKT Servicing Report to identify study requirements for future development applications on the proposed intensification sites. Parkland dedication requirements for future development will be as per the Planning Act and the City's Parkland Conveyance By-Law.

Thanks,

Sukriti

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**From:** [REDACTED]  
**Sent:** January 14, 2025 12:44 PM  
**To:** Agarwal,Sukriti <[sagarwal@cityofkingston.ca](mailto:sagarwal@cityofkingston.ca)>  
**Subject:** RE: A few comments on PC-25-001

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Hello Sukriti:

What worries me the most as a resident is the lack of integration of any measures that address local ways to mitigate climate "change." Do you feel Planning has no mandate to do this?

For instance, Montreal is creating [sponge parks](#) to soak up water from excessive rain events. The North King's Town plan shows no measures for rainwater management beyond the usual.

How did staff account for climate change in this vision for North King's Town?

Vicki

**Oddie,Niall**

---

**From:** Joyce Hostyn <[REDACTED]>  
**Sent:** January 22, 2025 2:36 PM  
**To:** North King's Town Secondary Plan  
**Subject:** North King's Town secondary plan

Caution: This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

I saw in the No Clearcuts Kingston newsletter that the updated plan includes little forests. Where can I view the updated plan? And can you add me to the list for updates.

Thanks so much Niall!

Joyce



**Oddie,Niall**

---

**From:** Oddie,Niall  
**Sent:** January 23, 2025 11:17 AM  
**To:** [REDACTED]  
**Cc:** Agarwal,Sukriti  
**Subject:** North King's Town  
**Attachments:** 2025-01-22 - Joyce Hostyn - Little Forests.pdf

Hi Joyce,

I have added you to the project mailing list. You'll receive an email in the coming couple of weeks advising of a public meeting at Planning Committee for the formal Official Plan and Zoning By-law amendments.

Copies of the draft materials are available on our [DASH platform](#) for review, using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

The reference to "Little Forests" is contained within the 'Proposed Official Plan Modifications' document. For convenience, I have copied the wording below:

"10H.2.14. New development is encouraged to incorporate fruit trees, nut trees and other edible plantings to promote food security and facilitate opportunities for foraging. The trees should be situated appropriately to avoid creating hazards associated with fallen fruit or nuts.

10H.2.15. New development is encouraged to establish 'Little Forests' within their planting plans, where a variety of trees and shrubs are planted in higher densities to mimic natural conditions and provide greater biodiversity.

10H.2.16. Where development is proposed adjacent to a public park or open space, including the K&P and Waterfront Trails, the public spaces should be integrated into the design, including pedestrian connections, façade 13 designs and landscape connections. Loading, parking and servicing areas should be screened from view with fencing and landscaping."

Please let me know if there are any further questions or comments.

Thanks  
Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3259  
[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)



The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

Sukriti Agarwal  
Manager, Policy Planning  
City of Kingston  
1211 John Counter Boulevard  
Kingston, ON K7K 6C7

January 23, 2025

Dear Sukriti Agarwal,

KFL&A Public Health would like to express our strong support for the vision, goals, and strategies outlined in the proposed North King's Town (NKT) Secondary Plan. We are pleased to be involved in this collaborative and inclusive approach, recognizing the critical role the built environment plays in shaping healthy, sustainable communities.

There is increasing evidence that healthy and sustainable communities that are compact, pedestrian-friendly, transit-supportive, and conducive to a variety of uses positively impact population health and health equity. We believe NKT has the potential to become one of these communities.

KFL&A Public Health supports the NKT Mobility Plan, which reflects the vision of a walkable, pedestrian-oriented, and multi-modal network with minimal emphasis on motor vehicles. We are excited about the long-range plan for improving safety, connectivity, and continuity in the overall transportation network, as well as the ambitious, but achievable, future NKT mode share target. We believe that proposed changes to the NKT built environment will support walking and cycling, encourage pedestrian travel and physical activity and will play a large role in shaping a healthy, sustainable NKT community.

KFL&A Public strongly supports initiatives aimed at enhancing safety in our streets, neighborhoods, and parks, recognizing that such improvements are essential to fostering greater physical activity. We fully endorse the proposed modifications to the NKT built environment, particularly those that have the potential to increase safety and reduce injury rates for vulnerable road users and other priority populations who face disproportionate risk. The introduction of traffic calming measures, including road humps, raised crosswalks, and road narrowing, has proven effective in reducing road-related injuries and fatalities, making them critical steps toward creating safer and more accessible spaces for all

[REDACTED]

[REDACTED]

[REDACTED]

members of the NKT community.

KFL&A Public Health supports the proposed amendments to Zoning By-Law 2022-62 and the Official Plan to implement the North King's Town Specific Policy Area, which includes provisions for a Mixed-Use designation. These land use policy changes are aligned with promoting active living environments, encouraging the development of spaces that support physical activity and well-being. By integrating residential, commercial, and recreational uses, these amendments contribute to the creation of communities that prioritize accessibility, walkability, and overall physical activity, fostering healthier lifestyles for residents.

We believe that the NKT Secondary Plan is a thoughtful and forward-looking approach to fostering a built environment that supports healthy living. The Plan, in its entirety, emphasizes social connection and ensures access to features that promote physical activity and mental well-being. By integrating these elements into the community's design, the NKT Secondary Plan has the potential to create a positive, long-lasting impact. KFL&A Public Health is confident that the implementation of the NKT Secondary Plan implementation will benefit both current and future generations by enhancing quality of life and serving as a model for other areas of the Kingston and beyond.

KFL&A Public Health is eager to continue supporting this initiative and collaborating with the NKT project team to ensure that these principles are effectively integrated into the planning and development of our community. Thank you for your leadership on this important project and we look forward to continuing our partnership and contributing to its success.

Sincerely,



Amie Krasnozón RN, BSc(N), BA, MHE  
Public Health Nurse, Healthy Communities  
Phone: [REDACTED]

CC: Niall Oddie  
Niki Van Vugt

[REDACTED]

[REDACTED]

[REDACTED]

**Oddie, Niall**

---

**From:** Oddie, Niall  
**Sent:** February 3, 2025 2:24 PM  
**To:** [REDACTED]  
**Cc:** Osanic, Lisa; Agarwal, Sukriti  
**Subject:** RE: North King's Town Project Meeting Notice

Good afternoon,

My sincere apologies for the delay in responding back to you and thank you for the phone conversation this morning. Responses to your questions have been provided in red text, directly into your email below.

I understand from our conversation that your questions are related mainly to the St. Lawrence Ward Heritage Character Area. Please note that these proposed amendments would not be a heritage designation under the *Ontario Heritage Act*. These are Official Plan policies to help guide any future development and you would not require a heritage permit to undertake any modifications to your house at 61 Main Street as a result of these amendments.

I understand that you have had difficulty using DASH in the past. I, unfortunately, do not have printed copies of the proposed amendments or supporting documentation to show you in person. However, you are welcome to come into the office at 1211 John Counter Boulevard, and I can go through the documents with you or make a computer available for your review. I have received comments from other community members that the following instructions are helpful when utilizing our [DASH platform](#):

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

From our conversation, I believe you would be most interested in reviewing:

- "3. Proposed Official Plan Modifications.pdf"
- "Cultural Heritage Study – Final Draft.pdf"

Please let me know if you would like to further discuss these proposed amendments, supporting studies or the content of Staff [Report Number PC-25-001](#).

Thanks

Niall



**Niall Oddie M.P.I, MCIP, RPP (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard



216 Ontario Street Kingston, ON K7L 2Z3  
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**From:** Ed Horba <[REDACTED]>  
**Sent:** February 3, 2025 11:19 AM  
**To:** Oddie,Niall <noddie@cityofkingston.ca>  
**Cc:** Osanic,Lisa <losanic@cityofkingston.ca>  
**Subject:** Re: North King's Town Project Meeting Notice

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Niall,

I sent an email on Jan 7, 2025 and have not heard back. Can you please advise if you received the email and if a response to the questions is forthcoming?

Other questions as they relate to properties on Main Street:

Have I (Ed Horba) been added to the mailing list for the project?

Yes, you have been added to the project mailing list. You will receive the next project email.

Who identified the need for the official plan re designation in the area in question?

This is a City-initiated application serving to implement the findings of the North King's Town (NKT) project.

When were the official plan and zoning amendments added to the project?

The NKT project was always intended to be implemented through Official Plan and Zoning By-law amendments. Draft land use considerations have been included in various progress drafts of the NKT materials over the past couple of years, however the Community Meeting in December, 2025 was the first time complete draft Official Plan and Zoning By-law amendments were presented for public comment.

When was the public made aware of the official plan and zoning amendments on the project?

Notice associated with the Community Meeting held December 5, 2024 was the first time that a comprehensive mailing list to all properties subject to the proposed Official Plan or Zoning By-law amendments, and properties within 120 metres of these lands, was distributed. Prior to this, correspondence relating to the NKT project was distributed to the project email list (~400 residents and interested parties), posted on Get Involved Kingston, and the City's website.

What was the rationale for the redesignation and where is the documentation to support it?

The NKT project was initiated for a variety of reasons, including identifying opportunities for infill redevelopment within a centrally located neighbourhood and further assessing the Wellington Street Extension. The Staff report (available through the link in my December 20, 2024 correspondence below) provides a discussion of the NKT project and the rationale for the various amendments.

From our conversation, I believe you are most interested in the proposed expansion of the St. Lawrence Ward Heritage Character Area, as this would be the only modification impacting 61 Main Street. I understand from our

phone call that you have had difficulty accessing DASH in the past. For your convenience, I have included an image below of the proposed text amendments associated with the proposed expansion of the St. Lawrence Ward Heritage Character Area.

Please note, that the proposed modifications relating to the St. Lawrence Ward Heritage Character Area are not a designation under the *Ontario Heritage Act*; these proposed amendments would not require you to obtain a heritage permit prior to undertaking any modifications to your home. These proposed amendments would not prevent you from being able to consider an addition to your home, or convert a portion of your home into an apartment. These proposed amendments are influenced from the findings of the NKT Cultural Heritage Study, which is only available on DASH as it is too large to email.

### St. Lawrence Ward Heritage Character Area

7.3.D.5. The St. Lawrence Ward Heritage Character Area, as shown on Schedule 9, is one of the oldest areas of the City with an urban style that has survived since the 1800's. ~~It is the intent of this Plan:~~ It is recognized that the heritage character of the area was created through the combination of buildings, street pattern, varying street widths and public spaces. It is the intent of this Plan to maintain the heritage integrity of the area with the application of the following policies:

- ~~a. to recognize the heritage character of the area as created through the combination of buildings, street pattern, varying street widths and public spaces; and,~~
- ~~b. to undertake further investigations that will define appropriate boundaries and conservation policies.~~
- a. The City will undertake further investigations that will define appropriate boundaries of a future heritage conservation district study area;
- b. *Development* involving the demolition of existing buildings will be discouraged. The City may require a heritage impact assessment prior to permitting demolition, where applicable.
- c. *Development* will be encouraged to adaptively re-use buildings in a manner that conserves the attributes that contribute to the heritage character of the area; and
- d. Applications requiring *Planning Act* approval may be required to submit studies, such as urban design or heritage impact assessment, to demonstrate how the proposed *development* is compatible with the characteristics of the St. Lawrence Ward Heritage Character Area and any adjacent cultural heritage resources. The North King's Town Cultural Heritage Study should be consulted for characteristics and context.

Who established the new boundaries?

It is assumed that this question is asking about the proposed boundaries of the St. Lawrence Ward Heritage Character Area as that was the focus of our conversation this morning. As noted above, the proposed

amendments relating to the St. Lawrence Ward Heritage Character Area are influenced by the findings of the NKT Cultural Heritage Study, prepared by Bray Heritage.

Why were the boundaries established as shown, why not a larger or smaller area?

The proposed boundaries relate back to the recommendations identified within the NKT Cultural Heritage Study, prepared by Bray Heritage, and generally follow the sub-area boundaries identified within the Cultural Heritage Study (Heritage Sub-Areas map, page 44) and areas identified as having a greater concentration of potential heritage properties (Heritage Properties map, page 45).

As most of the current City council appears to be pro development regardless of the impacts to residents, why would council approve this if it makes it harder to develop?

The NKT project has identified areas intended for residential infill and mixed use intensification within the area generally bounded by Division Street, John Counter Boulevard, the Great Cataraqui River and Colborne / Ordnance Streets. The identified intensification areas are primarily larger, vacant or underutilizes properties in locations accessible by transit and active transportation. The NKT project is trying to put in place the Official Plan and Zoning By-law amendments necessary to realize these development within the identified intensification areas; we are trying to make it easier to redevelop within the identified intensification areas.

The lands associated with the proposed St. Lawrence Ward Heritage Character Area are not intended for wide-scale redevelopment, given the unique character of the area and the concentration of potential built heritage resources. This is not to say that property owners could not apply for minor variances or rezoning applications for their property to change the use or to include a different building type on the property. Should those private applications be submitted, the proposed policies for the St. Lawrence Ward Heritage Character Area would be taken into consideration, together the rest of the applicable Official Plan policies. Please note, the proposed policies associated with the St. Lawrence Ward Heritage Character Area use uses such as “encourage” and “discourage”, which are not the same as “require” or “prohibit”.

Ed Horba

On Tue, Jan 7, 2025 at 10:16 AM Ed Horba [REDACTED] wrote:

Hello Niall,

Thank you for your response. Please add me to the mailing list for all future public communications on the project as I live at 61 Main Street which is included as part of Lands to be added to the heritage character area under the revisions to the official plan. My understanding from watching the recording is that it will be more difficult to rezone based on the changes proposed to my address.

Please outline what the specific changes are to my property and how it will be more difficult to obtain rezoning. Will there be any compensation for such increased expenses?

One point that seemed unusual is that the City's requirements for Archaeological studies was to be somehow increased, however this is a provincial jurisdiction and guidelines for the requirements to complete such work are laid out by the province, the City only has to ensure that the guidelines are followed, so I am not sure what the change is, please clarify.



Thank you

Ed Horba

On Fri, Dec 20, 2024 at 2:47 PM Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)> wrote:

Hi Ed,

A copy of the staff report and a recording of the December 5, 2024 Community Meeting presentation and question/answer session are available through the following link:

<https://events.cityofkingston.ca/default/Detail/2024-12-05-1800-Planning-Committee>

We are in the process of collecting feedback on the draft materials and will be submitting formal Official Plan and Zoning By-law amendments in early 2025. Once these formal applications are submitted, a Public Meeting will be held at Planning Committee, where the information will be presented, and opportunities to ask questions will be provided. Notice of the meeting will be distributed in accordance with the *Planning Act* (owners of land impacted by the changes and owners of lands within 120 metres of the changes) and to the project mailing list. Further, notice will also be placed within the Whig Standard, the project website and the City calendar

Several reports associated with the North King's Town project are available on our [DASH platform](#) for review. Unfortunately, I cannot send copies of these reports due to file sizes. You can access the materials using the following steps:

1. Use the search bar near the top right-hand corner to search for application "D01-011-2024".
2. Select "Record Info"
3. Select "Supporting Information" from the drop-down menu.

We anticipate submitting the formal Official Plan and Zoning By-law amendments in early 2025. To ensure that we have sufficient time to consider comments and make any necessary revisions to the draft materials presented at the Community Meeting, we would appreciate questions and comments by January 10, 2025. This being said, you may submit questions and comments at any time before Council makes a decision on the formal applications.

I will be on vacation until January 6, 2025, but I would be pleased to meet with you to discuss the project in greater detail and answer any further questions that you may have.

Thanks

Niall



**Niall Oddie M.PI, MCIP, RPP (he/him/his)**

Senior Planner

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City of Kingston

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[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)

The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** Ed Horba <[REDACTED]>  
**Sent:** December 20, 2024 10:48 AM  
**To:** Oddie, Niall <[noddie@cityofkingston.ca](mailto:noddie@cityofkingston.ca)>  
**Subject:** North King's Town Project Meeting Notice

**Caution:** This email is from an external source. Please exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Niall

I received notice by mail on December 19, 2024 of a community meeting for the project. As my notice was received after the December 5th, 2024 meeting I trust that there will be another meeting so that all those that did not get the notice in time will be able to view the information and to comment. I would like to provide comments but have not seen any of the project details.

Can you please provide details regarding the public comment process and timelines since there has been a delay due to postal strike and any other information on the project. (I am not comfortable using DASH).

Thank you

Ed Horba



# Public Notice Notification Map

File Number: D35-001-2025

- Subject Lands
- 120m Public Notification Boundary
- Properties In Receipt Of Notice (MPAC)

