



**City of Kingston  
Report to Planning Committee  
Report Number PC-26-003**

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**To:** Chair and Members of the Planning Committee  
**From:** Paige Agnew, Commissioner, Growth & Development Services  
**Resource Staff:** Tim Park, Director, Planning Services  
**Date of Meeting:** January 29, 2026  
**Subject:** Recommendation Report  
**File Number:** D14-013-2025  
**Address:** 525 Princess Street (being 507, 521, 523, 525 and 531 Princess Street, and 347, 349 and 351 Alfred Street) and 555 Princess Street (being 555, 557 and 559 Princess Street)  
**District:** Williamsville  
**Application Type:** Zoning By-Law Amendment  
**Owner:** 555 Princess Inc.  
**Applicant:** Fitzrovia

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**Council Strategic Plan Alignment:**

Theme: 1. Support Housing Affordability

Goal: 1.2 Promote increase in purpose-built rental housing.

**Executive Summary:**

The following is a report recommending approval to the Planning Committee regarding an application for a zoning by-law amendment submitted by Fitzrovia, on behalf of 555 Princess Inc., with respect to the two subject sites referred to as 525 Princess Street, which includes the properties addressed as 507, 521, 523, 525 and 531 Princess Street, and 347, 349 and 351 Alfred Street; and 555 Princess Street which includes the properties addressed as 555, 557 and 559 Princess Street.

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525 Princess Street is situated east of Alfred Street and measures approximately 0.39 hectares in size with road frontage along Princess Street to the south, Chatham Street to the east, Creighton Street to the north and Alfred Street to the west. The east site is currently developed with 3 mixed-use buildings and 2 surface parking lots. The site is designated Main Street Commercial and subject to Site Specific Policy Area Number 73 in the Official Plan (Exhibit E – Official Plan, Land Use). The site is zoned WM1 Zone (Williamsville Zone 1) and subject to Exception Overlay Number E83 and Holding Overlay Number H8 in Kingston Zoning By-Law 2022-62, as amended (Exhibit G – Zoning By-Law Number 2022-62).

555 Princess Street is situated west of Alfred Street and measures approximately 0.22 hectares in size with road frontage along Princess Street to the south and Alfred Street to the east. The site is currently undeveloped. The site at 555 is designated Main Street Commercial and subject to Site Specific Policy Area Number 73 in the Official Plan (Exhibit E – Official Plan, Land Use). The property is zoned WM1 Zone (Williamsville Zone 1) and subject to Exception Overlay Number E84 and Holding Overlay Number H41 in Kingston Zoning By-Law 2022-62, as amended (Exhibit G – Zoning By-Law Number 2022-62).

The current application seeks to amend Exception Overlay Numbers E83 and E84 to facilitate modifications to the 10-storey mixed-use built form previously approved for the sites through application D35-001-2019. The current proposal continues to contemplate 10-storey mixed-use buildings on each site. An elevated pedestrian bridge over Alfred Street has been introduced to the revised proposal to connect both buildings at the 4<sup>th</sup> storey. Proposed amendments include changes to permitted built form and massing through each height map, incorporation of previous parking reduction approvals, reduction in required accessible parking spaces, exemption from certain amenity space requirements, permissions for permitted projections and permissions for reduced bike parking space requirements.

The draft Zoning By-Law amendment is presented in Exhibit A (Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62). The recommendations of Report PC-26-003 includes a recommendation to authorize the Manager of Development Engineering to enter into an encroachment agreement necessary to facilitate the proposed pedestrian bridge.

The proposal, as revised, is consistent with the Provincial Planning Statement, conforms to the City of Kingston Official Plan, and represents good land use planning. The proposed development provides an increase in housing within the urban area where existing infrastructure can support the uses, supports active transportation, and is well situated within walking distance of commercial and recreational amenities helping support increased public transit ridership, and would contribute to high quality urban design.

**Recommendation:**

**That** the Planning Committee recommends to Council:

**That** the application for a zoning by-law amendment (File Number D14-013-2025) submitted by Fitzrovia, on behalf of 555 Princess Inc., for the properties municipally known as 525 Princess Street and 555 Princess Street be approved; and

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**That** Kingston Zoning By-Law Number 2022-62, as amended, be further amended, as per Exhibit A (Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62) to Report Number PC-26-003; and

**That** Council determines that in accordance with Section 34(17) of the *Planning Act*, no further notice is required prior to the passage of the by-law; and

**That** the amending by-law be presented to Council for all three readings; and

**That** Council authorize the Manager, Development Engineering, to approve an encroachment agreement for the pedestrian bridge between 555 Princess Street and 525 Princess Street, in a form satisfactory to the City Solicitor or their designate and the Director, Public Works and Solid Waste or their designate.

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**Authorizing Signatures:**

ORIGINAL SIGNED BY COMMISSIONER

**Paige Agnew, Commissioner,  
Growth & Development Services**

ORIGINAL SIGNED BY CHIEF ADMINISTRATIVE OFFICER

**Lanie Hurdle, Chief  
Administrative Officer**

**Consultation with the following Members of the Corporate Management Team:**

Jennifer Campbell, Commissioner, Community Services	Not required
Neil Carbone, Commissioner, Corporate & Emergency Services	Not required
David Fell, President & CEO, Utilities Kingston	Not required
Desirée Kennedy, Chief Financial Officer & City Treasurer	Not required
Jenna Morley, City Solicitor	<input checked="" type="checkbox"/>
Ian Semple, Commissioner, Transportation & Infrastructure Services	Not required

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**Options/Discussion:****Statutory Public Meeting**

This recommendation report forms the basis of a statutory public meeting at Planning Committee. Anyone who attends the statutory public meeting may present an oral submission, and/or provide a written submission on the proposed application. Also, any person may make written submissions at any time before City Council makes a decision on the application.

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of Kingston to the Ontario Land Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of Kingston before the by-law is passed, the person or public body is not entitled to appeal the decision. If a person or public body does not make oral submissions at a public meeting, or make written submissions to the City of Kingston before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

Planning Committee will consider the recommendations in this report and make its recommendation to City Council at this meeting.

Anyone wishing to be notified of Council's decision on the subject application must submit a written request to:

Chanti Birdi  
The Corporation of the City of Kingston  
Planning Services  
216 Ontario Street  
Kingston, ON K7L 2Z3  
613-546-4291 extension 3273  
[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)

**Background and Decision Date**

In accordance with By-Law Number 2007-43, this application was subject to a pre-application. Following the pre-application process, a complete application was submitted by the applicant and was deemed to be complete as of October 16, 2025. A Community Meeting was held at Planning Committee on November 20, 2025. A summary of the feedback received at the Community Meeting is provided in the Public Comments section of this Report.

In accordance with the *Planning Act*, this application is subject to a decision by Council on or before January 14, 2026, which is 90 days after a complete application was received. In the absence of a decision by Council in this timeframe, the applicant may exercise their right to appeal to the Ontario Land Tribunal (OLT). The application has gone beyond the 90-day review time as the applicant has been working through the detailed technical review comments provided by staff.

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## Site Characteristics

The east site at 525 Princess Street contains the civic addresses 507, 521, 523, 525 and 531 Princess Street, and 347, 349 and 351 Alfred Street. The site is approximately 0.39 hectares in size with road frontage along Princess Street to the south, Chatham Street to the east, Creighton Street to the north and Alfred Street to the north. The east site is currently developed with 3 mixed-use buildings and 2 surface parking lots. The existing buildings would be demolished to accommodate redevelopment of the site.

The west site at 555 Princess Street contains the civic addresses 555, 557 and 559 Princess Street. The site is approximately 0.22 hectares in size with road frontage along Princess Street to the south and Alfred Street to the east. The site is currently undeveloped.

Both sites are located adjacent to Kingston Transit services. A bus stop providing westward travel is located in front of 555 Princess Street, on the north side of Princess Street approximately 30 metres west of Alfred Street. This stop serves routes 4 (Cataraqui Centre Via Princess St), 501 (Express – Downtown via Princess St) and 502 (Express – Cataraqui Centre via Princess). A bus stop providing eastward travel is located on the south side of Princess Street approximately 15 metres west of Alfred Street. This stop serves routes 702 (Express – Downtown), 702 (Express – King's Crossing via Downtown), and 4 (Downtown via Princess St). Both bus stops are accessible by public sidewalk and support pedestrian crossings at a signalled intersection at Princess Street and Alfred Street.

The subject properties are located within the Williamsville Renewal project area which will see the future reconstruction of Princess Street between Division Street and Albert Street. Upgrades through this project may include the relocation of bus stops, however, it is anticipated that the sites will continue to be in close proximity to bus stops and route options.

Bicycle infrastructure also exists along this section of the Princess Street corridor and within the immediate area including east-west along both Brock and Johnson Streets, and north-south along portions of Division Street. Communauto, a car-sharing service that operates within the City, maintains vehicles at two locations within approximately 450 metres walking distance of the two sites including at Brock and Division Street and at the Memorial Centre, while an additional two car-sharing vehicles are available at a slightly further distance of 1 kilometer at the intersections of Albert and Union, and Queen and Bagot.

In general, the surrounding area is defined by its walkable character with access to many commercial, institutional, and recreational amenities including those found along the Princess Street corridor and the City's central business district which begins at Division Street approximately 300 metres distance east of the subject property.

## Proposed Application and Submission

The applicant is proposing to redevelop the two sites in a manner broadly consistent to the original plan for two 10-storey mixed-use buildings with ground floor commercial uses and upper-storey residential use which was approved through application D35-001-2019. The two buildings are now

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proposed to be connected by way of an elevated pedestrian bridge at the fourth storey of each building. The elevated bridge would be located at least 11 metres above the street level and is intended to connect the indoor amenity areas of each building to provide increased opportunities for shared access to differently programmed amenity spaces within each building. The location of the proposed bridge is shown on architectural drawings (Exhibit H – Architectural Drawing Packages) and is reflected in the height maps of the proposed Exception Overlays (Exhibit A – Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62).

The bridge is proposed near the lot lines farthest from Princess Street. Permission for a 0-metre setback along Alfred Street is proposed to enable the bridge to be constructed. Prior to construction, the applicant will be required to execute an encroachment agreement with the City for the structure over City's lands. This report includes a recommendation to authorize the Manager of Development Engineering to approve the encroachment agreement as described within this report below.

In addition to the introduction of a pedestrian bridge, the proposed amendment would also serve to increase the setback from the properties' Princess Street frontage from what was a low of 1.0 metre previously to a low of 2.0 metres which increases to 3.0 metres at the west corner of Alfred Street in the new proposal.

The building at 525 Princess Street is proposed to contain 316 upper-storey residential units with a mix of studio, one-bedroom, two-bedroom and three-bedroom units. The unit composition includes 13 three-bedroom townhouse units. The ground floor of the building is proposed to contain 910 square metres of commercial space. The 10-storey building is proposed to have a minimum streetwall height of 18.7 metres and a maximum streetwall height of 25.35. A streetwall is the wall of a building facing a street line. The total height of the building is 44.2 metres, including a 5.4 metre enclosed mechanical penthouse. The mechanical penthouse would have a setback of 6.5 metres from Princess Street and 5.5 metres from Alfred Street. The building is proposed to use stepbacks to minimize impacts on the pedestrian realm and adjacent properties. The proposed stepbacks are secured through the height map of the proposed Exception Overlay.

The site at 525 Princess Street proposes two vehicular accesses. Access to a one-storey underground parking garage for residents is proposed via an entrance off Alfred Street. The underground parking garage is proposed to contain 63 standard parking spaces, 6 accessible parking spaces and 286 long-term bike parking spaces.

A second access is proposed via Creighton Street to access 34 short-term bike parking spaces, a loading space and a short-term delivery space. Signage related to short-term bike parking and short-term delivery spaces will be secured through the site plan control process and the architectural drawings provided demonstrate the site can accommodate these features.

The building at 555 Princess Street is proposed to contain 172 residential units with a mix of studio, one-bedroom, two-bedroom and three-bedroom units. The ground floor is proposed to contain 350 square metres of commercial space. The 10-storey building is proposed to have a minimum streetwall height of 10.1 metres and a maximum streetwall height of 19.7 metres. The

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total height of the building is 44.4 metres, including a 5.4 metre enclosed mechanical penthouse. The mechanical penthouse would have a setback of 6.5 metres from Princess Street and approximately 3.6 metres from Alfred Street. The proposed setbacks are secured through the height map of the proposed Exception Overlay and are afforded a small degree of permitted variance from the height and setbacks shown on the height map.

The site at 555 Princess Street proposes access to a one-storey underground parking garage accessed from Alfred Street. The parking garage is proposed to contain 27 standard parking spaces, 6 small car parking spaces and 2 accessible parking spaces.

In support of the application, the applicant has submitted the following:

- Cover Letter, prepared by Fitzrovia
- Architectural Drawing Packages, prepared by Hariri Pontarini Architects;
- Planning Justification Report, prepared by Fotenn Planning + Design;
- 3D Building Modelling, prepared by Hariri Pontarini Architects;
- Shadow Study, prepared by Hariri Pontarini Architects
- Topographic Survey, prepared by Hopkins Chitty Land Surveyors Inc.;
- Pedestrian Level Wind Study, prepared by Gradient Wind Engineers & Scientists; and
- Draft Height Map, prepared by Fitzrovia.

All submission materials are available online through the Development and Services Hub (DASH) at the following link, [DASH](#), using “Look-up a Specific Address”. If there are multiple addresses, search one address at a time, or submission materials may also be found by searching the file number.

### **Provincial Planning Statement**

The Provincial Planning Statement (2024) provides policy direction on matters of provincial interest related to land use planning and development, which are intended to be complemented by local policies addressing local interests.

The subject sites are located within the Urban Boundary of the City of Kingston, which is considered a Settlement Area within the Provincial Planning Statement (PPS). The PPS directs growth and development to Settlement Areas to make strategic use of existing infrastructure and encourage a mix of land uses and complete communities.

The proposed development is consistent with provincial policy that directs growth to urban settlement areas with existing or planned infrastructure, servicing capacity, and transportation options. The proposed development enhances the complete communities encouraged by provincial policy by accommodating a mix of housing types and range of uses while providing housing in an area accessible to individuals of varying abilities and ages to multiple amenities.

A detailed review of the applicable policies is attached in Exhibit D (Consistency with the Provincial Planning Statement).

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**Official Plan Considerations**

The subject properties are designated Main Street Commercial and are subject to Site Specific Policy Area Number 73 in the Official Plan (Exhibit E – Official Plan, Land Use). Additionally, the lands are located within the Princess Street Corridor Specific Policy Area - Williamsville Main Street which identifies the site as being in an area of required ground floor commercial along the Princess Street frontage.

The goal of properties designated Main Street Commercial is to provide retail and other commercial services for surrounding neighbourhoods in a setting that is street oriented and pedestrian focused, including a mix of compatible residential and small-scale office or community uses and services. The Site-Specific Policy Area Number 73 provides that a height map will be introduced through the Zoning By-law to define the form of buildings, that the provisions of Section 10E.1.28 (Building Heights) and 10E.1.32 (Tall Buildings) do not apply, and the maximum height specified in Schedule PS-1 do not apply.

The current proposal amends the existing height maps found in Exception Overlay Numbers E83 and E84 in Section 21 of the Zoning By-Law. The site is subject to all other applicable sections of the Official Plan. As a result of the new height maps, the proposed development would continue to achieve stepbacks at various elevations helping to contribute to an animated and welcoming pedestrian environment. While the height maps would allow for a modest increase in height of approximately 8 metres, the shadow study has confirmed that the increase in height would not have any substantive difference from the development previously contemplated.

The height map also proposes to implement a greater setback from the Princess Street frontage than otherwise had been provided which, combined with the newly proposed pedestrian bridge, helps achieve the public realm design policies of the Official Plan for this area where pedestrian movement and the function and aesthetic quality of the street are priorities in the corridor, in order to transform the street into an attractive and vibrant destination.

While the proposal does result in a decrease in the number of units planned for the site, the reduction is modest, and the overall density is in line with that which has been planned for this section of Princess Street and continues to make efficient use of existing capital infrastructure.

A detailed review of the applicable policies is attached in Exhibit F (Conformity with the Official Plan).

**Zoning By-Law Discussion**

Both sites are zoned Williamsville Zone 1 (WM1) in the Kingston Zoning By-Law, with 525 Princess Street subject to Exception Overlay Number E83, and 555 Princess Street subject to Exception Overlay Number E84.

The WM1 Zone permits both residential and commercial use. No amendments to permitted use are proposed. Each building will contain a mix of residential and commercial uses.

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The proposed amendments relate to the provisions of the existing Exception Overlay Numbers E83 and E84. The recommended Zoning By-Law amendment is provided as Exhibit A (Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62) to this report and discussed below.

Table 1 – Proposed Amendments to Exception Overlay Number E83 (525 Princess Street)

Current Exception Overlay Number E83	Proposed Exception Overlay Number E83	Comment
(a) Subsection 10.2 of this By-law does not apply.	(a) The zone provisions described in Clause 10.2.2. of this By-law does not apply.	The existing clause exempts the property from Subsection 10.2. (Williamsville Zone 1(WM1)) in its entirety. The proposed amendment seeks to narrow the exemption to Clause 10.2.2. which otherwise requires the property to be developed in accordance with Table 10.2.1. (WM1 Provisions). This amendment is necessary to avoid conflict with the height maps proposed.
(b) The maximum building heights in Schedule 3 to this By-law do not apply.		This clause is no longer necessary. Schedule 3 (Required Ground Floor Commercial) requires that properties within the WM1 Zone provide ground floor commercial uses but does not provide maximum building heights.
(c) No part of the building will exceed the height limits, in metres, shown on Figure E83, with a maximum 5% variance on noted dimensions permitted.	(b) The maximum building heights are specified on Figure E83, with a maximum 0.5 metres variance on noted dimensions permitted.	The proposed amendment continues to establish maximum heights through the related height map. Instead of permitting a 5% variance to heights shown on the height maps, a set 0.5 metre maximum permitted variance is proposed. This approach provides greater certainty on

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		permitted heights and is consistent with recent practices.
(d) The minimum streetwall height for all buildings is 10.5 metres.	(d) The minimum streetwall height for all buildings is 18.7 metres.	The minimum permitted streetwall height is proposed to increase by 8.2 metres. The additional streetwall height is requested to provide taller ceilings on the ground floor and 4 <sup>th</sup> storey. The increased ceiling heights provide flexibility in use for commercial tenants and amenity area programming.
(e) The maximum streetwall height for all buildings is 16.25 metres.	-	A clause identifying the maximum streetwall height is not necessary. The height map identifies the maximum streetwall height permitted.
(f) Despite Clause 4.18.2, mechanical penthouses may exceed the maximum allowable building height by up to 5 metres.	-	The rooftop mechanical penthouse and amenity areas are proposed at 5.4 metres above the 10 <sup>th</sup> storey of the building. These portions of the building are shown on the height map, therefore, a clause identifying their height is not required. The height map also establishes the setback of enclosed mechanical penthouse and amenity area from the floor below and from street lines.
(g) The minimum ground floor height is 4.2 metres.	(e) The minimum floor to floor height of the first storey is 4.5 metres. Residential dwellings with ground floor access directly from Chatham Street and/or Creighton Street are	The minimum ground floor height is proposed to increase by 0.3 metres. The actual height of the first floor is currently proposed at 6.6 metres to provide increased ceiling heights, which can be an attractive feature for

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	not subject to this requirement.	commercial tenants and residential lobbies.
<p>(h) Buildings fronting on Princess Street are required to have ground floor commercial uses on the first storey where any portion of the lot aligns with the area identified as "Required Ground Floor Commercial Along Princess Street Frontage" on Schedule 3 of this By-law.</p> <p>(i) Where ground floor commercial uses are required by Paragraph (h), the entire street frontage of the first storey, excluding areas devoted to a lobby or other shared entrances/exits for other permitted uses, must be occupied by commercial uses. Portions of the floor area of the first storey that do not have frontage on a public street may be occupied by uses that service the building such as loading spaces, waste management facilities and rooms, mechanical rooms, bicycle spaces, and other similar uses.</p>	-	<p>These clauses are no longer necessary. The related amendment to Clause (a) of the Exception Overlay has the effect of applying Clause 10.2.3. of the Zoning By-Law, which includes subclauses 10.2.3.4. and 10.2.3.5. (Ground Floor Commercial Uses) which have the same effect as existing Clauses (h) and (i) of Exception Overlay Number E83.</p>
<p>(j) The minimum front yard is 1.0 metre.</p>	<p>(c) The building setbacks are specified in Figure E83, with a maximum 5% variance on noted dimensions permitted, except any setback from Princess Street where no variance is permitted.</p> <p>(f) The minimum setback for a pedestrian bridge to a lot line</p>	<p>The proposed clause uses the height map to establish setbacks. The proposed setback to Princess Street is 2 metres and the proposed setback from Alfred Street is generally 1 metre, except for the proposed bridge. The proposed clause also includes a maximum 5%</p>

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	along Alfred Street is 0.0 metres, and the minimum unobstructed clearance below the pedestrian bridge is 11.0 metres.	variance to the setbacks shown on the height map to account for potential minor changes to building design prior to building permit issuance.
(k) A minimum of 75% of a building's frontage must be built to the front property line, or applicable setback line, for the height of the streetwall.	-	This clause is no longer necessary. The related amendment to the existing Clause (a) of the Exception Overlay has the effect of applying Clause 10.2.3. of the Zoning By-Law which includes subclause 10.2.3. which establishes the same streetwall requirement.
(l) The minimum setback from any lot line that abuts an Urban Residential Zone is 1.165 metres.	-	This clause is no longer required as the site does not abut any properties in an Urban Residential Zone. Adjacent Zones include the WM1 (Williamsville Zone 1) and the URM7 (Urban Multi-Residential Zone 7).
(m) Balconies are prohibited below the fourth storey of buildings fronting onto Princess Street.	-	Balconies are not proposed at any storey of the proposed building.
(n) Parking spaces and loading spaces are prohibited in a yard abutting a street line.	-	The site design does not enable parking or loading spaces to be located in a yard abutting a street line, therefore, this clause is not necessary.
(o) The minimum off-street parking space ratio for residential use is 0.5 parking spaces per dwelling unit.	(g) The minimum off-street parking requirement for all parking types, excluding accessible spaces, is 0.2	The proposed amendment updates this clause to reflect a previous minor variance (City File D13-064-2023) which reduced the residential parking rate for the site to 0.2

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	parking spaces per dwelling unit.	parking spaces per unit. The amendment also seeks to acknowledge visitor and car-share spaces under this ratio, to reflect the intended site design and available space.
(p) The minimum off-street parking space ratio for commercial use is 1 parking space per 150 square metres of gross floor area dedicated to commercial uses.	-	This clause is no longer necessary. The Zoning By-Law no longer requires off-street parking for commercial use, except for accessible parking spaces.
(q) A minimum of 8 accessible spaces must be provided.	(h) A minimum of 6 accessible spaces are required.	The proposed amendments include a request to reduce the minimum number of accessible parking spaces to 6 for the site. Based on the number of residential units and amount of commercial area proposed, the site would otherwise require 9 accessible spaces if calculated using the effective ratio method of the Zoning By-Law. As discussed within this report below, the reduction is appropriate to balance the number of accessible and standard parking spaces on the site.
(r) A minimum of 1 loading space must be provided.	(i) A minimum of 1 loading space is required.	Carrying forward the existing permission to provide 1 loading space for the site is appropriate. The Zoning By-Law would require 2 loading spaces under Table 7.5.1., however, given the size and layout of the site, 1 loading space to serve both residential and commercial spaces is considered appropriate. A short-term

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		<p>delivery space and staging area is also provided on-site, near the loading space provided.</p>
<p>(s) The amenity area provisions of Subsection 4.3 of this By-Law apply.</p>	<p>(j) The dimension requirements for amenity areas described in Clause 4.3.3. of this by-law does not apply.</p>	<p>The proposed amendment seeks to narrow previous permissions by removing exemption from Subsection 4.3. in its entirety and exempting the site from only Clause 4.3.3. of the Zoning By-Law. This amendment is required to permit a portion of amenity space that has a length more than four times its width. This amendment is primarily required to permit an elongated portion of outdoor rooftop amenity space which is similar to existing design and permissions and can be seen on Plan A2.12 of Exhibit H (Architectural Drawing Packages).</p>
<p>(t) The accessory buildings provisions of Clauses 4.1.1 and 4.1.6 apply.</p>	<p>-</p>	<p>This clause is no longer necessary. The sites will be subject to provisions related to accessory buildings which apply to the WM1 Zone. Current plans do not include accessory buildings; however, supply sheds or other small accessory structures may be desired in the future to support site function.</p>
	<p>(k) Vents, flues and exhausts are permitted to project into any required setback a maximum distance of 0.5 metres, provided such feature</p>	<p>The amendments include a request to permit vents, flues and exhausts to have a 0.5 metre setback from any lot line. This request expands on existing permissions for</p>

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	<p>is setback a minimum of 0.5 metres to any lot line.</p>	<p>projections under Section 4.19. (Projections into Requires Setbacks) of the Zoning By-Law which allows similar features such as eaves, gutters, parapets and pilasters to be as close as 0.5 metres from a lot line.</p>
	<p>(l) Canopies are permitted to project into any required setback a maximum distance of 2.0 metres.</p>	<p>The proposed amendments include a request to permit canopies to project into required setbacks by 2 metres. Canopies are proposed along the Princess Street frontage where the effective setback of the canopies would be 0 metres from the lot line.</p>
	<p>(m) Horizontal bike spaces must have dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.9 metres.</p> <p>(n) Vertical bike spaces must have dimensions of 0.45 metre wide by 1.8 metres vertical length, with a minimum horizontal clearance from the wall of 1.2 metres.</p> <p>(o) Stacked bike spaces must have minimum dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.2 metres.</p>	<p>The proposed amendments include requests to reduce the width of bike parking spaces for horizontal, vertical and stacked spaces to accommodate specific bike racks.</p>
	<p>(p) Figure E8</p>	<p>The proposed height map is included in Exhibit L (Proposed Height Maps).</p>

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Table 2 – Proposed Amendments to Exception Overlay Number E84

<b>Current Exception Overlay Number E84</b>	<b>Proposed Exception Overlay Number E84</b>	<b>Comment</b>
(a) Subsection 10.2 of this By-law does not apply.	(a) The zone provisions described in Clause Subsection 10.2.2. of this By-law does not apply.	The existing clause exempts the property from Subsection 10.2. (Williamsville Zone 1(WM1)) in its entirety. The proposed amendment seeks to narrow the exemption to Clause 10.2.2. which otherwise requires the property to be developed in accordance with Table 10.2.1. (WM1 Provisions).
(b) The maximum building heights in Schedule 3 of this By-law do not apply.	-	This clause is no longer necessary. Schedule 3 (Required Ground Floor Commercial) requires that properties within the WM1 Zone provide ground floor commercial uses but does not provide maximum building heights.
(c) No part of the building will exceed the height limits, in metres, shown on Figure E83, with a maximum 5% variance on noted dimensions permitted.	(b) The maximum building heights are specified on Figure E84, with a maximum 0.5 metre variance on noted dimensions permitted.	The proposed amendment continues to establish maximum heights through the related height map. Instead of permitting a 5% variance to heights shown on the height maps, a set 0.5 metre maximum permitted variance is proposed. This approach provides greater certainty on permitted heights.
(d) The minimum streetwall height for all buildings is 10.5 metres.	(d) The minimum streetwall height for all buildings is 10.1 metres.	The minimum streetwall height is proposed to decrease by 0.4 metres to improve transition to existing development adjacent the site. The majority of the

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		streetwall is proposed at 19.7 metres, as shown on Plan A.01 of the revised drawing package of Exhibit H. The streetwall height decreases near the adjacent site at 561 Princess Street.
(e) The maximum streetwall height for all buildings is 16.25 metres.	-	A clause identifying the maximum streetwall height is not necessary. The height map identifies the maximum streetwall height permitted.
(f) Despite Clause 4.18.2, mechanical penthouses may exceed the maximum allowable building height by up to 5 metres.	-	The rooftop mechanical penthouse and amenity areas are proposed at 5.4 metres above the 10 <sup>th</sup> storey of the building. These portions of the building are shown on the height map, therefore, a clause identifying their height is not required. The height map also establishes the setback of enclosed mechanical penthouse and amenity area from the floor below and from street lines.
(g) The minimum ground floor height is 4.2 metres.	(e) The minimum floor to floor height of first storey is 4.2 metres.	The minimum ground floor height requirement is being maintained at 4.2 metres. The actual height of the first floor is currently proposed at 6.45 metres to provide increased ceiling heights, which can be an attractive feature for commercial tenants and residential lobbies.
(h) Buildings fronting on Princess Street are required to have ground floor commercial uses on the first storey where any portion of	-	These clauses are no longer necessary. The related amendment to Clause (a) of the Exception Overlay has the effect of applying Clause

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<p>the lot aligns with the area identified as "Required Ground Floor Commercial Along Princess Street Frontage" on Schedule 3 to this By-Law.</p> <p>(i) Where ground floor commercial uses are required, the entire street frontage of the first storey, excluding areas devoted to a lobby or other shared entrances/exits for other permitted uses, must be occupied by commercial uses. Portions of the floor area of the first storey that do not have frontage on a public street may be occupied by uses that service the building such as loading spaces, waste management facilities and rooms, mechanical rooms, bicycle spaces and other similar uses.</p>		<p>10.2.3. of the Zoning By-Law, which includes subclauses 10.2.3.4. and 10.2.3.5. which have the same effect as existing Clauses (h) and (i) of Exception Overlay Number E83.</p>
<p>(j) The minimum front yard is 1.0 metre.</p>	<p>(c) The building setbacks are specified in Figure E84, with a maximum 5% variance on noted dimensions permitted, except any setback from Princess Street where no variance is permitted.</p> <p>(f) The minimum setback for a pedestrian bridge to a lot line along Alfred Street is 0.0 metres, and the minimum unobstructed clearance below the pedestrian bridge is 11.0 metres.</p>	<p>The proposed clause uses the height map to establish setbacks. The proposed setback to Princess Street is 2 metres and the proposed setback from Alfred Street is generally 1 metre, except for the proposed bridge. The proposed clause also includes a maximum 5% variance to the setbacks shown on the height map to account for potential minor changes to building design prior to building permit issuance.</p>

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<p>(k) A minimum of 75% of a building frontage must be built to the front property line, or applicable setback line, for the height of the streetwall.</p>	<p>-</p>	<p>This clause is no longer necessary. The related amendment to the existing Clause (a) of the Exception Overlay has the effect of applying Clause 10.2.3. of the Zoning By-Law which includes subclause 10.2.3. which establishes the same streetwall requirement.</p>
<p>(l) The minimum setback from any lot line that abuts an Urban Residential Zone is 1.165 metres.</p>	<p>-</p>	<p>This clause is no longer required as the site does not abut any properties in an Urban Residential Zone. Adjacent Zones include the WM1 (Williamsville Zone 1) and the URM7 (Urban Multi-Residential Zone 7).</p>
<p>(m) Balconies are prohibited below the fourth storey of buildings fronting onto Princess Street.</p>	<p>-</p>	<p>Balconies are not proposed at any storey of the proposed building.</p>
<p>(n) Parking spaces and loading spaces are prohibited in a yard abutting a street line.</p>	<p>-</p>	<p>The site design does not enable parking or loading spaces to be located in a yard abutting a street line, therefore, this clause is not necessary.</p>
<p>(o) The minimum parking space ratio for residential uses is 0.5 parking spaces per dwelling unit.</p>	<p>(g) The minimum off-street parking requirement for all parking types, excluding accessible spaces, is 0.2 parking spaces per dwelling unit.</p>	<p>The proposed amendment updates this clause to reflect a previous minor variance (City File D13-064-2023) which reduced the residential parking rate for the site to 0.2 parking spaces per unit. The amendment also seeks to acknowledge visitor and car-share spaces under this ratio, to reflect the intended site design and available space.</p>

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<p>(p) The minimum parking space ratio for commercial uses is 1 parking space per 150 square metres of gross floor area dedicated to commercial uses.</p>	<p>-</p>	<p>This clause is no longer necessary. The Zoning By-Law no longer requires off-street parking for commercial use, except for accessible parking spaces.</p>
<p>(q) A minimum of 8 accessible spaces must be provided.</p>	<p>(h) A minimum of 2 accessible spaces is required.</p>	<p>The proposed amendments include a request to reduce the minimum number of accessible parking spaces to 2 for the site. Based on the number of residential units and amount of commercial space proposed, the Zoning By-Law would require 7 accessible spaces if calculated using the effective ratio method of the Zoning By-Law. As discussed within this report below, the reduction is appropriate to balance the number of accessible and standard parking spaces on the site.</p>
<p>(r) A minimum of 1 loading space must be provided.</p>	<p>(i) A minimum of 1 loading space is required.</p>	<p>Carrying forward the existing permission to provide 1 loading space for the site is appropriate. The Zoning By-Law would require 2 loading spaces under Table 7.5.1., however, given the size and layout of the site, 1 loading space to serve both residential and commercial spaces is considered appropriate. A short-term delivery space and staging area is also provided on-site, near the loading space.</p>

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<p>(s) The amenity area provisions of Subsection 4.3 of this By-law apply.</p>	<p>(j) The dimension requirements for amenity areas described in Clause 4.3.3. of this by-law does not apply.</p>	<p>The proposed amendment seeks to narrow previous permissions by removing exemption from Subsection 4.3. in its entirety and exempting the site from only Clause 4.3.3. of the Zoning By-Law. This amendment is required to permit a portion of amenity space that has a length more than four times its width. This amendment is primarily required to permit an elongated portion of outdoor rooftop amenity space which is similar to existing design and permissions and can be seen on Plan A2.05 of Exhibit H (Architectural Drawing Packages).</p>
<p>(t) The accessory building provisions of Clauses 4.1.1 and 4.1.6 apply.</p>	<p>-</p>	<p>This clause is no longer necessary. The sites will be subject to provisions related to accessory buildings which apply to the WM1 Zone. Current plans do not include accessory buildings; however, supply sheds or other small accessory structures may be desired in the future to support site function.</p>
	<p>(k) Vents, flues and exhausts are permitted to project into any required setback a maximum distance of 0.5 metres, provided such feature is setback a minimum of 0.5 metres to any lot line.</p>	<p>A request to permit vents, flues and exhausts is included in the proposed amendments to allow these features to have a 0.5 metre setback from any lot line. This request expands on existing permissions for projections under Section 4.19. (Projections into Requires Setbacks) of the Zoning By-</p>

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		<p>Law which allows similar features such as eaves, gutters, parapets and pilasters to be as close as 0.5 metres from a lot line.</p>
	<p>(l) Canopies are permitted to project into any required setback a maximum distance of 2.0 metres.</p>	<p>The proposed amendments include a request to permit canopies to project into required setbacks by 2 metres. Canopies are proposed along the Princess Street frontage where the effective setback of the canopies would be 0 metres from the lot line.</p>
	<p>(m) Horizontal bike spaces must have dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.9 metres.</p> <p>(n) Horizontal bike spaces must have dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.9 metres.</p> <p>(o) Stacked bike spaces must have minimum dimensions of 0.45 metre wide by 1.8 metre horizontal length, with a minimum vertical clearance of 1.2 metre.</p>	<p>The proposed amendments include requests to reduce the width of bike parking spaces for horizontal, vertical and stacked spaces to accommodate specific bike racks.</p>
	<p>(p) A maximum of 18% of parking spaces provided on a lot, excluding accessible spaces, visitor spaces and car-share spaces, are</p>	<p>The proposed amendments include a request to permit up to 18% of standard parking spaces at small car spaces. Subclause 7.4.9.4. permits a maximum of 10% of standard spaces to be small car</p>

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	permitted to be parking spaces for small cars.	spaces. The requested increase to 18% would facilitate 6 small car spaces, shown on Plan A2.01 of architectural plans for the site.
	(q) Figure E84	The proposed height map is included in Exhibit L (Proposed Height Maps).

Built Form and Massing

The current Exception Overlays use height maps to establish permitted built form and massing by identifying the maximum permitted heights of building portions on each site. The heights maps have the effect of establishing permitted heights and setbacks on each site. The amendments propose to replace the existing height maps with new maps that reflect the proposed built forms, illustrated in Exhibit H (Architectural Drawing Package). The proposed changes in built form are supported by a shadow analysis, wind study, and urban design review to ensure the changes will not have negative impacts on the subject or adjacent properties. Shadow analysis is provided in Section 5.5 of the Planning Justification Report, prepared by Fotenn Planning + Design. The proposed changes in the built form generate minimal increases in shadowing which are not anticipated to create negative impacts on adjacent properties. A key feature of the revised built form is the enhanced setback from the properties' Princess Street frontages from a previous low of 1.0 metre to a new setback of between 2.0 metres and 3.0 metres which facilitates a greater degree of pedestrian interaction at the street level.

A Pedestrian Level Wind Study was prepared by Gradient Wind Engineers & Scientists to support the proposal and was peer reviewed by Cermak Peterka Peterson Wind Engineering Consultants (CPP). The peer review by CPP broadly agreed with the findings that the resultant wind conditions over the nearby public sidewalks, transit stops, neighbouring existing surface parking lots, proposed outdoor amenities, and in the vicinity of the building access points serving the subject site, are acceptable subject to implementation of the recommended mitigation measures. Final mitigation measures will be determined through the site plan control process and may include wind screens, dense arrangements of plantings in tall planters, high back bench seating and other landscape elements.

The proposed amendments include a new height map and provisions which allow a 0.5 metre variance in heights and a 5% variance on setbacks from those dimensions noted on the height map. These permitted variances allow for slight changes in building design at building permit or in the future, should the design change. The extent of these variances are considered reasonable and are not anticipated to create negative impacts to the site or surrounding properties.

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### Standard Parking

The proposed amendments seek to update the Exception Overlay to capture a previous minor variance (City File Number D13-064-2023) which reduced the standard parking ratio for each site to 0.2 spaces per dwelling unit. The current amendment also seeks to include car-share parking spaces and visitor spaces within this ratio.

### Accessible Parking

The proposed amendments include a request to provide accessible parking spaces based on need, area available and the number of standard parking spaces provided.

The Zoning By-Law establishes two methods of calculating accessible parking requirements, requiring that the method resulting in the greater number of spaces be used. The number of accessible parking spaces required by Clause 7.2.3. (Number of Required Accessible Spaces) is based on either the number of parking spaces provided on the site or the effective ratios provided in Table 7.2.2. (Effective Ratios to Calculate Required Accessible Spaces), whichever is greater.

Using the first calculation method, the site at 525 Princess Street would be required to provide 3 accessible spaces which represents 4% of the total 69 parking spaces provided. Using the second method, the site would be required to provide 9 accessible spaces which represents 13% of the total spaces provided. The proposal of 6 accessible spaces at 525 Princess Street represents 8.7% of the total spaces provided which exceeds the minimum requirement of the first calculation method, exceeds the minimum requirement of the *Accessibility for Ontarians with Disabilities Act* (AODA) and provides accessible parking at an amount that considers the overall parking supply being provided on the site.

Using the first calculation method, the site at 555 Princess Street would require 1.4 accessible spaces (rounded up to 2 due to rounding language in the zoning by-law) which represents 4% of the total 35 parking spaces provided. Using the second method, the site would be required to provide 7 accessible spaces which represents 20% of the total spaces provided. The proposal of 2 accessible spaces at 555 Princess Street represents 5.7% of the total spaces provided which exceeds the minimum requirement of the first calculation method, exceeds the minimum requirement of the *Accessibility for Ontarians with Disabilities Act* (AODA) and provides accessible parking at an amount that considers the overall parking supply being provided on the site.

The proposal to reduce accessible parking requirements would continue to meet minimum requirements under the *Accessibility for Ontarians with Disabilities Act* (AODA), exceeds the number of spaces required by 1 of the 2 calculation methods provided in the Zoning By-Law and makes best use of the space available on the site. This change confirms with the City's Official Plan that outlines accessible parking is to be provided in accordance with the AODA.

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## Bike Parking

The proposed amendments include reduction in bike parking space width to support the use of specific bike storage equipment. Proposed bike rack details and dimensions are shown on updated Architectural drawings A2.01 and A2.02 for each site. Horizontal, vertical and stacked bike spaces are proposed at a width of 0.45 metres compared to the standard Zoning By-Law requirement of 0.6 metres in width. The required lengths, vertical clearances and horizontal clearances will be maintained in compliance with the Zoning By-Law standards.

## Permitted Projections

An amendment is proposed to permit projections such as vents, flues and exhausts into required setbacks provided these features are a minimum of 0.5 metres to any lot line. This clause builds on the existing permitted projections provided in Section 4.19. of the Zoning By-Law. Section 4.19. considers projections such as sills, chimneys, eaves, gutters and ornamental features but does not specify projections such as vents, flues and exhausts. These features are similar in nature to the existing permitted projections of Section 4.19. of the By-Law and are an appropriate addition which enables building construction without the need for additional setbacks and reduced floor areas to accommodate these features.

Permission for canopies to project into required setbacks by 2 metres is also proposed. Unlike the request projection for vents, flues and exhausts the request to permit canopies into required setbacks does not include a 0.5 metre minimum setback. Given the buildings are proposed to have a 2-metre setback from Princess Street, this provision would allow canopies to project to the lot line. Canopies are planned above building entrances to provide weather projection while contributing to high-quality design of the proposed buildings.

## Amenity Areas

The proposal includes a request to exempt each site from the requirements of Subsection 4.3.3. of the Zoning By-Law, which would otherwise require that amenity areas be provided so that the length does not exceed four times the width of the space. The existing Exception Overlays currently exempt each site from Section 4.3. of the Zoning By-Law in its entirety, therefore, the requested amendment narrows relief associated with amenity areas to focus solely on Subsection 4.3.3.

The site at 525 Princess Street proposes a total amenity area of 3,160 square metres and the site at 555 Princess Street proposes a total amenity area of 1,720 square metres. Both sites propose a mix of indoor and outdoor amenity areas. No reduction in the amount of required amenity space is proposed. Relief from Section 4.3.3. is requested to enable outdoor amenity space on the rooftop which includes a 2.5-metre-wide terrace that wraps around the enclosed rooftop amenity area and mechanical enclosure and connects to the larger exterior rooftop amenity area. Configuration of the amenity space in this way allows for additional unenclosed rooftop amenity space and is necessary to provide the required amount of amenity space given the site constraints.

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Provisions to be Removed

The proposed amendments described in Table 1 and Table 2 above include removal of clauses which are no longer required based on current Zoning By-Law requirements and changes to the proposed building design.

**Encroachment By-Law and Agreement**

The proposed buildings are intended to be connected by an elevated pedestrian bridge over Alfred Street. The subject Zoning By-Law amendment considers setbacks to lot lines necessary to facilitate the bridge, however, given that the bridge is proposed to traverse across City property, additional permissions will be required prior to the construction of this feature.

Encroachment By-Law Number 2004-107 (A By-Law to Protect the City's Highways Form Unauthorized Encroachment) considers a variety of permissible encroachment types but does not contemplate an elevated pedestrian bridge as proposed. As a type of structure not currently contemplated by the Encroachment By-Law, staff have included a recommendation that Council authorize the Manager of Development Engineering to enter into an agreement with the owner that would authorize the encroachment over City lands. The encroachment agreement would be subject to such conditions deemed necessary including requirements for maintenance, insurance, inspections, and the right-of-entry by the City.

While staff are supportive of the pedestrian bridge at this location in principle, it should be noted that the recommendation to enable the applicant to apply for an encroachment agreement does not guarantee approval of such agreement. Initial feedback through technical review by various City departments is generally supportive, however, the applicant will be required to supply required information for review and enter into an agreement to ensure the long-term maintenance of the bridge.

**Other Applications**

The subject properties are associated with an active application for Site Plan Control applications under City File Numbers D11-046-2022 (525 Princess Street) and D11-047-2022 (555 Princess Street). The related Site Plan Control applications contain plans which reflect the zoning approved through the previous Official Plan and Zoning By-Law Amendment File Number D35-001-2019. The applications are currently on hold pending updated plans following the current Zoning By-Law Amendment.

Future Holding Overlay removal applications will be required for each site prior to development. The site at 525 Princess Street is subject to Holding Overlay Number H8 and the site at 555 Princess Street is subject to Hold Overlay Number H41. Both Holding Overlays require confirmation of required services and a community benefits contribution prior to removal.

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## Technical Analysis

This application has been circulated to external agencies and internal departments for review and comment. All comments on the proposal have been addressed and no outstanding issues with this application remain at this time.

## Public Comments

The following is a summary of the public input received to date, including a summary of the feedback received at the Community Meeting on November 20, 2025. All original written public comments are available in Exhibit O of this report.

- Question: How will the development address the issue of housing affordability?

Response: The applicant has indicated an intention to provide five percent of the units as 'rent geared to income'. Of note, the City's [10-Year Municipal Housing & Homelessness Plan](#) recognizes that housing affordability exists along a continuum and sets out as one of its Strategic Directions to increase the stock of rental housing, including the number of private market rental units, which the plan notes "will serve in the longer term to stabilize rental prices and increase the vacancy rate."

- Question: What is the size and mix of units?

Response: Overall, the development proposes 13 three-bedroom townhouse units together with 488 apartment units which consist of 15% three-bedroom units, 20% two-bedroom units, 16% one-bedroom units, and 50% studio units. The sizes of units vary considerably but range from 310 square foot units for many studio units to 750 square foot for 2 two-bedroom units and approximately 900 square foot for three-bedroom units while the townhouse units are mostly in the 1,100 square foot range.

- Question: Would there be any natural sunlight to the four-storey townhouse.

Response: The shadow study provided by the application demonstrates that the townhouse units would benefit from exposure to natural sunlight both during both the spring and autumn equinoxes, when sun exposes the northern and eastern exterior walls until 10AM, with the eastern wall benefiting from sunlight until 2PM during both of these periods.

- Question: What is the public benefit of the connecting bridge?

Response: The pedestrian bridge serves both a private and public benefit by providing connectivity between the two buildings helping allow ease of access to share the differently programmed amenity space among all of the residents of the two buildings. By providing the connective feature between the two buildings the demand there is a reduced demand for street-level pedestrian crossings which would otherwise be required of residents traversing between the two buildings. The developer would be responsible

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for all costs associated with the construction and maintenance of the feature and an Agreement with the City would be required to address issues of ongoing maintenance, liability, and other items.

While aesthetic preference will vary widely among individuals, the provision of a signature feature such as this would help define the prominence of this location as one of a select few locations within the Williamsville neighbourhood which is intended for high-rise development. The bridge would facilitate placemaking and would be consistent with Official Plan policies which promote innovation in building design to create an interesting and varied built environment.

### **Effect of Public Input on Draft By-Law**

The questions and comments made throughout the public commenting period have been addressed throughout the body of this report and have not led to substantial changes to the proposed amending by-law.

### **Conclusion**

The recommended Zoning By-Law amendment facilitates revisions to the built form previously approved for this site and enables the creation of two mixed-use buildings within the Williamsville neighbourhood consistent with the planned development of this area. The development represents an appropriate use of a lot in the Urban Boundary, on full municipal services that is compatible with the evolving and varied character of this length of Princess Street.

The proposal, is consistent with the Provincial Planning Statement, conforms to the Official Plan, and represents good land use planning. The application is recommended for approval.

### **Existing Policy/By-Law:**

The proposed amendment was reviewed against the policies of the Province of Ontario and City of Kingston to ensure that the changes would be consistent with the Province's and the City's vision of development. The following documents were assessed:

#### **Provincial**

*Planning Act*

Provincial Planning Statement, 2024

#### **Municipal**

City of Kingston Official Plan

Zoning By-Law Number 2022-62

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**Notice Provisions:**

Pursuant to the requirements of the *Planning Act*, notice of the statutory public meeting was provided 20 days in advance of the public meeting in the form of a sign posted on the subject properties and by mail to 398 property owners (according to the latest Assessment Rolls) within 120 metres of the subject properties. In addition, a courtesy notice placed in The Kingston Whig-Standard on January 26, 2026.

If the application is approved, a Notice of Passing will be circulated in accordance with the provisions of the *Planning Act*.

At the time of writing of this report, 2 pieces of written public correspondence have been received, and all planning related matters have been addressed within the body of this report. Any public correspondence received after the publishing of this report will be included as an addendum to the Planning Committee agenda.

**Climate Risk Considerations**

Stormwater management plans for each site will be reviewed and secured through the site plan control process to ensure surface run-off is maintained between the pre-development and post-development flows and in accordance with City policies.

While not a consideration of the current Zoning By-Law amendment, the applicant has indicated that efficient heating methods, such as geothermal design, are being considered.

The subject sites are well located to support active transportation options and use of public transit, which contributes to reduction in greenhouse gas emissions associated with large-scale use of personal automobiles.

**Indigenization, Inclusion, Diversity, Equity & Accessibility (IIDEA) Considerations**

The proposed development will be required to comply with applicable accessible requirements of the Ontario Building Code, including provision of accessible units.

**Financial Considerations:**

None

**Contacts:**

James Bar, Manager, Development Approvals, 613-546-4291 extension 3213

Garret Hoegi, Manager, Development Engineering, 613-546-4291 extension 3294

Ian Clendening, Senior Planner, 613-546-4291 extension 3126

Chanti Birdi, Intermediate Planner, 613-546-4291 extension 3273

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**Other City of Kingston Staff Consulted:**

None

**Exhibits Attached:**

- Exhibit A Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62
- Exhibit B Key Map
- Exhibit C Neighbourhood Context
- Exhibit D Consistency with the Provincial Planning Statement
- Exhibit E Official Plan, Land Use
- Exhibit F Conformity with the Official Plan
- Exhibit G Zoning By-Law Number 2022-62
- Exhibit H Architectural Drawing Packages
- Exhibit I Shadow Study
- Exhibit J Topographic Survey
- Exhibit K Pedestrian Level Wind Study
- Exhibit L Proposed Height Maps
- Exhibit M Site Photographs
- Exhibit N Public Notice Notification Map
- Exhibit O Public Comments

**By-Law Number 2026-XX**

**A By-Law to Amend By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (Change to Exception Number ‘E83’ and Exception Number ‘E84’ (525 Princess Street and 555 Princess Street))**

**Passed:** [Meeting Date]

**Whereas** the Council of The Corporation of the City of Kingston enacted By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (the “Kingston Zoning By-Law”);

**Whereas** the Council of The Corporation of the City of Kingston deems it advisable to amend the Kingston Zoning By-Law;

**Therefore be it resolved that** the Council of The Corporation of the City of Kingston hereby enacts as follows:

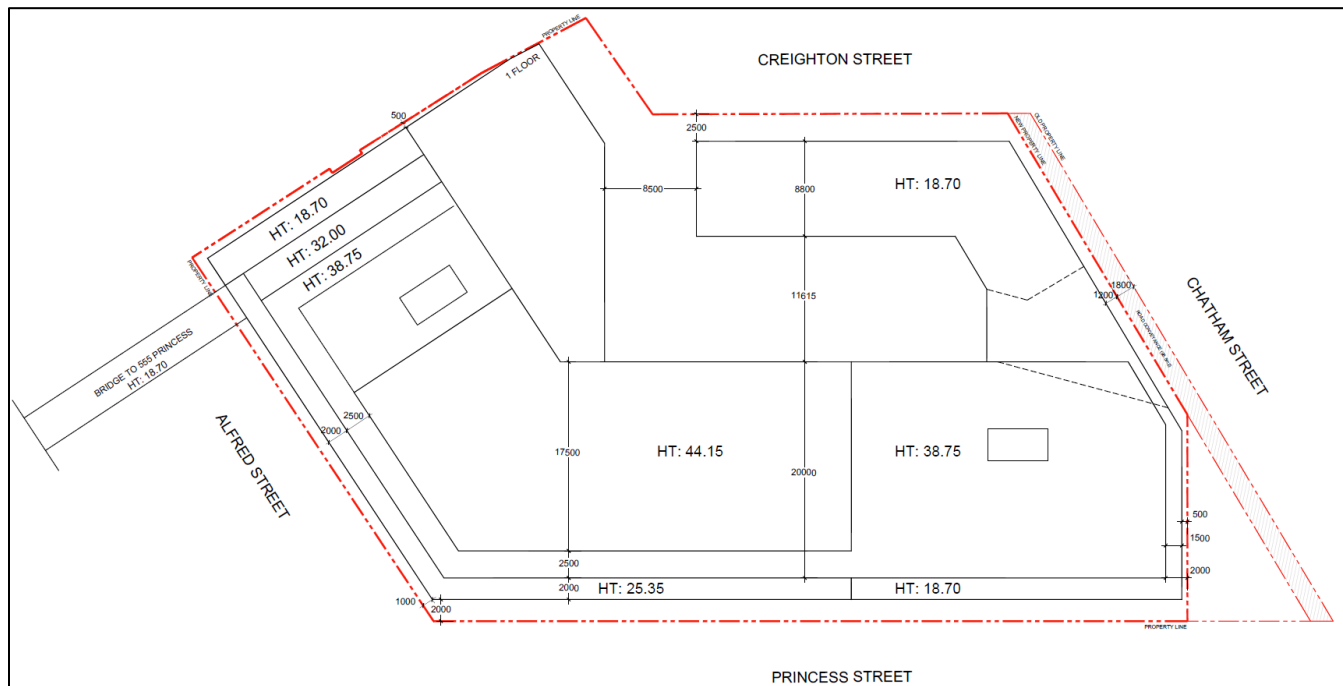
1. By-Law Number 2022-62 of The Corporation of the City of Kingston, entitled “Kingston Zoning By-Law Number 2022-62”, is amended as follows:
  - 1.1. By amending Exception Number E83 in Section 21 – Exceptions, by deleting it in its entirety and replacing it as follows:

**“E83.** Despite anything to the contrary in this By-law, the following provisions apply to the lands subject to this Exception:

    - (a) The zone provisions described in Clause 10.2.2. of this By-law do not apply;
    - (b) The maximum **building heights** are specified on Figure E83. The maximum **building heights** may be increased by the greater of:
      - (i) 0.5 metres; and
      - (ii) the percentage of the maximum **building height** prescribed by the *Planning Act*;
    - (c) The **building setbacks** are specified on Figure E83. The **building setbacks** may be varied by the greater of:
      - (i) 5%; and

- (ii) the percentage of the **setback** distance prescribed by the *Planning Act*;
- (d) The minimum **streetwall height** for all **buildings** is 18.7 metres;
- (e) The minimum floor to floor **height** of the **first storey** is 4.5 metres. Residential **dwelling units** with ground floor access directly from Chatham Street and/or Creighton Street are not subject to this requirement;
- (f) The minimum **setback** for a pedestrian bridge to a **lot line** along Alfred Street is 0.0 metres, and the minimum unobstructed clearance below the pedestrian bridge is 11.0 metres;
- (g) The minimum **parking space** requirement for all parking types, excluding **accessible spaces**, is 0.2 **parking spaces** per **dwelling unit**;
- (h) A minimum of 6 **accessible spaces** is required;
- (i) A minimum of 1 **loading space** is required;
- (j) The dimension requirements for **amenity areas** described in Clause 4.3.3. of this By-law do not apply;
- (k) Vents, flues and exhausts are permitted to project into any required **setback** a maximum distance of 0.5 metres, provided such feature is **setback** a minimum of 0.5 metres to any **lot line**;
- (l) **Canopies** are permitted to project into any required **setback** a maximum distance of 2.0 metres;
- (m) **Horizontal bike spaces** must have dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.9 metres;
- (n) **Vertical bike spaces** must have dimensions of 0.45 metre wide by 1.8 metres vertical length, with a minimum horizontal clearance from the wall of 1.2 metres;
- (o) **Stacked bike spaces** must have minimum dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.2 metres; and

(p) Figure E83



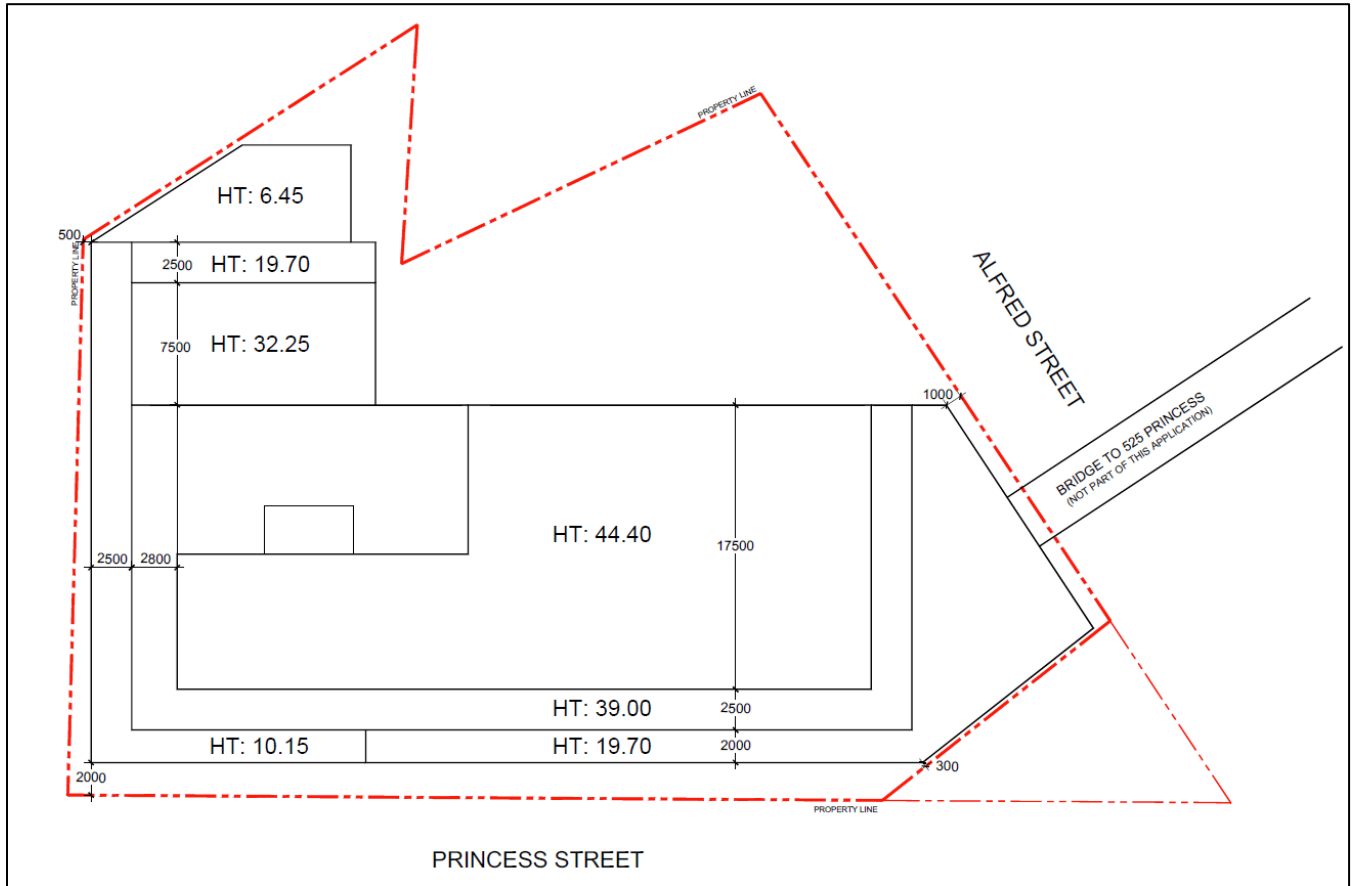
1.2. By amending Exception Number E84 in Section 21 – Exceptions, by deleting it in its entirety and replacing it as follows:

“**E84.** Despite anything to the contrary in this By-law, the following provisions apply to the lands subject to this Exception:

- (a) The zone provisions described in Clause 10.2.2. of this By-law do not apply;
- (b) The maximum **building heights** are specified on Figure E84. The maximum **building heights** may be increased by the greater of:
  - (i) 0.5 metres; and
  - (ii) the percentage of the maximum **building height** prescribed by the *Planning Act*;
- (c) The **building setbacks** are specified on Figure E84. The **building setbacks** may be varied by the greater of:
  - (i) 5%; and

- (ii) the percentage of the **setback** distance prescribed by the *Planning Act*;
- (d) The minimum **streetwall height** for all **buildings** is 10.1 metres;
- (e) The minimum floor to floor **height** of the **first storey** is 4.2 metres;
- (f) The minimum **setback** for a pedestrian bridge to a **lot line** along Alfred Street is 0.0 metres, and the minimum unobstructed clearance below the pedestrian bridge is 11.0 metres;
- (g) The minimum **parking space** requirement for all parking types, excluding **accessible spaces**, is 0.2 **parking spaces** per **dwelling unit**;
- (h) A minimum of 2 **accessible spaces** is required;
- (i) A minimum of 1 **loading space** is required;
- (j) The dimension requirements for **amenity areas** described in Clause 4.3.3. of this by-law do not apply;
- (k) Vents, flues and exhausts are permitted to project into any required **setback** a maximum distance of 0.5 metres, provided such feature is **setback** a minimum of 0.5 metres to any **lot line**;
- (l) **Canopies** are permitted to project into any required **setback** a maximum distance of 2.0 metres;
- (m) **Horizontal bike spaces** must have dimensions of 0.45 metre wide by 1.8 metres horizontal length, with a minimum vertical clearance of 1.9 metres;
- (n) **Vertical bike spaces** must have dimensions of 0.45 metre wide by 1.8 metres vertical length, with a minimum horizontal clearance from the wall of 1.2 metres;
- (o) **Stacked bike spaces** must have minimum dimensions of 0.45 metre wide by 1.8 metre horizontal length, with a minimum vertical clearance of 1.2 metre;
- (p) A maximum of 18% of **parking spaces** provided on a **lot**, excluding **accessible spaces**, **visitor spaces** and **car-share spaces**, are permitted to be **parking spaces** for small cars; and

(q) Figure E84



2. This By-Law shall come into force in accordance with the provisions of the *Planning Act*.

Given all Three Readings and Passed: [Meeting Date]

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**Janet Jaynes**  
City Clerk

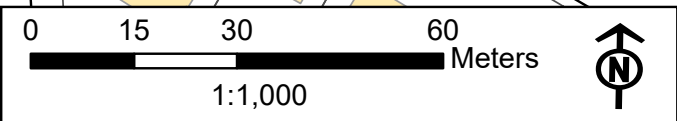
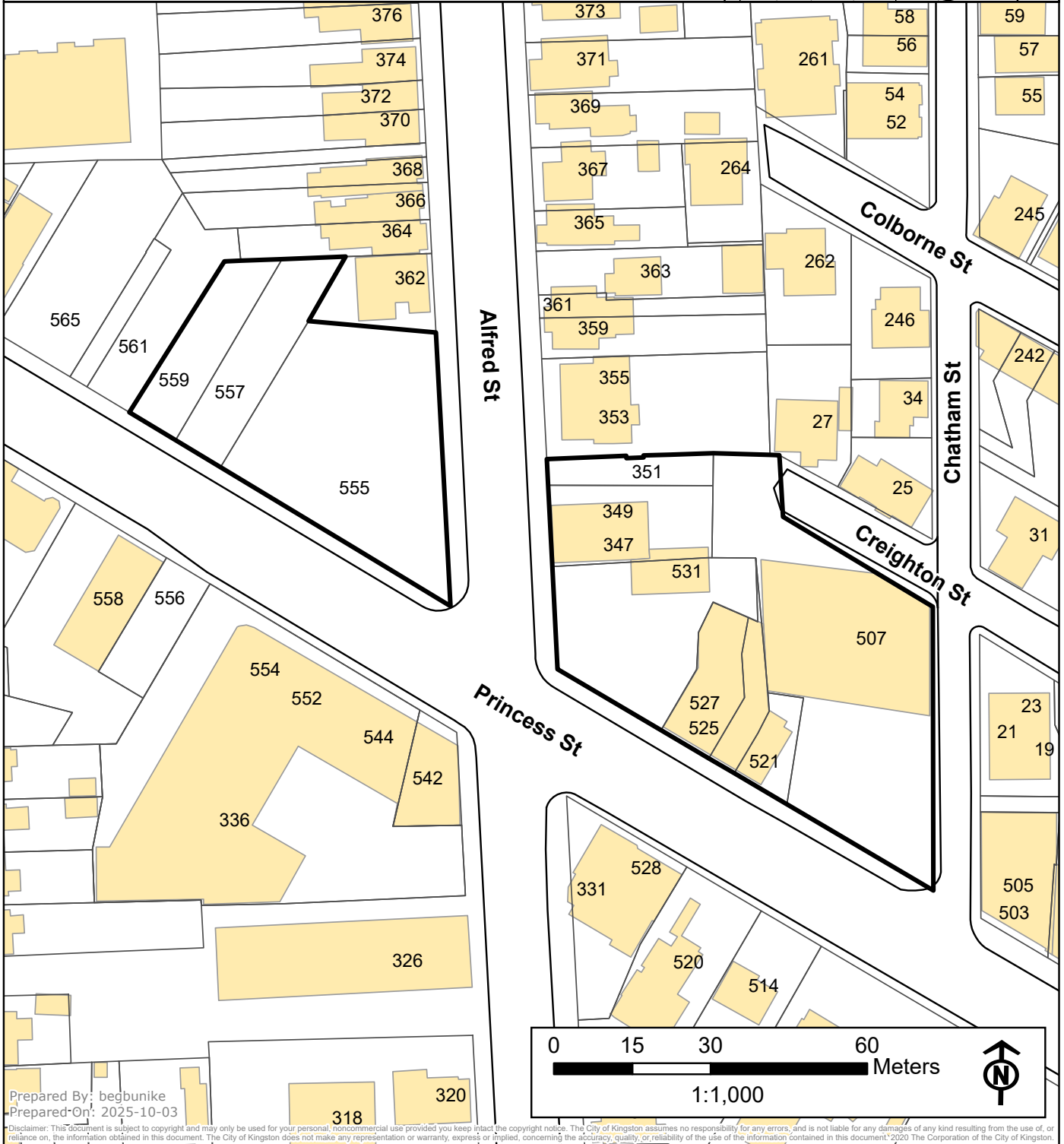
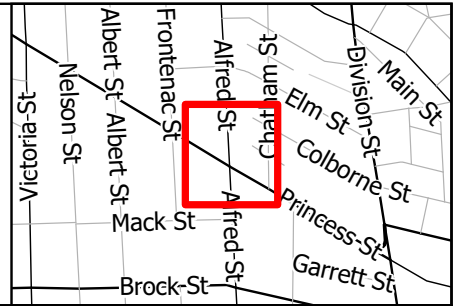
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**Bryan Paterson**  
Mayor



Planning Committee  
**Key Map**  
Address: 525 - 555 Princess Street  
File Number: D14-013-2025

Subject Lands



Prepared By: begbunike  
Prepared On: 2025-10-03

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Planning Committee  
**Neighbourhood Context**  
Address: 525 - 555 Princess Street  
File Number: D14-013-2025

- Subject Lands
- Property Boundaries
- Proposed Parcels



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## Demonstration of How the Proposal is Consistent with the Provincial Planning Statement (2024)

Policy	Commentary
<b>2.1 Planning for People and Homes</b>	
<p>2.1.6. Planning authorities should support the achievement of complete communities by:</p> <p>a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, longterm care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;</p> <p>b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and</p> <p>c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.</p>	<p>The proposed development includes both commercial and residential uses. Both sites have road frontage along Princess Street which is in arterial road with existing bikes lanes and access public transit stops. The sites are located within walking distance of public parks (including Memorial Centre park and Victoria Park), commercial uses (including convenience stores, grocery stores and restaurants) and institutional uses (including Queen’s University).</p> <p>The proposed buildings are designed to contain a mix of studio, 1 bedroom, 2 bedroom and 3 bedroom units. A mix of unit sizes and bedrooms counts contributes to the needs of varying populations.</p>
<b>2.2 Housing</b>	
<p>2.2.1. Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:</p> <p>b) permitting and facilitating:</p> <p style="padding-left: 20px;">2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within</p>	<p>The proposed redevelopment would provide a 488 residential units and 1,260 square metres of commercial space in an area with existing infrastructure including water and sewer services, roads, sidewalks and public transit.</p>

<b>Policy</b>	<b>Commentary</b>
<p>previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;</p> <p>c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and</p> <p>d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.</p>	
<p><b>2.3 Settlement Areas and Settlement Area Boundary Expansions</b></p>	
<p>2.3.1.1. Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.</p>	<p>The subject sites are located within the Urban Boundary as shown on Schedule 2 (City Structure) of the City of Kingston Official Plan. Sites within the urban boundary are the focus of growth and development.</p>
<p>2.3.1.2. Land use patterns within settlement areas should be based on densities and a mix of land uses which:</p> <p>a) efficiently use land and resources;</p> <p>b) optimize existing and planned infrastructure and public service facilities;</p> <p>c) support active transportation;</p> <p>d) are transit-supportive, as appropriate; and</p> <p>e) are freight-supportive.</p>	<p>The proposed buildings seek to introduce 488 residential units and 1,260 square metres of commercial space across both sites combined. The intensification of sites contributes to efficient use of urban land and related services and resources. The sites are located in proximity to existing municipal services, including water and wastewater services.</p> <p>The sites are well located to support active transportation. The sites are located within walking distance of commercial and recreational uses, and bus stops. Short-term and long-term bike parking spaces are provided at each site to encourage active transportation.</p>
<p>2.3.1.3. Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing</p>	<p>The proposed development provides appropriate intensification of each site. Each site is proposed to include a mix of residential unit sizes and types from studio units to 3 bedroom units.</p>



Policy	Commentary
options and prioritizing planning and investment in the necessary infrastructure and public service facilities.	
<b>2.8 Employment</b>	
<p>2.8.1.1. Planning authorities shall promote economic development and competitiveness by:</p> <p>d) encouraging intensification of employment uses and compatible, compact, mixed-use development to support the achievement of complete communities;</p>	Both sites are proposed to contain mixed-use buildings with ground floor commercial use and upper-storey residential use.
<b>2.8 Energy Conservation, Air Quality and Climate Change</b>	
<p>2.9.1. Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that:</p> <p>a) support the achievement of compact, transit-supportive, and complete communities;</p> <p>d) promote green infrastructure, low impact development, and active transportation, protect the environment and improve air quality;</p>	The proposed development provides intensification of the existing sites to provide compact mixed-use buildings. The subject sites are well located to support ridership of existing public transit and enable active transportation options to access nearby commercial, recreational and institutional uses.
<b>3.6 Sewage, Water and Stormwater</b>	
<p>3.6.2. Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.</p>	Each site is proposed to be serviced by municipal water and sewage services.
<p>3.6.8. Planning for stormwater management shall:</p> <p>b) minimize, or, where possible,</p>	Stormwater management reports and plans were received as part of previous Planning applications on each site, including D35-001-

<b>Policy</b>	<b>Commentary</b>
<p>prevent or reduce increases in stormwater volumes and contaminant loads;</p> <p>d) mitigate risks to human health, safety, property and the environment;</p> <p>e) maximize the extent and function of vegetative and pervious surfaces;</p> <p>f) promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and</p> <p>g) align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale.</p>	<p>2019 which established the existing Exception Overlays under the Zoning By-Law. Previous plans demonstrate feasibility of the site to accommodate stormwater and the current proposal provides a similar ground coverage and site design as the designs contemplated through previous stormwater management assessments. Confirmation that the current design can continue to provide adequate stormwater management controls and mitigations will be undertaken as part of the site plan control process.</p>
<p><b>3.9 Public Spaces, Recreation, Parks, Trails and Open Space</b></p>	
<p>3.9.1. Healthy, active, and inclusive communities should be promoted by:</p> <p>a) planning public streets, spaces and facilities to be safe, meet the needs of persons of all ages and abilities, including pedestrians, foster social interaction and facilitate active transportation and community connectivity;</p> <p>b) planning and providing for the needs of persons of all ages and abilities in the distribution of a full range of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources;</p>	<p>Two parkettes are proposed to be conveyed to the City as parkland dedication and use by the general public. On-site, indoor and outdoor amenity spaces are provided for the benefit of residents.</p>
<p><b>4.6 Cultural Heritage and Archaeology</b></p>	
<p>4.6.3. Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property unless the heritage attributes of the protected heritage property will be</p>	<p>Previous Planning applications associated with the sites have provided a heritage impact statement to confirm that proposed development on the subject sites would not negatively impact the character of the Williamsville area or designated sites at 506</p>

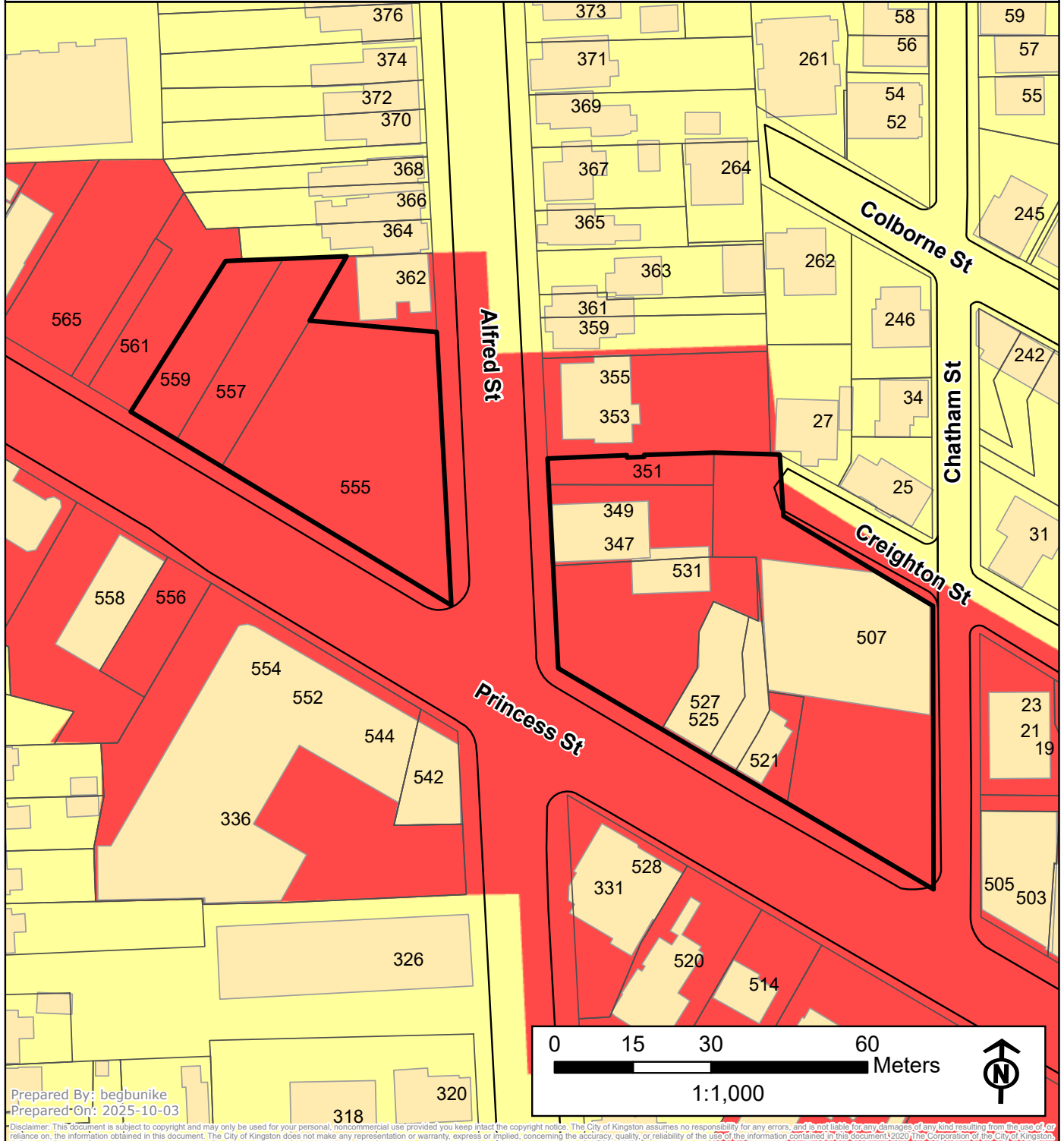
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conserved.	Princess Street and 520 Princess Street. The study found that a positive impact could be expected based on increased activity of the streetscape around 506 Princess Street and 520 Princess Street.



Planning Committee  
**Official Plan**  
**Existing Land Use**  
Address: 525 - 555 Princess Street  
File Number: D14-013-2025

 Main Street Commercial  
 Residential

 Subject Lands



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**Demonstration of Conformity to the Official Plan**

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<b>Section 2. Strategic Policy Direction</b>		
<p>2.1.1. Most growth will occur within the Urban Boundary, shown on Schedule 2, where development will be directed to achieve greater sustainability through:</p> <p>A. appropriate (minimum) densities.</p> <p>b. land use patterns that foster transit and active transportation;</p> <p>c. enhanced access to public amenities and spaces for all residents, visitors and workers;</p> <p>d. maximized use of investments in infrastructure and public amenities;</p> <p>i. parks that are planned to be accessed by urban residents within a ten minute walk and situated in locations that lessen the need for pedestrians to cross an arterial road or major highway;</p> <p>j. where possible, the preservation of mature trees for shade and their other beneficial ecological and community effects;</p> <p>m. encouraging a mix of land uses that provide for employment, education, personal service and convenience retail in close proximity to residential land uses, subject to compatibility matters as outlined in Section 2.7.</p>	<p>Sustainable Development – Urban Areas – Focus of Growth</p>	<p>The subject sites are located at the corner of arterial and local roads, with access to public transit, parks, commercial uses and institutional uses within walking distance. Each site provides short-term and long-term bike parking spaces and the sites are located near existing bike lanes along Princess Street.</p> <p>A total of 488 residential units are proposed across both sites. The increase in units and residents helps to support ridership of existing transit routes and places residents in proximity to potential employment opportunities.</p> <p>Each building proposes ground floor commercial uses which can serve residents and the wider public. The Zoning By-Law permits a wide variety of potential commercial uses at each site.</p> <p>Neither site contains existing mature trees. Landscaping and planting opportunities will be explored through the site plan control process and programming of proposed parkettes will be explored in consultation with the Parks department at a later date. Each parkette is proposed to be conveyed to the City to fulfill parkland dedication requirements.</p>

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<p>2.1.4. In reviewing development applications, the City will promote sustainability through:</p> <p>f. design which promotes a reduction of automobile trips, active transportation and transit, including secured public access to bicycle storage and parking;</p> <p>i. design that reduces municipal costs associated with the provision of infrastructure and municipal service delivery over the long term;</p> <p>k. development that suits the demographic and/or socio-economic needs of the community.</p>	<p>Sustainable Development – Development Review</p>	<p>f. A parking ratio of 0.2 spaces per dwelling unit was established through a previous minor variance application for each site. The reduction acknowledges that the sites are well located to facilitate active transportation and use of public transit as feasible alternatives to reliance on personal automobiles. Each site provides short-term and long-term bike parking.</p> <p>i. The proposed development does not require the building of new roads and can connect to existing municipal services.</p> <p>k. Each building provides a variety of unit types and sizes, ranging from studio unit to 3 bedroom units.</p>
<p>2.2.7. The City's existing Centres and Corridors, as shown on Schedule 2, are areas of mixed use and mixed buildings, including employment, residential, commercial and supporting uses and facilities. These will be the areas where intensification will be focused, and where greater densities of residential and non-residential development will be permitted.</p>	<p>City Structure – Centres and Corridors</p>	<p>The subject sites are located within a Corridor as identified on Schedule 2 of the Official Plan. The proposal includes a mix of residential and commercial uses on both sites.</p>
<p>2.3.1 The focus of the City's growth will be within the Urban Boundary, shown on Schedule 2, where adequate urban services exist, or can be more efficiently extended in an orderly and phased manner, as established by this Plan. Kingston's Water Master Plan and Sewer Master Plan will</p>	<p>Principles of Growth – Growth Focus</p>	<p>The subject sites are located within the Urban Boundary as shown on Schedule 2 of the Official Plan. The sites are located where water and wastewater infrastructure exists.</p>

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guide the implementation of the infrastructure planning.		
2.3.2. In 2013, residential density within the City’s Urban Boundary was 25.7 units per net hectare. The City intends to increase the overall net residential and non-residential density within the Urban Boundary through compatible and complementary intensification, the development of under-utilized properties and brownfield sites, and through the implementation of area specific policy directives tied to Secondary Planning Areas and Specific Policy Areas, as illustrated in Schedule 13.	Principles of Growth – Intensification	The proposed buildings include a combined total of 488 residential units and 1,260 square metres of commercial space within an area planned for growth and intensification.
2.3.4 Specific Policy Areas, identified in Schedule 13 to the Plan, are those areas which are largely developed and are experiencing fundamental change. Planning for such change may occur by way of a special area study (e.g., Williamsville Main Street Study), used to set expectations for guiding development and land use change.	Principles of Growth – Specific Policy Areas	The subject sites are located within the Williamsville Specific Policy Area on Schedule 13 of the Official Plan. Review of the proposal against the applicable policies of Section 10E are provided within this table, below.
2.4.1. The City supports sustainable development of a compact, efficient, urban area with a mix of land uses and residential unit densities that optimize the efficient use of land in order to:  a. reduce infrastructure and public facility costs;	Phasing of Growth – Vision	The subject sites are located with along existing roads with access to existing municipal services.  The proposed increase in residential units and commercial space in this area supports the use of active transportation and public transit options, which reduces reliance of personal automobiles.

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<p>b. reduce energy consumption and greenhouse gas emissions;</p> <p>c. support active transportation and viable public transit;</p> <p>d. conserve agriculture and natural resources within the City; and</p> <p>e. reduce reliance on private vehicles.</p>		
<p>2.4.3. It is the intent of this Plan to achieve an increase in the City’s net urban residential densities through promoting intensification and requiring minimum densities for residential development. It is also the intent of the City that intensification be focused within Centres and Corridors and in specific policy areas.</p>	<p>Phasing of Growth – Residential Density</p>	<p>The proposed development includes a total of 488 residential units and 1,260 square metres of commercial space across both sites.</p>
<p>2.4.4. New residential development and new secondary plans are subject to the following policies and minimum densities:</p> <p>c. for mixed use building developments in existing and proposed Centres and Corridors, a minimum density of 75 residential units per net hectare is established as the target for new residential development in order to support active transportation and transit;</p>	<p>Phasing of Growth – Minimum Residential Density</p>	<p>The site at 525 Princess Street proposes a residential density of approximately 800 residential units per net hectare and the site at 555 Princess Street proposes a residential density of approximately 793 residential units per net hectare.</p>
<p>2.6.6. New high-rise residential buildings should generally be located:</p> <p>a. within:</p>	<p>Residential Development – Appropriate Locations for New Low-Rise, Mid-Rise and</p>	<p>Both sites are located within a Corridor on Schedule 2 of the Official Plan, are located within the Williamsville Main Street Specific Policy Area and are on the periphery of a predominantly low-</p>

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<ul style="list-style-type: none"> <li>• a Centre or Corridor;</li> <li>• an area planned for high-rise residential in a Secondary Plan or a Specific Policy Area Plan; or</li> <li>• the periphery of a low-rise or mid-rise residential neighbourhood;</li> </ul> <p>b. within walking distance of areas designated for commercial use (i.e., any of the uses within the Commercial Hierarchy except for Neighbourhood Commercial);</p> <p>c. within walking distance of parkland, open space or community facilities; and</p> <p>d. on an existing arterial or collector road.</p>	<p>High-Rise Residential</p>	<p>rise residential neighbourhood to the north.</p> <p>Both sites are located off Princess Street, an arterial road, and within walking distance of parks, institutional uses and commercial uses.</p>
<p>2.7.3. The land use compatibility matters to be considered under Section 2.7.2 include, but are not limited to:</p> <ul style="list-style-type: none"> <li>a. shadowing;</li> <li>b. loss of privacy due to intrusive overlook;</li> <li>c. increased levels of light pollution, noise, odour, dust or vibration;</li> <li>d. increased and uncomfortable wind speed;</li> <li>e. increased level of traffic that can disrupt the intended function or amenity of a use or area or cause a decrease in the functionality of active transportation or transit;</li> </ul>	<p>Land Use Compatibility Principles – Land Use Compatibility Matters</p>	<p>a. Section 5.5 (Shadow Analysis) of the Planning Justification Report prepared by Fotenn Planning + Design includes shadow diagrams prepared by Hariri Pontarini Architects and assesses the difference in shadowing between the proposed buildings and the as-of-right built forms permitted by the current Exception Overlays. Analysis finds that the proposed buildings will introduce some shadowing on surrounding properties, which will be limited in duration, location and extent through intentional building design. Stepbacks on the north side of each building are included to, among other transition matters, minimize shadowing on adjacent low-rise properties north of the sites.</p> <p>b. The massing of buildings is intentionally concentrated along Princess Street to lessen potential</p>

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<p>f. environmental damage or degradation;</p> <p>g. diminished service levels because social or physical infrastructure necessary to support a use or area are overloaded;</p> <p>h. reduction in the ability to enjoy a property, or the normal amenity associated with it, including safety and access, outdoor areas, heritage or setting;</p> <p>i. visual intrusion that disrupts the streetscape or buildings;</p> <p>j. degradation of cultural heritage resources;</p> <p>k. architectural incompatibility in terms of scale, style, massing and colour;</p> <p>l. the loss or impairment of significant views of cultural heritage resources and natural features and areas to residents; or</p> <p>m. adverse effects on neighbouring properties related to stormwater management or drainage.</p>		<p>intrusive overlook and impact on low-rise residential properties north of the sites. The buildings are not proposed to have individual balconies on the north side, helping to further reduce potential intrusive overlook on adjacent residential properties.</p> <p>c. The proposed mix of residential and commercial uses is not anticipated to create increased levels of light pollution, noise, odour, dust or vibration above normal and expected levels for the proposed uses.</p> <p>d. A Pedestrian Level Wind Study was prepared by Gradient Wind and peer reviewed by Cermak Peterka Petersen (CPP) Wind Engineering Consultants. Potential mitigation measures include wind screens, dense plantings in tall planters and other landscaping elements depending on the programming of each space. The study demonstrates appropriate wind levels can be achieved through reasonable and appropriate mitigations measures. The final design, location and type of mitigations will be determined and secured through the site plan control process.</p> <p>e. Traffic and parking have been assessed for the subject sites through previous Planning applications. Traffic and parking were initially assessed at the time of the combined Official Plan Amendment and Zoning By-Law Amendment (City File Number D35-001-2019). Through File D35-001-2019, a Traffic and Parking Study was prepared by Robinson Consultants Inc. Consulting Engineers which found slight increases in traffic as a result of the proposed buildings were expected to be well within intersection capacities without reducing the level of service</p>

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		<p>beyond acceptable levels. Subsequently, a Parking Brief, dated October 20, 2023, was prepared by Parsons to support a minor variance request (City File Number D13-064-2023) to reduce the residential parking rate to 0.2 spaces per dwelling unit. From the time of this latest brief, the proposed number of dwelling units has been reduced from 614 units to 488 units across both sites.</p> <p>f. The subject sites are not located in close proximity to any natural heritage feature, including no waterbodies, woodlands or wetlands, and are not anticipated to result in any environmental damage or degradation to these features.</p> <p>g. The subject sites are located where physical infrastructure exists and is not anticipated to overwhelm existing road or transportation networks. The increase in residential density at this location will support transit ridership on nearby lines and benefit from existing sidewalks, roads and bike lanes. The sites are located within a short travel distance to the downtown core where residents of the proposed buildings can access cultural amenities such as theatres, recreation facilities, places of worship and community facilities. Nearby social services and infrastructure are not anticipated to be negatively impacted by the increase in residential units at these locations, and may benefit from an increased usership on a case-by-case basis.</p> <p>h. Through building design which focuses massing along Princess Street and minimizes shadow impacts on residential uses north of the site, it is not anticipated that the proposed changes to the built form will reduce the ability for</p>

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		<p>nearby neighbours to enjoy their properties.</p> <p>i. The building designs are intended to complement the existing neighbourhood, including concentration of massing towards Princess Street, stepbacks above the streetwall level, and ground floor commercial uses. The proposed pedestrian bridge is setback from Princess Street and is intended to create a flow between buildings rather than an interruption of the streetscape or visual lines.</p> <p>j. The subject sites do not contain and do not directly abut cultural heritage resources. The proposed development is not anticipated to have any negative impact on nearby properties with designated status under the <i>Ontario Heritage Act</i> (being 506 Princess Street and 520 Princess Street).</p> <p>k. The proposed buildings have been reviewed from an urban design perspective and are considered compatible in terms of scale, style, massing and colour. The buildings are designed to concentrate massing on the exterior corners of the lots and provide stepbacks towards adjacent and nearby low-rise uses. The stepback configuration of the buildings is secured through the height maps of each applicable Exception Overlay. The height maps regulate which portions of the property can be developed at specified heights. It should be noted that the subject Zoning By-Law amendments cannot secure building features such as colour or exterior material, however, the buildings are planned to consist of high-quality materials which provide visual interest and an intentional change in material between the building base and upper levels which provides visual</p>

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		<p>interest, contrast between the base and upper levels, and a break in overall massing. The Planning Justification Report, prepared by Fotenn Planning + Design, includes an urban design brief and shadow analysis in Section 5.0 of the report which outlines assessment of the urban structure and built form, placement and orientation of buildings, access, parking, utilities, loading, landscape elements, amenity areas, the pedestrian realm and accessibility.</p> <p>l. The proposed buildings will not impair views of nearby cultural heritage resources. The site at 525 Princess Street is located opposite Princess Street from 506 Princess Street and 520 Princess Street which are designated under Part IV of the <i>Heritage Act</i>. The proposed buildings are not anticipated to create a loss of views to these properties.</p> <p>The sites do not contain and are not adjacent natural heritage features.</p> <p>m. The current building designs cover a similar ground area as the previous designs permitted through the current Exception Overlays. As such, a stormwater management report has not been required to support the current Zoning By-Law amendment, however, stormwater management plans will be reviewed and secured through the site plan control process.</p>
<p>2.7.4. Mitigation measures may be used to achieve development and land use compatibility. Such measures may include one or more of the following:</p>	<p>Land Use Compatibility Principles – Mitigation Measures</p>	<p>The buildings are proposed with a 2 metre setback from Princess Street which is an increase in overall setback from the current height map of each Exception Overlay.</p> <p>The building at 525 Princess Street is proposed at a height of 44.15 metres, including the enclosed rooftop mechanical and amenity spaces. The</p>

Policy	Category	Review
<p>a. ensuring adequate setbacks and minimum yard requirements;</p> <p>b. establishing appropriate transition in building heights, coverage, and massing;</p> <p>c. requiring fencing, walls, or berming to create a visual screen;</p> <p>d. designing the building in a way that minimizes adverse effects;</p> <p>e. maintaining mature vegetation and/or additional new landscaping requirements;</p> <p>f. controlling access locations, driveways, service areas and activity areas; and,</p> <p>g. regulating location, treatment and size of accessory uses and structures, lighting, parking areas, garbage storage facilities and signage.</p>		<p>building at 555 Princess Street is proposed at a height of 44.4 metres, including the enclosed rooftop mechanical and amenity spaces. The buildings are designed with stepbacks above the 4<sup>th</sup> floor to provide a break in massing. Review of urban design elements, shadow analysis and wind study have been undertaken to ensure the proposed heights and form are appropriate modifications to the existing permitted built form.</p> <p>The buildings are designed to place service areas internal to the building and shielded from view by the building design. Entrances to underground parking garages on each site are proposed via Alfred Street. An additional service entrance is proposed off Creighton Street for the building at 525 Princess Street. No direct vehicular entrances are proposed via Princess Street.</p> <p>The sites contain minimal existing vegetation and new parkettes are proposed on each lot. The site at 525 Princess Street is proposed to dedicate 109 square metres of land at the corner of Chatham Street and Princess Street as parkland dedication. The site at 555 Princess Street is proposed to dedicate 118 square metres of land at the corner of Alfred Street and Princess Street as parkland dedication. Design and programming of these parkettes will be determined in consultation with Parks services at a later date.</p>
<p>2.7.6. Only development proposals that meet the long-term needs of the intended users or occupants will be supported. Proponents, whether developing individual buildings on a single site, or</p>	<p>Land Use Compatibility Principles – Functional Needs</p>	<p>a. The proposed buildings have been reviewed from an urban design, wind impact and shadow impact perspective and are found to be of suitable scale, massing and density for the existing context and area.</p>

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<p>multiple buildings being built at one time or phased over time, will be required to demonstrate to the satisfaction of the City that the functional needs of the occupants or users will be met by providing:</p> <p>a. suitable scale, massing and density in relation to existing built fabric;</p> <p>b. appropriate landscaping that meets or improves the characteristic green space amenity of the site and surroundings and enhances the City’s tree planting program;</p> <p>c. adequate land area and appropriate site configuration or provision for land assembly, as required;</p> <p>d. efficient use of municipal services, including transit;</p> <p>e. appropriate infill of vacant or under-utilized land; and,</p> <p>f. clearly defined and safe:</p> <ul style="list-style-type: none"> <li>• site access;</li> <li>• pedestrian access to the building and parking spaces;</li> <li>• amenity areas;</li> <li>• building entry; and,</li> <li>• parking and secure and appropriate bicycle facilities.</li> </ul>		<p>b. The site at 525 Princess Street proposes outdoor amenity space as a mix of courtyard and rooftop space for resident uses and a 109 square metre parkette at the corner of Chatham Street and Princess Street for public use. The site at 555 Princess Street proposed outdoor amenity space as a mix of ground level and rooftop space for residents and proposes a 118 square metre parkette at the corner of Alfred Street and Princess Street for public use.</p> <p>c. The site at 525 Princess Street consolidates parcels to enable redevelopment of the block between Alfred Street and Chatham Street. The proposal makes best use of the space available by placing parking in an underground parking garage. It is noted that the proposed parking garage is constrained in size which contributes to the request for reduction in accessible parking spaces.</p> <p>d. The sites are well located in an area with existing services, including water, wastewater and public transit services.</p> <p>f. The current Zoning By-Law amendment demonstrates the ability of the site to provide site accesses, amenity areas and provision of bike parking. Any additional signage, pavement markings or other wayfinding features will be finalized through the site plan control application to ensure the site is functional and safe.</p>
<b>Section 3. Land Use Designation &amp; Policy</b>		
3.4.C.2. Permitted commercial uses include retail, service and office uses that are suitable for	Main Street Commercial – Permitted	The proposed buildings include ground floor commercial spaces. The underlying WM1 Zone applicable to

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<p>an accessible main street pedestrian format, and are intended to serve the surrounding neighbourhoods. For properties within a Main Street Commercial designation, land use on the ground floor is required to be commercial, unless otherwise identified in a Specific Policy Area of Section 10 of this Plan.</p>	<p>Commercial Uses</p>	<p>each site in the Zoning By-Law permits a variety of commercial uses including but not limited to retail stores, restaurants, banks and fitness centres.</p>
<p>3.4.C.5. Within the Main Street Commercial designation, residential uses are permitted as upper storey uses. Ground floor residential uses may be permitted if identified in a Specific Policy Area of Section 10 of this Plan. Where ground floor residential uses are permitted, the building design must contribute to the pedestrian activity and amenity of the street and complement the commercial storefront design and character of the street. The height of the ground floor units must enable future conversions to commercial uses.</p>	<p>Main Street Commercial – Residential Uses</p>	<p>The ground floor of each building is primarily dedicated to commercial uses with the exception of residential lobbies and amenity areas which contribute to an active streetscape and pedestrian realm. Proposed ground floor heights range from 6.45 metres to 6.6 metres to support a variety of potential commercial tenants.</p> <p>The upper storeys are dedicated to residential use including amenity spaces.</p>
<p>3.4.C.7. All new development or conversions will be required to provide parking for vehicles and bicycles in accordance with the zoning by-law and will be encouraged to locate parking underground or in structures. If it is not possible to locate sufficient parking on site, parking may be provided off-site, at a distance stipulated in the zoning by-law, through long-term agreements registered on title to both</p>	<p>Main Street Commercial – Parking</p>	<p>Both sites propose an underground parking structure to supply standard parking, accessible parking and long-term bike parking. The proposed Zoning By-Law amendment seeks to incorporate the permissions of a previous minor variance which reduced the standard parking requirement to 0.2 spaces per dwelling unit. Visitor and car-share parking requirements are proposed to be calculated as part of the 0.2 space per dwelling unit calculation.</p>

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properties. Cash-in lieu of parking may be accepted by Council in accordance with Section 9.5.11 of this Plan, where it is not feasible to provide on-site parking.		
3.4.C.9. New development within the Williamsville Main Street is directed by the Williamsville Main Street Study (2012) and Addendum (2020), which provide specific design direction and are further detailed in the policies for the Princess Street Corridor Specific Policy Area: Williamsville Main Street in Section 10E.1 of this Plan.	Main Street Commercial – Williamsville Main Street	A review of policies related to the Princess Street Corridor Specific Policy Area: Williamsville Main Street in Section 10E.A., which implement the findings of the Williamsville Main Street study, are provided within this table.
<b>Section 4. Infrastructure &amp; Transportation</b>		
4.1.1. New development will proceed only if the City is satisfied that adequate services, roads, and utilities are available, or can be made available, to serve the proposal adequately. In determining the adequacy of servicing, utility systems, or the transportation system, the City will consider not only the proposal, but also the potential for development that exists in the same service area.	General Policies – New Development	The subject sites are located in an area served by municipal water and wastewater infrastructure, municipal roads and public transit.
4.6.6. The City supports the development of convenient, accessible and appealing streetscapes through such measures as providing wide sidewalks, street furniture, trees and amenities, including convenient transit stops.	Transportation – Pedestrian Friendly Streetscapes	The proposed development provides an increased setback of 2 metres from Princess Street, compared to reduced setbacks permitted through the existing Exception Overlays. The building designs maintain commercial uses with entrances off Princess Street to facilitate an active pedestrian realm. The setback from Princess Street and dedication of corner parkettes facilitates the potential

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		for street furniture, improved bus stops, and commercial spill-out activities.
<p>4.6.61. The zoning by-law will be used to regulate the supply of accessible parking as required by provincial legislation. The location of accessible parking spaces shall provide enhanced accessibility through a consideration of factors including, but not limited to, the distance between parking spaces and accessible building entrances, security of the parking area, lighting of the area, protection from the weather, and ease of maintenance.</p>	<p>Transportation – Accessible Parking</p>	<p>The proposal to provide accessible parking above Accessibility for Ontarians with Disabilities Act (AODA) requirements is consistent with the Official Plan policy to provide accessible parking as required by provincial legislation. Use of the effective ratio method described within the Zoning By-Law would result in a requirement well above the minimum requirements of the AODA.</p> <p>The location and enhancements to accessible parking spaces provided will be reviewed by the Municipal Accessibility Advisory Committee during the site plan control process to ensure the best location and use of the spaces provided.</p>
<p>4.7.8. All new multi-residential, commercial, industrial and institutional development must incorporate appropriate vaults, central storage, collection areas and other facilities and enclosures for the storage of garbage, recycling, organics and other waste materials in order to facilitate sustainable waste management practices. All such facilities for the storage of garbage, recycling, organics, and other waste materials must be shown on site plan control applications, as required under Section 9 of this Plan.</p>	<p>Solid Waste Management – Requirements for New Development</p>	<p>Servicing areas, including storage of waste and recycling is currently proposed internal to each building which functions to contain waste, limit pests and shield these service areas from view.</p>
<p><b>Section 8. Urban Design</b></p>		
<p>8.4. Through the review of development proposals, construction of public works, or the preparation and approval</p>	<p>Urban Design Guidelines –</p>	<p>a. The proposed buildings are subject to accessibility requirements of the <i>Ontario Building Code</i> and <i>Accessibility for Ontarians with Disabilities Act, 2005</i>,</p>

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<p>of area plans, the City will promote the provision of barrier-free access and safety by:</p> <ul style="list-style-type: none"> <li>a. providing for age-friendly needs and the requirements of people with disabilities, and others requiring access supports through improved amenities such as parking, benches, and washrooms, clear signage, visual or auditory indicators, and other means as appropriate;</li> <li>b. improving public security through enhanced lighting, visibility of public areas, provision of entrance locations in well-traveled areas, and ease of access for emergency personnel or vehicles;</li> <li>c. clearly defining building entrances and avoiding designs that would create areas that are hidden from public view and thus potentially available for criminal activity;</li> <li>d. arranging public uses and amenities within a convenient walking distance;</li> <li>e. providing adequate walkway widths, visually permeable materials and structures, and landscaping elements that do not obstruct sightlines in the design of streetscapes, transportation facilities, or public buildings and places; and,</li> <li>f. promoting safe environments by applying Crime Prevention Through Environmental Design</li> </ul>	<p>Accessibility and Safety</p>	<p>which includes requirements for barrier free entrances, units, and parking spaces. Site features such as benches, signage, and visual, auditory and tactical aids will be reviewed through the site plan control process.</p> <ul style="list-style-type: none"> <li>b. Principal building entrances for commercial units and residential lobbies are located in well-traveled areas including along the Princess Street frontage and from courtyard amenity space areas. Lighting and on-site directional signage will be reviewed through the site plan control process.</li> <li>c. Building entrances are proposed to be clearly defined and identifiable and accessed from public sidewalks or pathways within internal courtyards. Each site is designed to avoid creating areas hidden from public view.</li> <li>d. Outdoor amenity areas on each site can be accessed directly from the rear of each building. Public parkettes are proposed on the corners of Princess Street and Chatham Street (at 525 Princess Street) and Princess Street and Alfred Street (at 555 Princess Street). The parkettes are intended for public use and are easily accessible from public sidewalks.</li> <li>e. Pedestrian walkways will be reviewed through the site plan control process to ensure site functionality. As designed, the site can easily accommodate safe walkways and pathways that do not obstruct the design of streetscapes, transportation facilities or building entrances.</li> <li>f. The application of CPTED concepts and principals will be reviewed through the site plan control process. Review may include orientation of pathways, visual permeability of building materials</li> </ul>

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(CPTED) concepts and principles in the design of buildings, site layout and landscaping of development sites.		around key portions of each building, adequate lighting, on-site signage and delineation of public and private spaces. It is anticipated that the building design can accommodate CPTED concepts and principals.
<p>8.5. Through the review of development proposals, construction of public works, or the preparation and approval of area plans, the City shall maintain or enhance the character of valued streetscapes, community areas, and landscapes by:</p> <p>a. preserving human scale in locations that are pedestrian-oriented and establishing an appropriate street wall height by controlling building heights through an implementing zoning by-law, requiring building step-backs, having entrances at street level, providing street furniture, and other means as appropriate;</p> <p>b. providing shade through natural or built means to provide comfortable outdoor environments and provide protection from ultraviolet radiation;</p> <p>c. protecting views to the water, City Hall and other significant buildings or landscapes;</p> <p>d. siting new buildings and structures in a manner that repeats and complements the siting and spacing of existing buildings, structures or landscaped areas in order to continue a pattern that is</p>	<p>Urban Design Guidelines – New Development</p>	<p>a. The building proposed at 525 Princess Street proposes a streetwall ranging from 4 to 6 storeys and the building proposed at 555 Princess Street proposes a streetwall ranging from 2 to 6 storeys. The proposed streetwalls take into account existing surrounding uses and are provided at a scale which is pedestrian oriented and human-scale. Stepbacks are proposed for upper storeys to contribute to a human-scale experience on the street level. Commercial entrances are proposed along Princess Street which support the pedestrian experience.</p> <p>b. Canopies are proposed above building entrances to provide shade and weather protection. The proposed amendments include a request to permit canopies to project 2 metres into required setbacks.</p> <p>c. The subject sites are not subject to protected views.</p> <p>d. The proposed buildings include intentional streetwall heights and stepbacks to complement the existing built forms of the area.</p> <p>e. Service areas, such as garbage and recycling rooms, are proposed within the interior ground floor of each building.</p> <p>f. A parkette is proposed on the corner of each site, to be dedicated to fulfill parkland dedication requirements. The programming of each space will be determined through future consultation with Parks staff. The location of each</p>

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<p>characteristic of surrounding neighbourhoods and heritage areas;</p> <p>e. the strategic use of building separation, landscaping and buffers to mitigate inharmonious elements of the built or natural environment, such as railways, service areas, or incompatible uses;</p> <p>f. designing public spaces or requiring the design of common spaces in private projects to have a clear sense of definition, and provide sufficient amenity, accessibility and security to encourage public use and linkage to other public areas;</p> <p>g. preserving and enhancing the context of special buildings, streetscapes, landscapes and sites that have been identified as having architectural, or cultural heritage value or interest; and,</p> <p>h. encouraging innovative methods to minimize the visual impact of utility features, either by containing utility features within streetscape elements or by screening them from view.</p>		<p>parkette facilitates easy public access from public sidewalks.</p> <p>g. The subject sites are located in proximity to two properties designated under Part IV of the <i>Ontario Heritage Act</i>. The proposed buildings are not anticipated to negatively impact the nearby designated properties. The design of the buildings is anticipated to have a positive impact on the streetscape and local context.</p> <p>h. The enclosed rooftop mechanical area is setback from the edge of the 10<sup>th</sup> storey to minimize visibility and is proposed to be constructed with a material that contributes to desirable urban design rather than detracting from it.</p>
<p>8.6. The City requires the design of new development to be visually compatible with surrounding neighbourhoods and areas of cultural heritage value or interest through its site plan control review, preparation of zoning standards, and urban design</p>	<p>Urban Design Guidelines – New Development</p>	<p>Setbacks and stepbacks are intentionally proposed to complement the existing surrounding context and positively contribute to the urban design of the area. While the subject Zoning By-Law amendment and site plan control process cannot regulate the materiality of the proposed buildings, the contemplated surface materials and design elements are shown on the</p>

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<p>guidelines, as appropriate, that address the following:</p> <p>a. siting, scale and design of new development in relation to the characteristics of the surrounding neighbourhood or the significant cultural heritage resources including, scale, massing, setbacks, access or landscape treatments;</p> <p>b. protecting natural heritage features and areas and cultural heritage landscapes through the siting, design and review of new development;</p> <p>c. promoting innovation in building design to create an interesting and varied built environment, to increase sustainability by improving energy efficiency, and to deliver barrier-free accessibility;</p> <p>d. achieving compatibility in land use and with a predominant architectural style, street pattern or site arrangement where that style or arrangement forms a valuable component of the existing neighbourhood or the cultural heritage value or interest of the identified area. Section 2.7 provides additional policy in this regard; and,</p> <p>e. encourage spaces, services and facilities that highlight arts and culture in a manner that generates and sustains cultural vitality.</p>		<p>architectural drawings of Exhibit H to this report.</p> <p>The proposal includes an elevated pedestrian bridge between the buildings which is a unique feature for the area and across the City and is intended to act as a landmark in the area.</p> <p>The subject sites do not contain natural or cultural heritage features.</p>

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<b>Section 9. Administration &amp; Implementation</b>		
<p>9.5.9. When considering an application to amend the zoning by-law, the Planning Committee and Council will have regard to such matters as:</p> <ul style="list-style-type: none"> <li>a. conformity of the proposal with the intent of the Official Plan policies and schedules;</li> <li>b. compatibility of the proposal with existing uses and zones, sensitive uses, the natural heritage system, cultural heritage resources, and compatibility with future planned uses in accordance with this Plan;</li> <li>c. compatibility of proposed buildings or structures with existing buildings and structures, with zoning standards of adjacent sites, with any future planned standards as provided in this Plan, and with any urban design guidelines adopted by the City for the area;</li> <li>d. the extent to which the proposal is warranted in this location and the extent to which areas zoned for the proposed use are available for development;</li> <li>e. the suitability of the site for the proposal, including its ability to meet all required standards of loading, parking, open space or amenity areas;</li> <li>f. the suitability of the density relative to the neighbourhood and/or district, in terms of units</li> </ul>	<p>By-Laws – Planning Committee/ Council Considerations</p>	<p>The proposed Zoning By-Law amendment is consistent with the policies of the Official Plan, as outlined within this table. This sites are designated Main Street Commercial and are subject to Site Specific Policy Area Number 7. An Official Plan amendment is not required to facilitate the proposed uses.</p> <p>The proposed buildings consider existing uses and potential future uses under adjacent WM1 and URM7 Zones. The proposed buildings contain ground floor commercial uses consistent with the area requirement along Princess Street.</p> <p>The use of setbacks, stepbacks and materiality contribute to compatibility of the proposed buildings with surrounding uses and built form.</p> <p>The proposed amendments to Exception Overlay Numbers E83 and E84 maintain a similar built form as current permissions and does not propose a change in use. The introduction of residential units and commercial spaces at these locations are well supported by nearby amenities and businesses.</p>

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<p>per hectare, bedrooms per hectare, floor space index, and/or employees per hectare, as applicable;</p> <p>g. the impact on municipal infrastructure, services and traffic;</p> <p>h. comments and submissions of staff, agencies and the public; and,</p> <p>i. the degree to which the proposal creates a precedent.</p>		
<b>Section 10E – Princess Street Corridor Specific Policy Area</b>		
<p>10E.1.1. The vision for the Williamsville Main Street corridor is a vibrant and active intensification area with a mix of land uses framing an improved, pedestrian-oriented streetscape.</p>	<p>Williamsville Main Street – Vision</p>	<p>The proposed buildings are designed to facilitate an active pedestrian realm, including providing ground floor commercial uses with access from the sidewalk along Princess Street.</p>
<p>10E.1.2.</p> <p>a. Ensure community vitality through a mix of uses that includes retail/commercial at grade.</p> <p>b. Improve the pedestrian and cyclist experience along Princess Street.</p> <p>c. Identify opportunities to green the public and private realm.</p> <p>d. Guide development at an appropriate scale and density that is compatible with the street width and neighbourhood context.</p> <p>e. Encourage high quality architecture that is representative of the cultural heritage of Williamsville.</p>	<p>Williamsville Main Street – Guiding Principles</p>	<p>a. Each site proposes ground floor commercial use and upper-storey residential use. The type of commercial uses permitted are outlined within the Zoning By-Law. The high ceilings proposed for each ground floor level provides flexibility to attract a variety of commercial tenants.</p> <p>b. Each building is proposed with a 2 metre setback from Princess Street which is an overall increase in setback from current permissions for the sites. The increased setback supports a widened pedestrian path and enables street furniture, commercial spill-out uses and enhancing landscaping. A parkette is proposed on the corner of each site which will provide amenity and greenspace for public use. The design and specific amenities of each space</p>

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<p>f. Protect existing residential areas from adverse effects.</p> <p>g. Provide a sustainable framework for future development, including phasing-in of development so that it does not compromise the long-term servicing strategy for Williamsville.</p>		<p>will be determined in consultation with Parks department staff at a later date.</p> <p>c. Landscaping and tree planting opportunities on private property, including outdoor amenity space on each site, will be reviewed through the site plan control process. Landscaping, tree planting, street furniture and public art on the proposed parkettes will be reviewed in consultation with the Parks department when conveyed to the City.</p> <p>d. The design of each building considers the surrounding context and provides a transitional streetwall which accounts for the existing built form of surrounding uses.</p> <p>e. The proposed design of each building is illustrated in the architectural drawing sets for each site. The buildings are proposed to feature high quality architectural design including brick and glazing throughout.</p> <p>f. Each building is designed to consider existing surrounding uses and utilizes setbacks, stepback and articulation to limit impact on nearby properties. Wind and shadow studies have also been provided to ensure that the design of each building will not create adverse impacts on nearby uses, including the residential neighbourhood north of the site.</p> <p>g. Both sites are located in areas where municipal services are available. It is anticipated that the site at 555 Princess Street will require application under the Municipal Servicing Allocation Policy prior to construction.</p>
<p>10E.1.3. Permitted uses are outlined in Section 3.4.C, Main Street Commercial. Ground floor commercial uses are</p>	<p>Williamsville Main Street – Permitted Uses</p>	<p>The subject sites are proposed to be developed in accordance with the permitted uses of the Main Street Commercial designation and WM1 Zone</p>

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<p>required for properties on Princess Street as shown on Schedule PS-1. Other properties in the area may be developed with a mix of commercial/office and residential at-grade.</p>		<p>of the Zoning By-Law. Each building proposes ground floor commercial uses.</p>
<p>10E.1.5. The Williamsville Main Street has an evolving character that will continue to be defined by its role as a main transportation corridor and a priority transit route for the City. New development must protect, enhance, support or adaptively re-use cultural heritage resources. The heritage character statement for the Williamsville corridor is as follows: “The Williamsville study area is a linear mixed use district with land uses and built form largely determined by the evolving nature of Princess Street.” The key heritage character defining element of the corridor is the pattern of streets and blocks determined by the juxtaposition of Princess Street and the existing street grid.</p>	<p>Williamsville Main Street – Cultural Heritage Resources and Character</p>	<p>The design of each building and the connecting pedestrian bridge is intended to be a focal point of the area and a landmark feature. The design of each building is intended to positively contribute to the character of the corridor.</p>
<p>10E.1.6. Transit service is important to further growth and development within the Williamsville Main Street. The Williamsville Main Street is part of the Princess Street Corridor, which is identified in the Official Plan as a priority transit route and the focus of future intensification.</p>	<p>Williamsville Main Street – Transit</p>	<p>The proposed intensification of residential units on each site supports public transit ridership along the corridor.</p>
<p>10E.1.7. New development within the Williamsville Main Street shall be designed in</p>	<p>Williamsville Main Street – Urban &amp;</p>	<p>The proposed building designs contemplate the urban design policies of Section 8 of the Official Plan as well as</p>

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<p>accordance with the urban design principles developed through the addendum to the Williamsville Main Street Study (2020). Any application for new development will be reviewed in terms of its compatibility with the architectural character of the area in which it is located and based on the addendum to the Williamsville Main Street Study (2020).</p>	<p>Sustainable Design</p>	<p>the considerations of the Williamsville Main Street Study. The Planning Justification Report includes a detailed urban design brief which has been reviewed by urban design staff in Planning Services.</p>
<p>10E.1.8. Storm water management is required. Buildings and site design shall control the rate of storm water run-off as per the City's current design criteria.</p>	<p>Williamsville Main Street – Urban &amp; Sustainable Design</p>	<p>Initial stormwater management reports and plans have been reviewed as part of previous Planning applications on the subject sites, which indicate feasibility of appropriate stormwater management. The proposed site designs are similar to previous plans, however, further stormwater management plans will be reviewed through the site plan control process.</p>
<p>10E.1.11. New development will be sited and designed to optimize pedestrian comfort related to weather, including, but not limited to, precipitation, heat, cold, shadowing, and wind.</p>	<p>Williamsville Main Street – Urban &amp; Sustainable Design</p>	<p>A wind study has been provided to confirm pedestrian comfort. Canopies are proposed above building entrances to provide weather protection and improve the comfort of the pedestrian realm.</p>
<p>10E.1.12. The use of high quality and appropriate exterior building materials at ground level, particularly at the streetwall and areas that are visible from Princess Street, is an important design consideration to help new development support the public realm and fit within the planned context for Williamsville. New development should utilize primary exterior materials selected for their permanence,</p>	<p>Williamsville Main Street – Urban &amp; Sustainable Design</p>	<p>The proposed buildings include the use of varying brick colour and increased glazing to enhance visual interest and positively contribute to the area character.</p>

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durability and energy efficiency.		
10E.1.14. The setbacks along the street frontages are intended to provide a wider pedestrian realm. A widened pedestrian realm provides room for pedestrian movement, window shopping, chance meetings, retail overflow, small patios, and doorways and building entrances. This area may also include private street furniture, private signage, merchandise displays, and other similar elements.	Williamsville Main Street – Public Realm	Each building is proposed with a 2 metre setback from Princess Street which is an increase from the current permissions for each site.
10E.1.16. The sidewalk and front façade of developments fronting onto Princess Street should generally be continuous, except where building forecourts, gardens, or other public access is required.	Williamsville Main Street – Public Realm	The proposed buildings provide a continuous setback of 2 metres along Princess Street, which is a more consistent façade than by-right designs. Public sidewalks exist and are planned to be maintained along Princess Street.
10E.1.17. The floor-to-floor height of the ground level must be a minimum of 4.5 metres. This will facilitate commercial uses at grade and will ensure that the ground floor has a continuous character.	Williamsville Main Street – Ground Floor Conditions	<p>The building at 525 Princess Street proposes a minimum floor to floor height of the ground level at 4.5 metres, however, the majority of the ground floor is intended to be 6.6 metres.</p> <p>The building at 555 Princess Street proposed a minimum floor to floor height of the ground level at 4.2 metres, which maintains the existing permission of the Exception Overlay. The actual ground floor height is proposed primarily at 6.45 metres.</p>
10E.1.18. In the case of corner sites, the commercial uses should wrap the corner of the building, occupying a portion of the frontage on the secondary street.	Williamsville Main Street – Ground Floor Conditions	The building at 525 Princess Street proposes commercial units which wrap Princess Street, Alfred Street and Chatham Street frontages. The building at 555 Princess Street proposes

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<p>10E.1.19. Entrances for all land uses off of Princess Street, Division Street, Bath Road and Concession Street, as well as any commercial uses that wrap the corner of any side streets off of Princess Street, must be constructed at-grade to be accessible and to allow for viable commercial spaces.</p>		<p>commercial units which wrap Princess Street and Alfred Street frontages.</p> <p>Entrances to commercial and residential spaces off Princess Street are proposed at grade.</p>
<p>10E.1.21. Where residential uses are proposed on side streets not listed in Section 10E.1.19, each unit shall have an independent pedestrian access. Some entrances may be raised above sidewalk level to provide transition from the public to private realm and/or to provide private amenity space or landscaping to buffer the residential unit from the public realm.</p>	<p>Williamsville Main Street – Ground Floor Conditions</p>	<p>The site at 525 Princess Street proposes townhouse dwellings along Chatham Street and Creighton Street frontages which will be accessed by a set of stairs to independent entrances.</p>
<p>10E.1.22. Canopies, cantilevers, awnings, recessed entrances, covered walkways and porticoes are recommended to provide weather protection to pedestrians and help articulate building elevations.</p>	<p>Williamsville Main Street – Ground Floor Conditions</p>	<p>Canopies are proposed above residential and commercial unit entrances.</p>
<p>10E.1.23. To encourage pedestrian interaction and enhance safety, facades facing Princess Street or adjacent to public open spaces shall be composed of large areas of glazing and should occupy a minimum of 60% of the ground floor frontage. The treatment of the ground floor shall be highly transparent with strong visual connections between the street</p>	<p>Williamsville Main Street – Ground Floor Conditions</p>	<p>Both buildings propose the use of glazing at ground floor and upper levels to support high quality urban design and an improved pedestrian realm.</p>

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<p>and the ground floor interior spaces. Clear glass is preferable to promote the highest level of visibility. Lifestyle graphics and other forms of images that result in a solid panel behind glazing, or other permanent opaque coverings on windows and doors that prevent views into the ground floor of buildings are not considered glazing for the purpose of this policy.</p>		
<p>10E.1.24. Where residential or office uses are included above commercial uses, a separate exterior entrance must be provided at-grade. Long frontages without active entrances are discouraged.</p>	<p>Williamsville Main Street – Ground Floor Conditions</p>	<p>Residential entrances are provided at the ground level, separate from commercial entrances. The sites do not propose long frontages without entrances.</p>
<p>0E.1.25. Building massing will be articulated or broken up through a continuous rhythm of building fronts achieved through a pattern of projections and recessions, entrances, display spaces, signage, and glazed areas to ensure that facades are not overly wide. The intent is to create the sense of having multiple buildings along the width of the building. Vertical breaks and stepbacks will also be required.</p>	<p>Williamsville Main Street – Building Width and Articulation</p>	<p>Both buildings are proposed to feature a mix of brick and glazing to provide visual interest and provide distinction between the ground floor and upper storeys. Stepbacks are provided to provide breaks in the building's overall massing.</p>
<p>10E.1.26. New development shall support a vibrant pedestrian environment by establishing and maintaining a continuous streetwall that frames Princess Street. New development should provide a</p>	<p>Williamsville Main Street – Streetwall Heights</p>	<p>The building at 525 Princess Street proposes a streetwall which varies from 4 storeys to 6 storeys and the building at 555 Princess Street proposes a streetwall 2 storeys to 4 storeys along Princess Street. The majority of the streetwall for 555 Princess Street is proposed at 4 storeys, with a small portion reducing to 2 storeys adjacent</p>

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streetwall height of three to four storeys.		existing development. The 2 storey portion of streetwall would be 10.15 metres in height, due to an increase in ceiling height of the first floor.
<p>10E.1.27. Where new development is adjacent to existing development on a side street, the new development will reflect the scale of that development in its design and provide for an appropriate built form transition.</p> <p>10E.1.29. Mid-rise buildings shall be set back from lot lines shared with properties designated for residential use. The intent is to concentrate building massing near Princess Street and provide physical separation between the new larger development and existing residential uses. This setback will be detailed in the implementing zoning by-law.</p>	Williamsville Main Street – Streetwall Heights	Both sites are located south of existing residential development. Both sites incorporate setbacks on the north side to improve transition to existing residential uses. Four storey townhouses are proposed along Chatham Street and Cregihnton Street frontages, which provides a gentle transition to similar built forms in the existing neighbourhood. Both buildings concentrate massing towards the corners of Princess Street and Alfred Street.
10E.1.28.	Williamsville Main Street – Streetwall Heights	Both sites are exempt from Section 10E.1.28. through Site Specific Policy Area Number 73.
10E.1.32.	Williamsville Main Street – Tall Buildings	Both sites are exempt from Section 10E.1.32. through Site Specific Policy Area Number 73.
<p>10E.1.33. New developments shall not contain balconies that project beyond the face of the building for the first three storeys for all facades that face a street. For clarity, balconies can be inset to provide private amenity space for residents for the first three storeys.</p> <p>Balconies on the rear façade of <i>developments</i> may project from the building face above</p>	Williamsville Main Street – Projections	The proposed buildings do not include balconies.

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<p>the second storey but should be set back from the rear property line. Balconies of new <i>developments</i> shall not encroach into the public road allowance.</p>		
<p>10E.1.40. Vehicle access points and loading and servicing areas shall be appropriately located and screened from public view, and shall be developed in accordance with the following policies:</p> <p>a. Wherever possible, vehicular access to on-site parking, loading, and servicing facilities shall be provided from side streets and rear lanes, and not from Princess Street.</p> <p>b. Loading and service areas will be screened from prominent public areas and adjacent residential areas.</p> <p>c. Service and drop-off area circulation shall not interfere with accessible pedestrian circulation.</p> <p>d. Servicing and loading areas should be located in a coordinated manner within buildings rather than in adjacent structures or in outdoor areas. Garbage, loading, servicing, and utility functions should be integrated either adjacent to, or within the interior of a building at the rear whenever possible, with access from a rear lane or side street.</p>	<p>Williamsville Main Street – Vehicle Access, Loading and Servicing</p>	<p>a. No vehicular accesses are proposed from Princess Street. Vehicular entrances, including access to loading spaces, short-term vehicle spaces and service areas are accessed from Alfred Street and Creighton Street.</p> <p>b. Loading and service areas are proposed interior to the buildings and are shielded from view.</p> <p>c./d. Vehicular and pedestrian accesses are well organized to avoid potential conflict. Loading and servicing areas are provided internal to each building.</p> <p>e. There are no curb cuts proposed along Princess Street. Existing site accesses from Princess Street will be closed as part of the redevelopment.</p>

Policy	Category	Review
<p>e. The number of curb cuts shall be reduced along Princess Street, wherever possible. This will increase opportunities for landscaping treatments and street furnishings, while creating continuity and providing safety to the pedestrian environment. New curb cuts are not recommended for developments fronting onto Princess Street. All such developments should be accessed by existing rear lanes, new rear lanes, side streets, or adjacent properties.</p>		
<p>10E.1.42. In addition to parks within walking distance of the Williamsville Main Street, such as Victoria Park and the Memorial Centre, future development plans for the main street area are encouraged to include small urban parkettes. As intensification occurs, and more people live and work in the area, it is important that the main street be supported by new open spaces that allow residents access to outdoor space, that improve the pedestrian experience of the streetscape, that provide green landscaping where possible, and that bring people to the area.</p> <p>a. Parkettes are intended to be small in size, to accommodate intense and all-season uses, and to contain hardscape surfaces and elements, such as sitting areas and public art,</p>	<p>Williamsville Main Street – Parkettes</p>	<p>The site at 525 Princess Street proposes a parkette measuring 109 square metres in area to be conveyed as parkland dedication on the corner of Princess Street and Chatham Street. The site at 555 Princess Street proposes a parkette measuring 118 square metres in area to be conveyed as parkland dedication on the corner of Princess Street and Alfred Street.</p> <p>The design and function of the parkettes will be determined at a later date to provide public access to the amenities.</p>

Policy	Category	Review
<p>and adequate soft landscape planting amenities.</p> <p>b. Where publicly accessible open space is required as part of the development of private property, this open space would need to be secured through parkland dedication, donation, acquisition, or a combination of these methods.</p> <p>c. Parkette features should reinforce the urban street edge and the parkette should be configured to allow for the functional design and placement of public amenities, such as street trees or benches.</p> <p>d. The final decision on the design of a parkette, and the facilities or amenities to be included in a parkette, shall be made by the City.</p>		



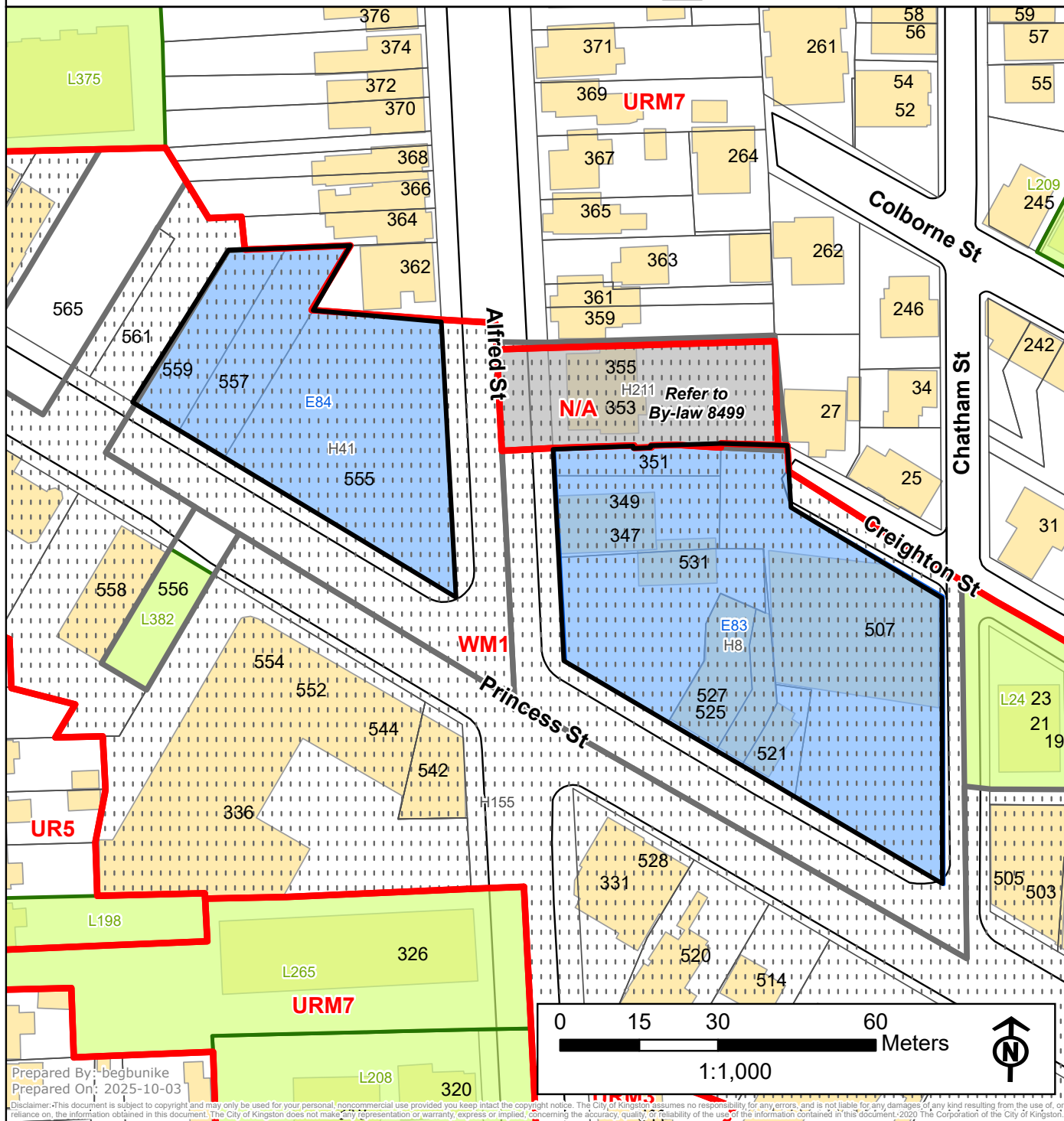
Planning Committee  
Existing Zoning  
Kingston Zoning By-Law 2022-62

Address: 525 - 555 Princess Street  
File Number: D14-013-2025

Subject Lands

Schedule 1 Zoning Map

- Zone
- Not Subject to the Kingston Zoning By-law
- Schedule E - Exception Overlay
  - Legacy Exceptions (LXXX)
  - Exceptions (EXXX)
- Schedule F - Holding Overlay
  - Holding Overlay (HXXX)



Prepared By: begbunike  
Prepared On: 2025-10-03

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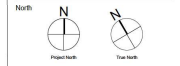




# Exhibit H Report Number PC-26-003

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Project File:  
 525 PRINCESS STREET  
 KINGSTON, OH

9th FLOOR

Project Number: 2026  
 Scale: 1/32"  
 Date: 12 DECEMBER 03, 2025  
 Drawn By: HPA  
 Revision:

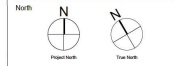
A2.10

# Exhibit H

## Report Number PC-26-003

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3. Materials or products or methods mentioned in general notes, details, and schedules are subject to the Architect's approval. The contractor shall obtain the Architect's approval for any substitutions or changes. These items or substitutions will be located as indicated by the Architect.



Revised For	DATE
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Contractor	10/21/2025
Other	

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Project Name:  
**525 PRINCESS STREET**  
 KINGSTON, OH

**10TH FLOOR**

Project Number: 2025  
 Scale: 1/8" = 1'-0"  
 Date: 15 DECEMBER 03, 2025  
 Drawn by: HPA  
 Revision:

Drawing No.:  
**A2.11**











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3. Materials of equivalent or better materials or of different brands, types, and grades are subject to the Architect's approval. The Authority shall be notified of any such changes prior to the start of construction. These items are clearly marked as to be located as indicated by the Architect.



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Project Name  
**525 PRINCESS STREET**  
KINGSTON, ON

**EAST ELEVATION**  
**NORTH ELEVATION**

Project Number: 2026  
Scale: 1/32  
Date: 15 DECEMBER 03, 2025  
Drawn by: HPA  
Drawing No.:  
Revision:















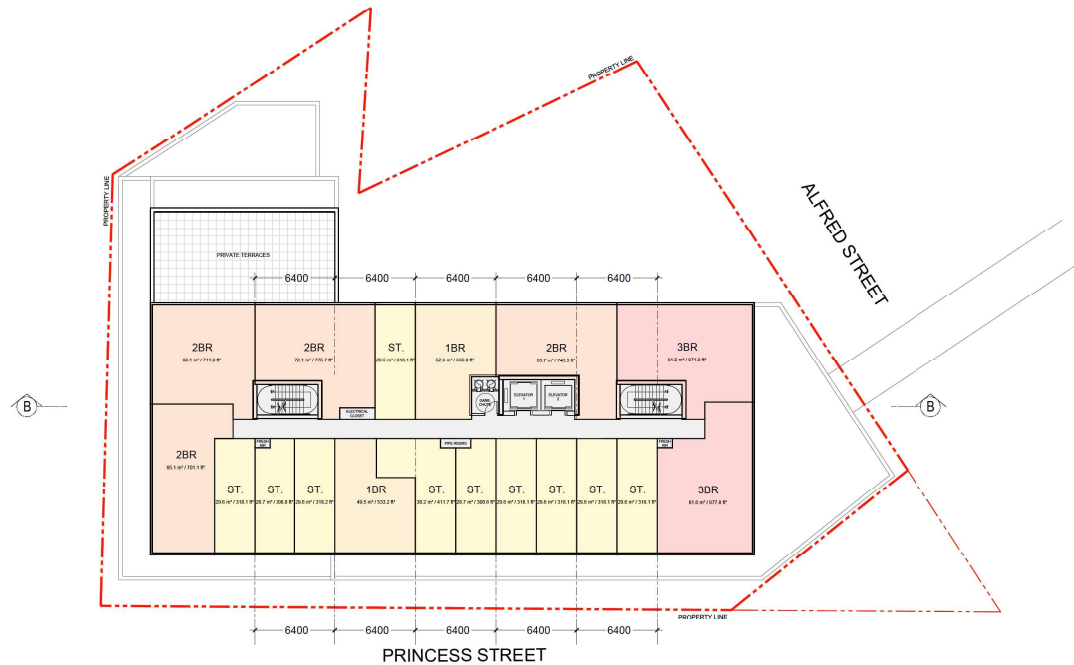
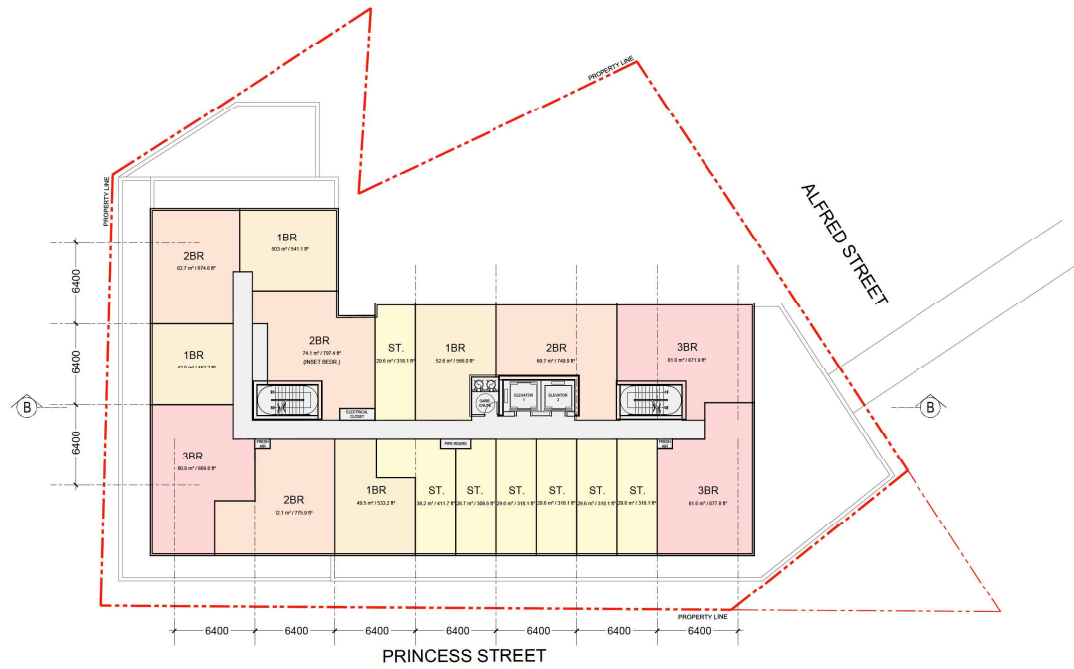
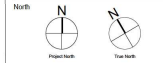






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No.	Description	Date

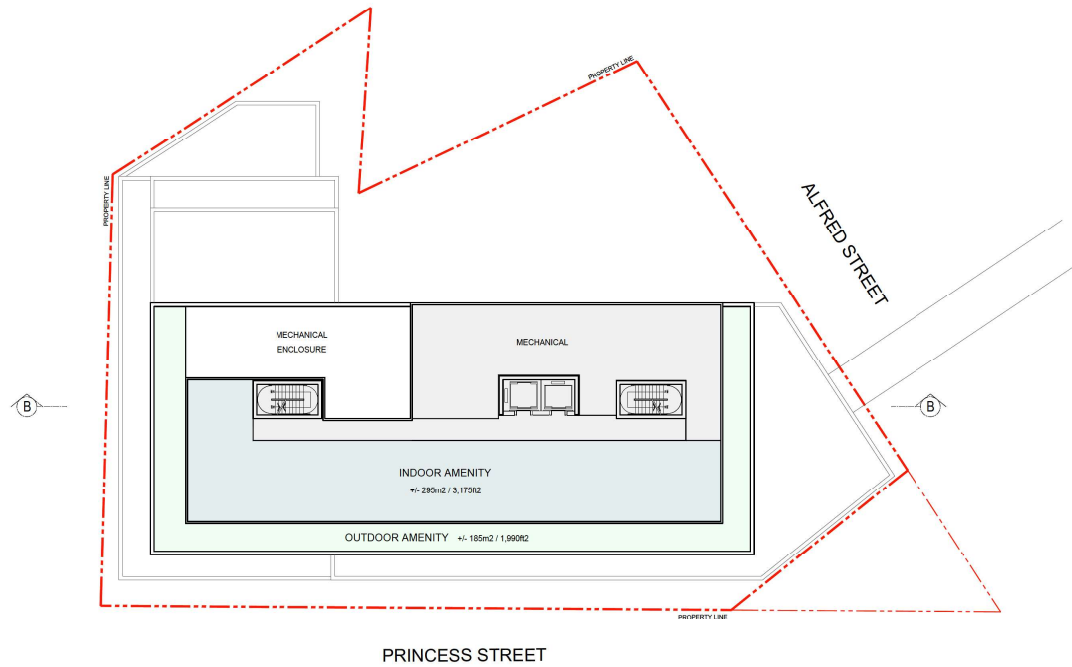
Architect of Record  
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Project Name:  
555 PRINCESS STREET  
KINGSTON, ON

6th to 8th FLOOR  
9th FLOOR

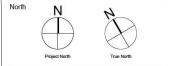
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Scale: 1/50  
Date: 15 DECEMBER 03, 2025  
Drawn by: HPA

Drawing No.: A2.04  
Revision:



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Project File:  
 555 PRINCESS STREET  
 KINGSTON, ON

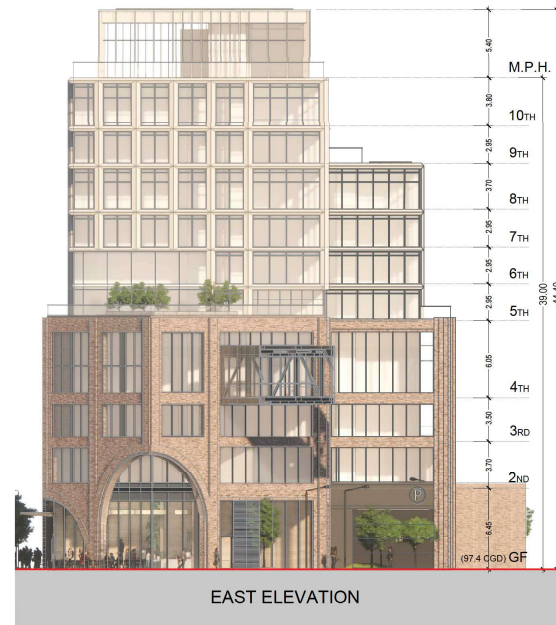
10th FLOOR  
 M.P.H. LEVEL

Project Number: 2025  
 Scale: 1/50  
 Date: 15 DECEMBER 03, 2025  
 Drawn by: HPA

Drawing No.:  
 Revision:

A2.05





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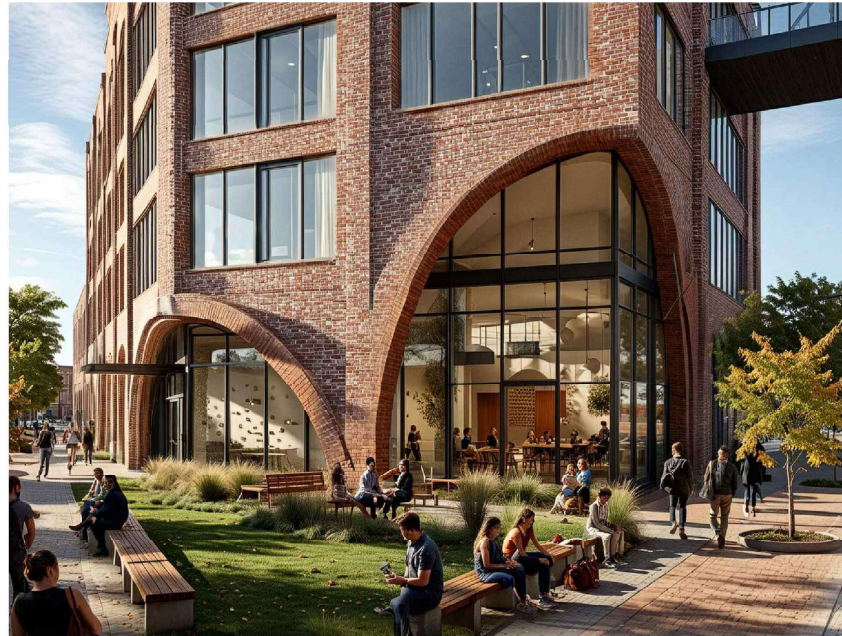
Project File:  
555 PRINCESS STREET  
KINGSTON, ON

EAST ELEVATION  
SOUTH ELEVATION

Project Number: 2025  
Scale: 1/32  
Date: 15 DECEMBER 03, 2025  
Drawn by: HPA  
Checked by: HPA

Drawing No.: A4.01  
Revision:





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555 PRINCESS STREET  
 KINGSTON, OH

RENDERINGS

Project Number: 2025  
 Scale: 1/8" = 1'-0"  
 Date: DECEMBER 03, 2025  
 Drawn By: BPA  
 Drawing No.: A5.01



# 525-555 Princess Street - Kingston

Shadow Study  
September 22<sup>nd</sup>, 2025

HARIRI PONTARINI  
ARCHITECTS

2 | 525-555 Princess Street, Kingston

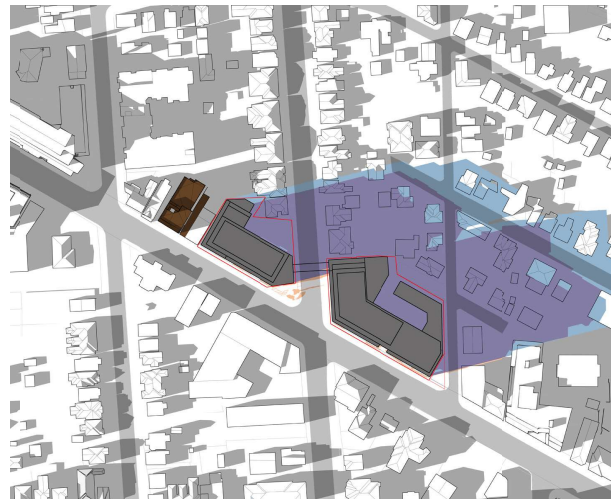
# Shadow Study

March 21<sup>st</sup>

- As-Of-Right Shadow Only
- Proposed Development Shadow Only
- Both Proposed Development and As-Of-Right Shadow



08:41 AM - 1.5 hours after Sunrise



05:45 PM - 1.5 hours before Sunset

3 | 525-555 Princess Sreet, Kingston

# Shadow Study March 21<sup>st</sup>



08:00 AM



09:00 AM



10:00 AM



11:00 AM



12:00 PM

4 | 525-555 Princess Sreet, Kingston

# Shadow Study March 21<sup>st</sup>



01:00 PM



02:00 PM



03:00 PM



04:00 PM



05:00 PM

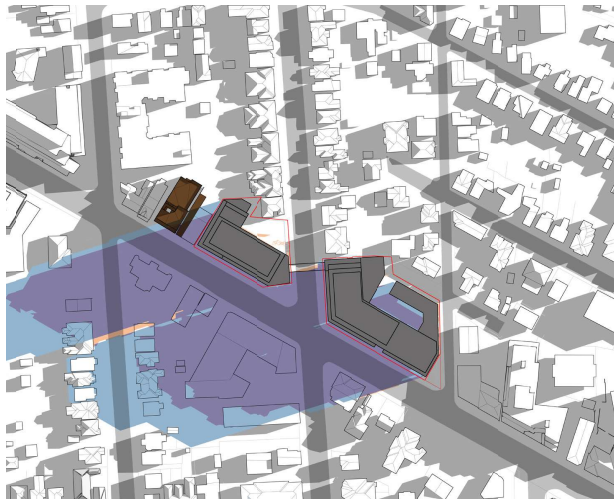
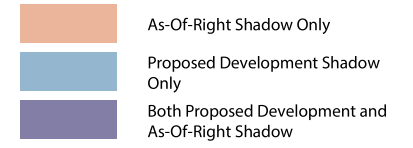


06:00 PM

5 | 525-555 Princess Sreet, Kingston

# Shadow Study

## June 21<sup>st</sup>



06:58 AM - 1.5 hours after Sunrise



07:17 PM - 1.5 hours before Sunset



08:00 AM

6 | 525-555 Princess Sreet, Kingston

# Shadow Study

## June 21<sup>st</sup>



09:00 AM



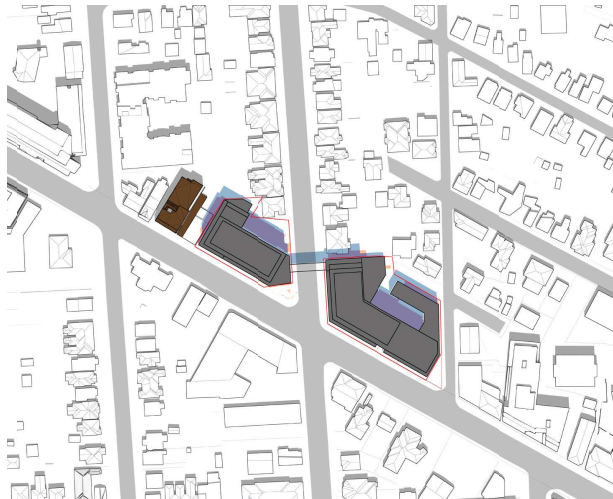
10:00 AM



11:00 AM



12:00 PM



01:00 PM



02:00 PM

7 | 525-555 Princess Sreet, Kingston

# Shadow Study

## June 21<sup>st</sup>



03:00 PM



04:00 PM



05:00 PM



06:00 PM



07:00 PM



08:00 PM

8 | 525-555 Princess Street, Kingston

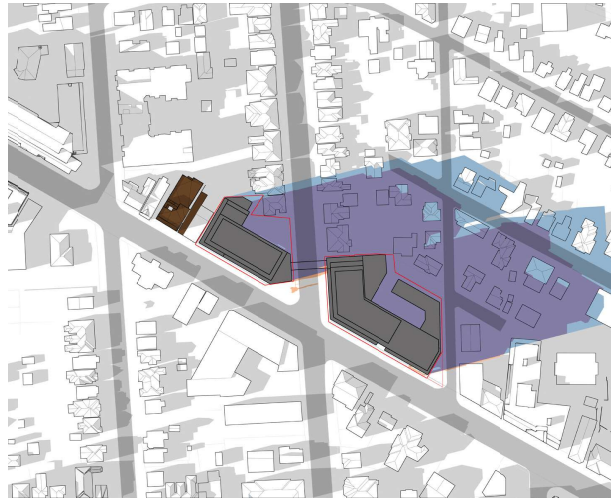
# Shadow Study

September 21<sup>st</sup>

- As-Of-Right Shadow Only
- Proposed Development Shadow Only
- Both Proposed Development and As-Of-Right Shadow

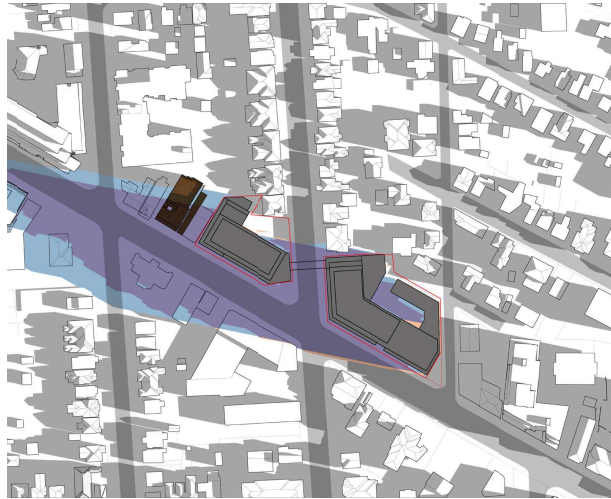


08:26 AM - 1.5 hours after Sunrise



05:30 PM - 1.5 hours before Sunset

# Shadow Study September 21<sup>st</sup>



08:00 AM



09:00 AM



10:00 AM



11:00 AM



12:00 PM

10 | 525-555 Princess Sreet, Kingston

# Shadow Study September 21<sup>st</sup>



01:00 PM



02:00 PM



03:00 PM



04:00 PM



05:00 PM

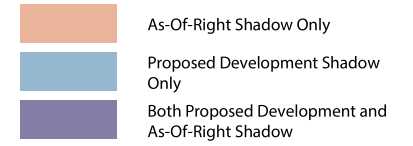


06:00 PM

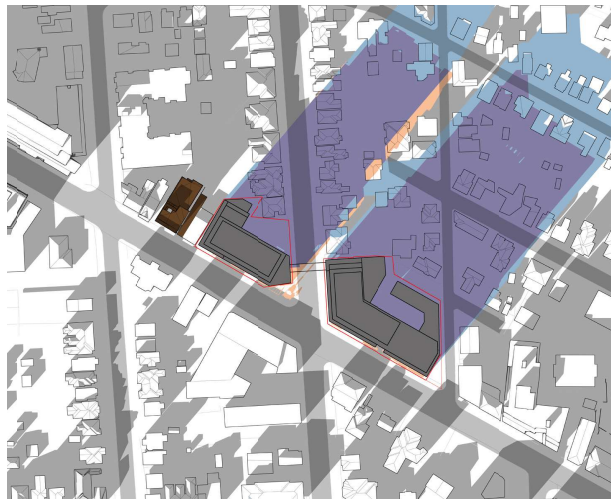
11 | 525-555 Princess Street, Kingston

# Shadow Study

## December 21<sup>st</sup>



09:14 AM - 1.5 hours after Sunrise



02:54 PM - 1.5 hours before Sunset



09:00 AM

# Shadow Study December 21<sup>st</sup>



10:00 AM



11:00 AM



12:00 PM



01:00 PM

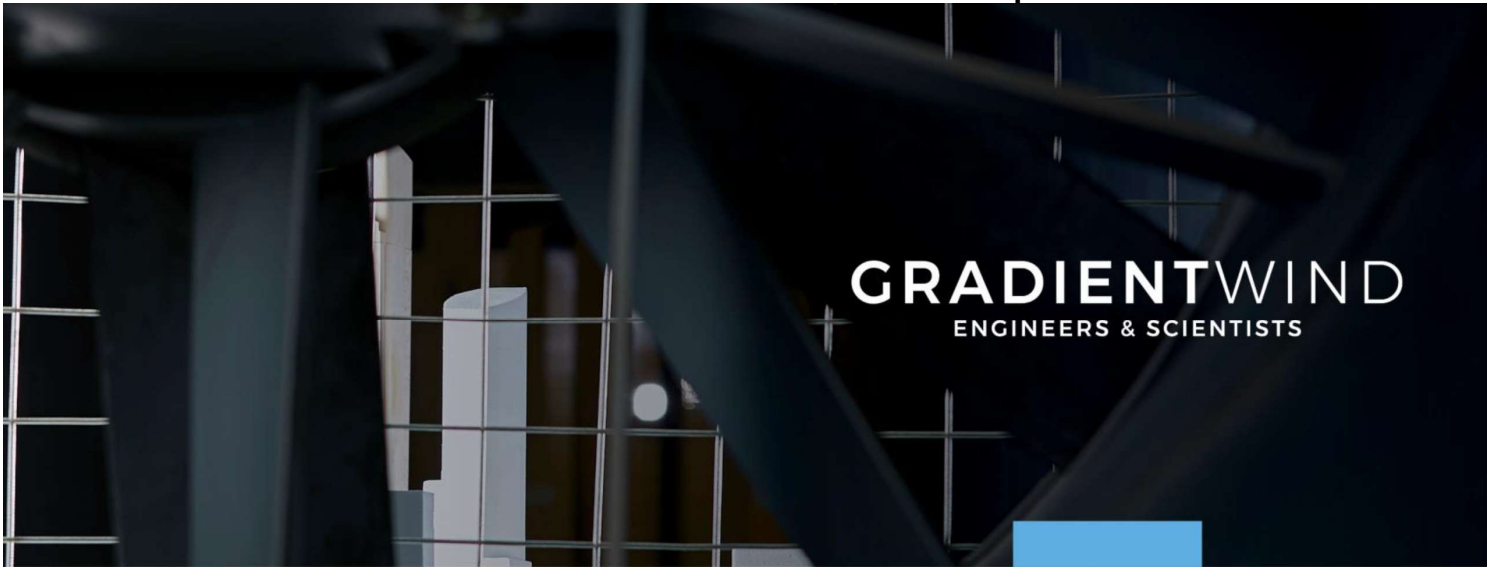


02:00 PM



03:00 PM





**PEDESTRIAN LEVEL  
WIND STUDY**

525 Princess Street  
Kingston, Ontario

Report: 25-142-PLW



September 29, 2025

PREPARED FOR  
555 Princess Inc.  
2 St. Clair Avenue West, Suite 2100  
Toronto, ON M4V 1L5

PREPARED BY  
Justin Denne, M.A.Sc., Junior Wind Scientist  
David Huitema, M.Eng., P.Eng., CFD Lead Engineer





## EXECUTIVE SUMMARY

This report describes a pedestrian level wind (PLW) study to satisfy Zoning By-Law Amendment application submission requirements for the proposed mixed-use residential development located at 525 in Kingston, Ontario (hereinafter referred to as the “subject site” or “proposed development”). Our mandate within this study is to investigate pedestrian wind conditions within and surrounding the subject site, and to identify areas where wind conditions may interfere with certain pedestrian activities so that mitigation measures may be considered, where required. The analysis herein was prepared in conjunction with the PLW study for 555 Princess Street, a concurrent application.

The study involves simulation of wind speeds for sixteen (16) wind directions in a three-dimensional (3D) computer model using the computational fluid dynamics (CFD) technique, combined with meteorological data integration, to assess pedestrian wind comfort and safety within and surrounding the subject site according to industry standard wind comfort and safety guidelines. A complete summary of the predicted wind conditions is provided in Section 5 and illustrated in Figures 3A-9, and is summarized as follows:

- 1) Most grade-level areas within and surrounding the subject site are predicted to experience conditions that are considered acceptable for the intended pedestrian uses throughout the year. Specifically, conditions over the nearby public sidewalks, transit stops, neighbouring existing surface parking lots, proposed outdoor amenities, and in the vicinity of the building access points serving the subject site, are considered acceptable.
  - a. During the typical use period (that is, May to October, inclusive), wind comfort conditions within the proposed parkland dedication at the southeast corner of 525 Princess Street are predicted to be suitable for a mix of mostly sitting and standing. If the noted windier regions within the parkland dedication will accommodate sedentary activities, sitting conditions may be extended by implementing targeted mitigation around designated seating areas such as wind screens, dense arrangements of plantings in tall planters, high-back bench seating, and other common landscape elements.
- 2) Regarding the MPH Level common amenity terrace serving the proposed development, conditions during the typical use period are predicted to be suitable for a mix of sitting and standing.



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- a. If required by programming, mitigation may include 1.8-m-tall wind screens around the full perimeter of the terrace in combination with inboard mitigation targeted around designated seating areas, such as free-standing canopies or trellises with vertical components, wind screens, dense arrangements of plantings in tall planters, and other common landscape elements.
  - b. The extent of the mitigation measures is dependent on the programming of the terrace, and an appropriate mitigation strategy may be developed in collaboration with the building and landscape architects as the design of the proposed development progresses towards the future Site Plan Control application process.
- 3) The foregoing statements and conclusions apply to common weather systems, during which no dangerous wind conditions, as defined in Section 4.4, are expected anywhere over the subject site. During extreme weather events, (for example, thunderstorms, tornadoes, and downbursts), pedestrian safety is the main concern. However, these events are generally short-lived and infrequent and there is often sufficient warning for pedestrians to take appropriate cover.



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Appendix A – Simulation of the Atmospheric Boundary Layer



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## 1. INTRODUCTION

Gradient Wind Engineering Inc. (Gradient Wind) was retained by 555 Princess Inc. to undertake a pedestrian level wind (PLW) study to satisfy Zoning By-Law Amendment (ZBLA) application submission requirements for the proposed mixed-use residential development located at 525 Princess Street in Kingston, Ontario (hereinafter referred to as the “subject site” or “proposed development”). Our mandate within the current study is to investigate pedestrian wind conditions within and surrounding the subject site, and to identify areas where wind conditions may interfere with certain pedestrian activities so that mitigation measures may be considered, where required. The analysis herein was prepared in conjunction with the PLW study to satisfy ZBLA application submission requirements for the proposed development at 555 Princess Street<sup>1</sup>, as these applications are concurrent and their construction would be similarly completed concurrently.

The study is based on industry standard computer simulations using the computational fluid dynamics (CFD) technique and data analysis procedures, industry standard wind comfort and safety guidelines, architectural drawings provided by Hariri Pontarini Architects, surrounding street layouts and existing and approved future building massing information obtained from the City of Kingston, and recent site imagery. Since the City of Kingston does not specify wind guidelines, industry standard wind comfort and safety criteria were applied, as described in Section 4.4.

## 2. TERMS OF REFERENCE

The subject site comprises two parcels of land located at 525 and 555 Princess Street in Kingston, situated at the northeast and northwest corners, respectively, at the intersection Princess Street and Alfred Street. The proposed developments each comprise a 10-storey mixed-use residential building topped with a mechanical penthouse (MPH).

525 Princess Street is bordered by Creighton Street and low-rise residential dwellings to the north, Chatham Street and low-rise massing followed by a 10-storey building to the east, Princess Street to the south, and Alfred Street to the west. The ground floor of the proposed development comprises townhouses to the north, commercial units to the southeast and west, a residential lobby to the

---

<sup>1</sup> Gradient Wind Engineering Inc., ‘555 Princess Street – Pedestrian Level Wind Study’, [Sept 15, 2025]



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southwest, a parking ramp and loading area to the northwest, and a central indoor amenity. A central outdoor amenity is proposed within a courtyard, delineating the townhouses from the remainder of the ground floor to the south, and a parkland dedication is proposed at the southeast corner of the subject site. Levels 2-3 and 5-10 are programmed for residential occupancy. The building steps back from the north elevation at Level 2, the north, east, and south elevations at Level 5, the southwest elevations at Level 7, and the north elevation at Level 9, each accommodating private terraces. At Level 4, a pedestrian bridge extends over Alfred Street from an indoor amenity to the west, connecting with the proposed development at 555 Princess Street. A common amenity terrace is programmed at the MPH Level, accessible via a centrally located indoor amenity.

Regarding wind exposures, the near-field surroundings of the development (defined as an area falling within a 200-metre (m) radius of the subject site) are characterized by a mix of mostly low-rise residential and commercial buildings in all directions with isolated mid-rise buildings along Princess Street. Victoria Park is located 130 m to the south. The far-field surroundings (defined as the area beyond the near field and within a 2-kilometre (km) radius) comprise mostly low-rise massing in all compass directions with clusters of mid- and high-rise buildings to the east-southeast and south. Notably, clusters of mid- and high-rise buildings comprising the Kingston downtown core is located to the east of the subject site and a cluster of mid-rise buildings comprising Queen's University is located to the south-southeast. The inlet to the St. Lawrence River and Lake Ontario is located approximately 1.5 km to the south, and the Great Cataraqui River flows into the St. Lawrence River from the northeast to the east-southeast, approximately 1.5 km to the east.

A site plan for the proposed massing scenario is illustrated in Figure 1A, while the existing scenario is illustrated in Figure 1B. Figures 2A-2H illustrate the computational models used to conduct the study.

### **3. OBJECTIVES**

The principal objectives of this study are to (i) determine pedestrian level wind conditions at key areas within and surrounding the subject site; (ii) identify areas where wind conditions may interfere with the intended uses of outdoor spaces; and (iii) recommend suitable mitigation measures, where required.



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## 4. METHODOLOGY

The approach followed to quantify wind conditions over the site is based on CFD simulations of wind speeds across the subject site within a virtual environment, meteorological analysis of the Kingston area wind climate, and synthesis of computational data with the noted wind guidelines. The following sections describe the analysis procedures, including a discussion of the noted pedestrian wind guidelines.

### 4.1 Computer-Based Context Modelling

A computer based PLW study was performed to determine the influence of the wind environment on pedestrian comfort over the proposed development site. Pedestrian comfort predictions, based on the mechanical effects of wind, were determined by combining measured wind speed data from CFD simulations with statistical weather data obtained from Kingston Norman Rogers Airport in Kingston, Ontario. The general concept and approach to CFD modelling is to represent building and topographic details in the immediate vicinity of the subject site on the surrounding model, and to create suitable atmospheric wind profiles at the model boundary. The wind profiles are designed to have similar mean and turbulent wind properties consistent with actual site exposures.

An industry standard practice is to omit trees, vegetation, and other existing and proposed landscape elements from the model due to the difficulty of providing accurate seasonal representation of vegetation. The omission of trees and other landscaping elements produces stronger wind speed values.

### 4.2 Wind Speed Measurements

The PLW analysis was performed by simulating wind flows and gathering velocity data over a CFD model of the subject site for 16 wind directions. The CFD simulation model was centered on the proposed development, complete with surrounding massing within a radius of approximately 515 m. The process was performed for two context massing scenarios, as noted in Section 2.



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Mean and peak wind speed data obtained over the subject site for each wind direction were interpolated to 36 wind directions at 10° intervals, representing the full compass azimuth. Measured wind speeds approximately 1.5 m above local grade and the common amenity terraces serving the proposed development were referenced to the wind speed at gradient height to generate mean and peak velocity ratios, which were used to calculate full-scale values. Gradient height represents the theoretical depth of the boundary layer of the earth's atmosphere, above which the mean wind speed remains constant. Further details of the wind flow simulation technique are presented in Appendix A.

### 4.3 Historical Wind Speed and Direction Data

A statistical model for winds in Kingston was developed from approximately 50 years of hourly meteorological wind data recorded at Kingston Norman Rogers Airport and obtained from Environment and Climate Change Canada. Wind speed and direction data were analyzed during each month of the year to determine statistically prominent wind directions and the corresponding speeds, and to characterize similarities between monthly weather patterns.

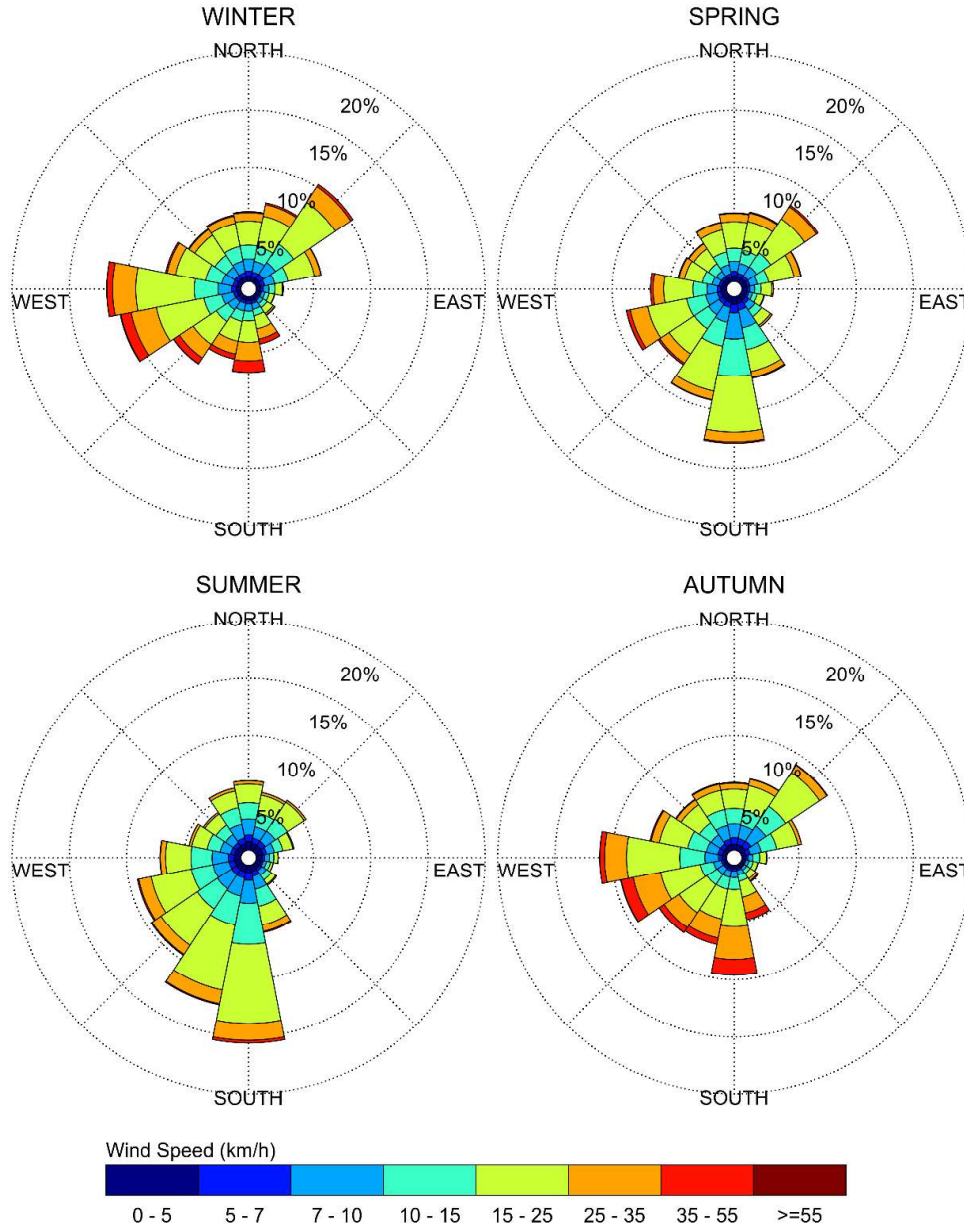
The statistical model of the Kingston area wind climate, which indicates the directional character of local winds on a seasonal basis, is illustrated on the following page. The plots illustrate seasonal distribution of measured wind speeds and directions in kilometers per hour (km/h). Probabilities of occurrence of different wind speeds are represented as stacked polar bars in sixteen azimuth divisions. The radial direction represents the percentage of time for various wind speed ranges per wind direction during the measurement period. The preferred wind speeds and directions can be identified by the longer length of the bars. For the Kingston area, the most common winds occur for southerly and westerly wind directions, followed by those from the southwest and northeast, while the most common wind speeds are below 36 km/h. The directional preference and relative magnitude of wind speed changes somewhat from season to season.



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## SEASONAL DISTRIBUTION OF WIND KINGSTON NORMAN ROGERS AIRPORT, KINGSTON, ONTARIO



**Notes:**

1. Radial distances indicate percentage of time of wind events.
2. Wind speeds are mean hourly in km/h, measured at 10 m above the ground.

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**4.4 Pedestrian Wind Comfort and Safety Guidelines**

The pedestrian wind comfort and safety guidelines are based on the mechanical effects of wind without consideration of other meteorological conditions (that is, temperature and relative humidity). The comfort guidelines assume that pedestrians are appropriately dressed for a specified outdoor activity during any given season. Since both mean and gust wind speeds affect pedestrian comfort, their combined effect is considered. Specifically, the guidelines are defined as a Gust Equivalent Mean (GEM) wind speed, which is the greater of the mean wind speed or the gust wind speed divided by 1.85. The wind speed ranges are based on the Beaufort scale, which describes the effects of forces produced by varying wind speed levels on objects. Four pedestrian comfort classes and corresponding gust wind speed ranges are used to assess pedestrian comfort: (1) Sitting; (2) Standing; (3) Walking; and (4) Uncomfortable. Wind conditions suitable for sitting are represented by the colour blue, standing by green, and walking by yellow; uncomfortable conditions are represented by the colour orange. Specifically, the comfort classes, associated wind speed ranges, and limiting guidelines are summarized as follows:

**PEDESTRIAN WIND COMFORT CLASS DEFINITIONS**

Wind Comfort Class	GEM Speed (km/h)	Description
SITTING	≤ 10	GEM wind speeds no greater than 10 km/h occurring at least 80% of the time are considered acceptable for sedentary activities, including sitting.
STANDING	≤ 15	GEM wind speeds no greater than 15 km/h occurring at least 80% of the time are considered acceptable for activities such as standing, strolling, or more vigorous activities.
WALKING	≤ 20	GEM wind speeds no greater than 20 km/h occurring at least 80% of the time are considered acceptable for walking or more vigorous activities.
UNCOMFORTABLE	> 20	Uncomfortable conditions are characterized by predicted values that fall below the 80% target for walking. Brisk walking and exercise, such as jogging, are considered acceptable for moderate excesses of this guideline.

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Regarding wind safety, gust wind speeds greater than 90 km/h, occurring more than 0.1% of the time on an annual basis (based on wind events recorded for 24 hours a day), are classified as dangerous. From calculations of stability, it can be shown that gust wind speeds of 90 km/h would be the approximate threshold wind speed that would cause an average elderly person in good health to fall.

Experience and research on people’s perception of mechanical wind effects has shown that if the wind speed levels are exceeded for more than 20% of the time, the activity level would be judged to be uncomfortable by most people. For instance, if GEM wind speeds of 10 km/h were exceeded for more than 20% of the time most pedestrians would judge that location to be too windy for sitting. Similarly, if GEM wind speeds of 20 km/h at a location were exceeded for more than 20% of the time, walking or less vigorous activities would be considered uncomfortable. As these guidelines are based on subjective reactions of a population to wind forces, their application is partly based on experience and judgment.

Once the pedestrian wind speed predictions have been established throughout the subject site, the assessment of pedestrian comfort involves determining the suitability of the predicted wind conditions for discrete regions within and surrounding the subject site. This step involves comparing the predicted comfort classes to the target comfort classes, which are dictated by the location type for each region (that is, a sidewalk, building entrance, amenity space, or other). An overview of common pedestrian location types and their typical windiest target comfort classes are summarized below. Depending on the programming of a space, the target comfort class may differ from this table.

**TARGET PEDESTRIAN WIND COMFORT CLASSES FOR VARIOUS LOCATION TYPES**

Location Types	Target Comfort Classes
Primary Building Entrance	Standing
Secondary Building Access Point	Walking
Public Sidewalk / Bicycle Path	Walking
Outdoor Amenity Space	Sitting / Standing
Café / Patio / Bench / Garden	Sitting / Standing
Transit Stop (Without Shelter)	Standing
Transit Stop (With Shelter)	Walking
Public Park / Plaza	Sitting / Standing
Parking Lot	Walking



## 5. RESULTS AND DISCUSSION

The following discussion of the predicted pedestrian wind conditions for the subject site is accompanied by Figures 3A-6B, which illustrate wind conditions at grade level for the proposed and existing massing scenarios, and by Figures 8A-8D, which illustrate wind conditions over the common amenity terrace serving the proposed development. Conditions are presented as continuous contours of wind comfort within and surrounding the subject site and correspond to the various comfort classes noted in Section 4.4.

Wind comfort conditions are also reported for the typical use period, which is defined as May to October, inclusive. Figures 7 and 9 illustrate wind comfort conditions at grade level and within the noted common amenity terrace serving the proposed development, respectively, consistent with the comfort classes in Section 4.4.

The details of these conditions are summarized in the following pages for each area of interest.

### 5.1 Wind Comfort Conditions – Grade Level

**Sidewalks along Princess Street, Alfred Street, Chatham Street, and Creighton Street:** Following the introduction of the proposed development, wind comfort conditions over the nearby public sidewalks along Princess Street, Alfred Street, Chatham Street, and Creighton Street are predicted to be suitable for mostly standing, or better, throughout the year, with isolated regions suitable for walking between 525 and 555 Princess Street and around the southeast corner of 525 Princess Street. The noted conditions are considered acceptable.

Conditions over the sidewalks along Princess Street, Alfred Street, Chatham Street, and Creighton Street with the existing massing are predicted to be suitable for standing, or better, throughout the year. While the introduction of the proposed development is predicted to produce windier conditions over nearby sidewalks in comparison to existing conditions, wind comfort conditions with the proposed development are nevertheless considered acceptable.

**Transit Stops along Princess Street:** Prior to and following the introduction of the proposed development, wind conditions in the vicinity of the nearby transit stops along Princess Street are predicted to be suitable for standing, or better, throughout the year. The noted conditions are considered acceptable.



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**Neighbouring Existing Surface Parking Lots and Restaurant Patios:** Prior to and following the introduction of the proposed development, wind comfort conditions over nearby existing surface parking lots are predicted to be suitable standing, or better, throughout the year. The noted conditions are considered acceptable.

During the typical use period, wind comfort conditions within the existing restaurant/bar patio to the south of the proposed development at 506 Princess Street are predicted to be suitable for sitting prior to and following the introduction of the proposed development. The noted conditions are considered acceptable.

**Outdoor Amenities and Parkland Dedication:** During the typical use period, wind comfort conditions within the proposed outdoor amenity serving the courtyard of 525 Princess Street and are predicted to be suitable for sitting and are considered acceptable.

During the same period, wind comfort conditions within the proposed parkland dedication at the southeast corner of 525 Princess Street are predicted to be suitable for a mix of mostly sitting and standing, with an isolated region of walking conditions. If the parkland dedication will not be programmed for seating activities, the noted conditions may be considered acceptable. If required by programming, sitting conditions may be extended by implementing targeted mitigation around designated seating areas such as wind screens, dense arrangements of plantings within tall planters, high-back bench seating, and other common landscape elements.

**Building Access Points:** Wind comfort conditions in the vicinity of the primary and secondary building access points serving the proposed development are predicted to be suitable for standing, or better, throughout the year and are considered acceptable.



## 5.2 Wind Comfort Conditions – MPH Level Common Amenity Terrace

During the typical use period (that is, May to October, inclusive), wind comfort conditions within the MPH Level common amenity terrace are predicted to be suitable for a mix of sitting and standing, where sitting conditions are primarily predicted along the façades of the MPH.

If required by programming, sitting conditions may be extended over the full terrace area by implementing 1.8-m-tall wind screens around the full perimeter of the terrace in combination with inboard mitigation targeted around designed seating areas. Such mitigation may take the form of free-standing canopies or trellises with vertical components, wind screens, dense arrangements of plantings in tall planters, and other common landscape elements, and the extent of the mitigation is dependent on the programming of the terrace.

## 5.3 Wind Safety

Within the context of typical weather patterns, which exclude anomalous localized storm events such as tornadoes and downbursts, no pedestrian areas within or surrounding the subject site are expected to experience conditions that could be considered dangerous, as defined in Section 4.4.

## 5.4 Applicability of Results

Pedestrian wind comfort and safety have been quantified for the specific configuration of existing and foreseeable construction around the subject site. Future changes (that is, construction or demolition) of these surroundings may cause changes to the wind effects in two ways, namely: (i) changes beyond the immediate vicinity of the subject site would alter the wind profile approaching the subject site; and (ii) development in proximity to the subject site would cause changes to local flow patterns.





## 6. SUMMARY AND RECOMMENDATIONS

A complete summary of the predicted wind conditions is provided in Section 5 of this report and illustrated in Figures 3A-9. Based on computer simulations using the CFD technique, meteorological data analysis, and experience with numerous similar developments, the study concludes the following:

- 1) Most grade-level areas within and surrounding the subject site are predicted to experience conditions that are considered acceptable for the intended pedestrian uses throughout the year. Specifically, conditions over the nearby public sidewalks, transit stops, neighbouring existing surface parking lots, proposed outdoor amenities, , and in the vicinity of the building access points serving the subject site, are considered acceptable.
  - a. During the typical use period (that is, May to October, inclusive), wind comfort conditions within the proposed parkland dedication at the southeast corner of 525 Princess Street are predicted to be suitable for a mix of mostly sitting and standing. If the noted windier regions within the parkland dedication will accommodate sedentary activities, sitting conditions may be extended by implementing targeted mitigation around designated seating areas such as wind screens, dense arrangements of plantings in tall planters, high-back bench seating, and other common landscape elements.
- 2) Regarding the MPH Level common amenity terrace serving the proposed development, conditions during the typical use period are predicted to be suitable for a mix of sitting and standing.
  - a. If required by programming, mitigation may include 1.8-m-tall wind screens around the full perimeter of the terrace in combination with inboard mitigation targeted around designated seating areas, such as free-standing canopies or trellises with vertical components, wind screens, dense arrangements of plantings in tall planters, and other common landscape elements.
  - b. The extent of the mitigation measures is dependent on the programming of the terrace, and an appropriate mitigation strategy may be developed in collaboration with the building and landscape architects as the design of the proposed development progresses towards the future Site Plan Control application process.



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- 3) The foregoing statements and conclusions apply to common weather systems, during which no dangerous wind conditions, as defined in Section 4.4, are expected anywhere over the subject site. During extreme weather events, (for example, thunderstorms, tornadoes, and downbursts), pedestrian safety is the main concern. However, these events are generally short-lived and infrequent and there is often sufficient warning for pedestrians to take appropriate cover.

Sincerely,

***Gradient Wind Engineering Inc.***

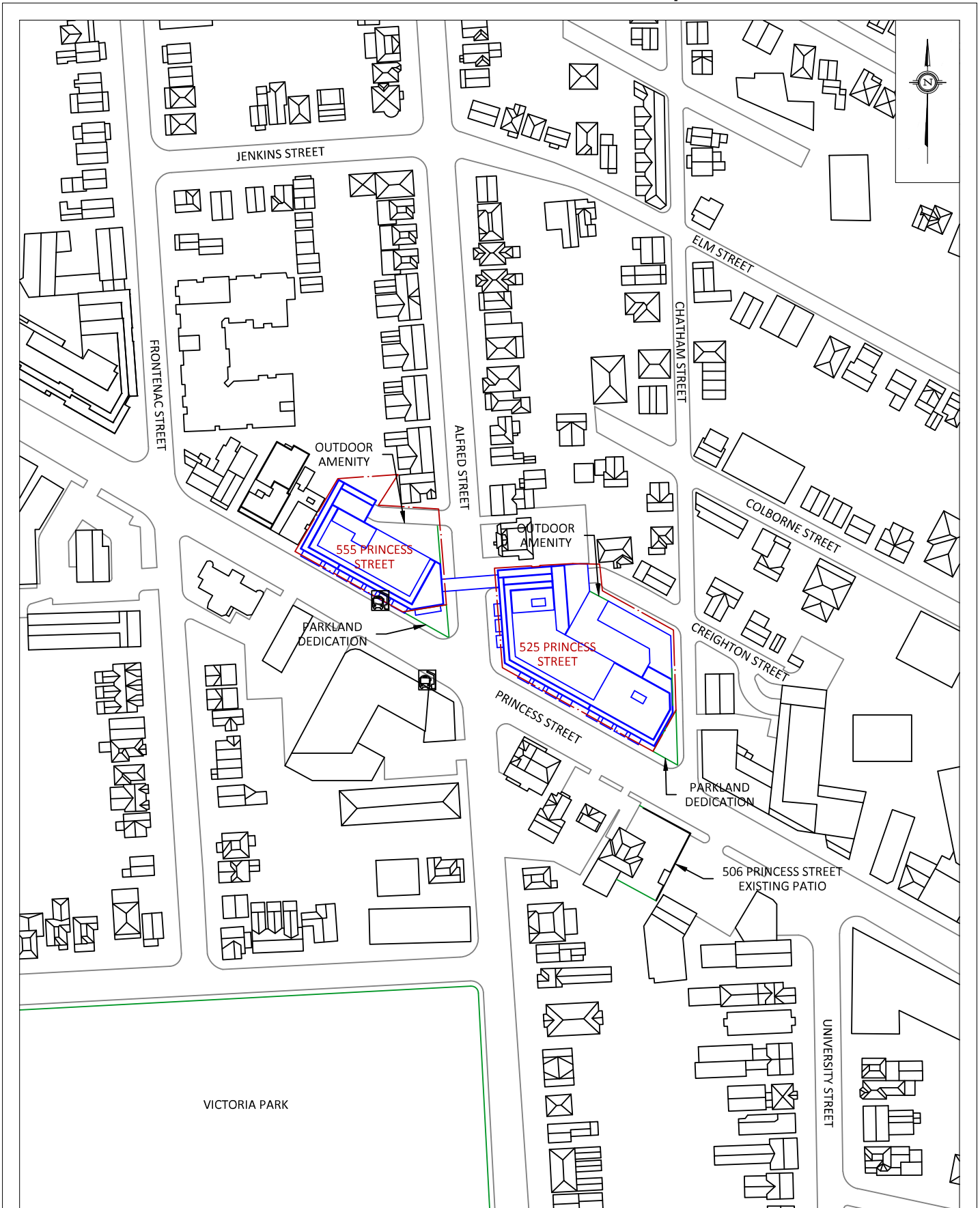


Justin Denne, M.A.Sc.  
Junior Wind Scientist

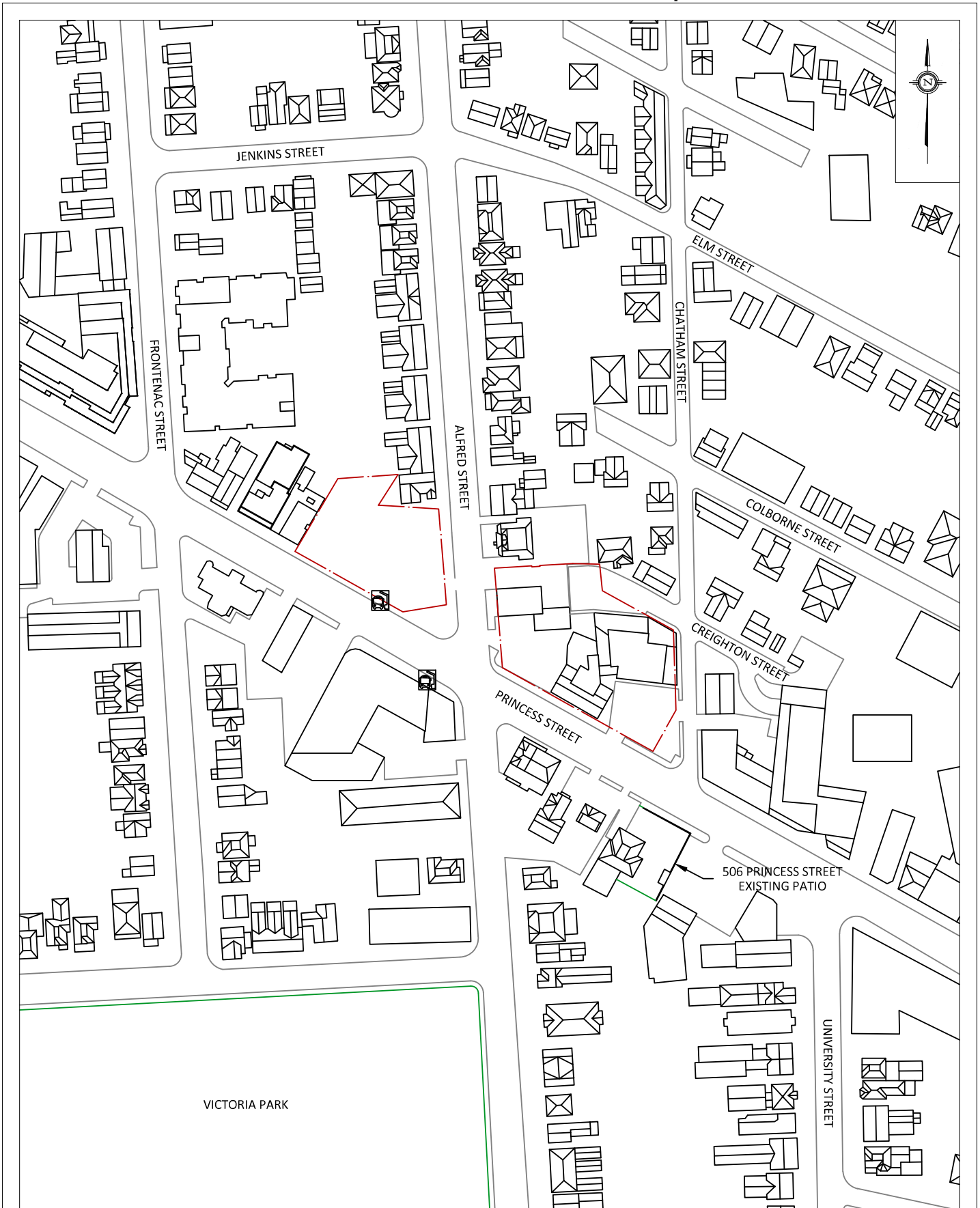


David Huitema, M.Eng., P.Eng.  
CFD Lead Engineer





<b>GRADIENTWIND</b> ENGINEERS & SCIENTISTS 127 WALGREEN ROAD, OTTAWA, ON 613 836 0934 • GRADIENTWIND.COM	PROJECT	525 PRINCESS STREET, KINGSTON PEDESTRIAN LEVEL WIND STUDY		DESCRIPTION	FIGURE 1A: PROPOSED SITE PLAN AND SURROUNDING CONTEXT
	SCALE	1:2000	DRAWING NO.	25-142-PLW-1A	
	DATE	SEPTEMBER 12, 2025	DRAWN BY	S.K.	

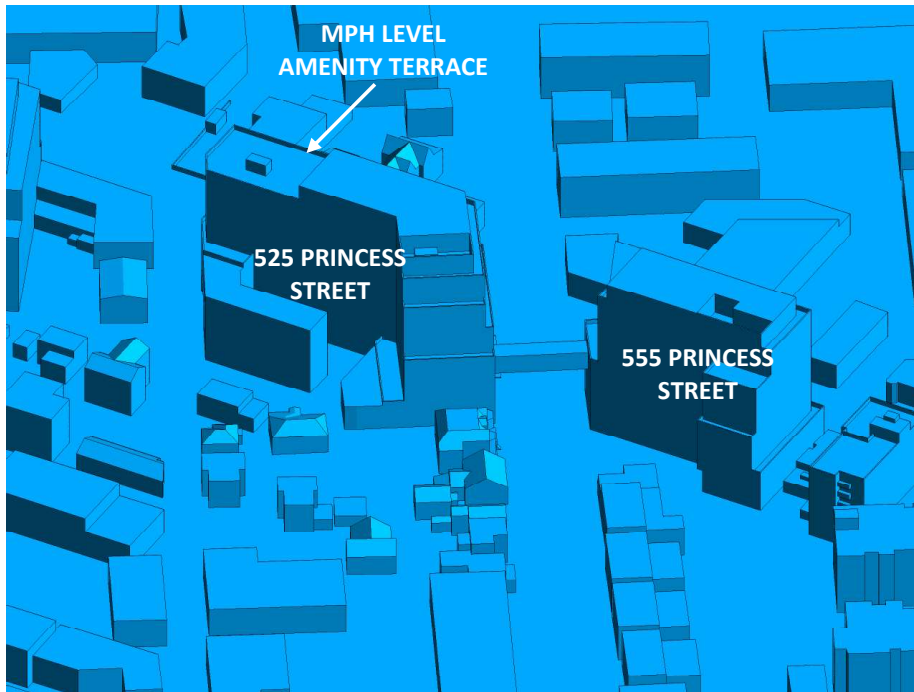


<p><b>GRADIENTWIND</b> ENGINEERS &amp; SCIENTISTS 127 WALGREEN ROAD, OTTAWA, ON 613 836 0934 • GRADIENTWIND.COM</p>	PROJECT	525 PRINCESS STREET, KINGSTON PEDESTRIAN LEVEL WIND STUDY		DESCRIPTION	<p>FIGURE 1B: EXISTING SITE PLAN AND SURROUNDING CONTEXT</p>
	SCALE	1:2000	DRAWING NO.	25-142-PLW-1B	
	DATE	SEPTEMBER 12, 2025	DRAWN BY	S.K.	

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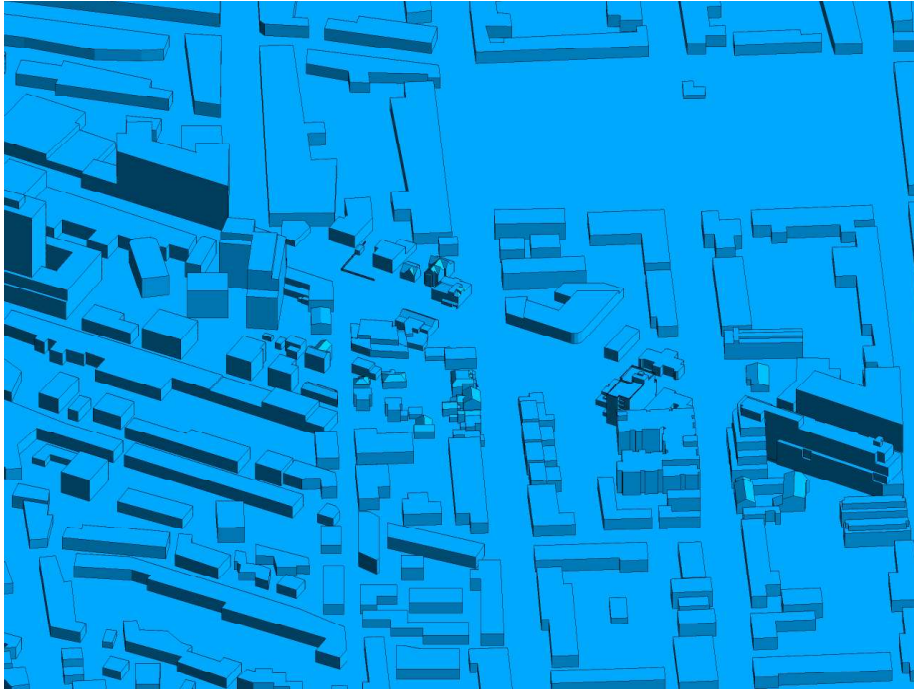
**FIGURE 2A: COMPUTATIONAL MODEL, PROPOSED MASSING, NORTH PERSPECTIVE**



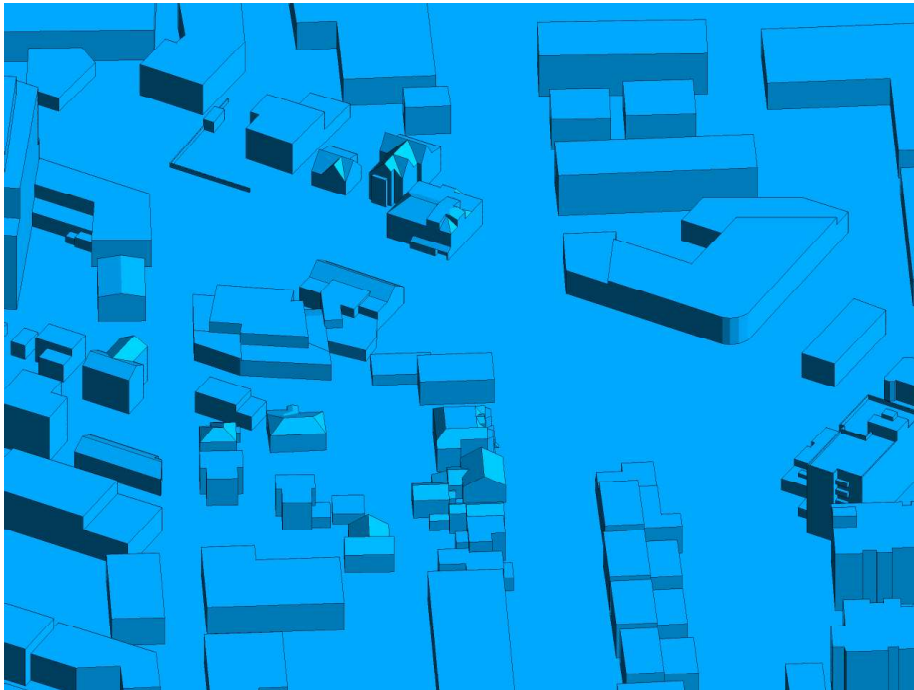
**FIGURE 2B: CLOSE-UP VIEW OF FIGURE 2A**



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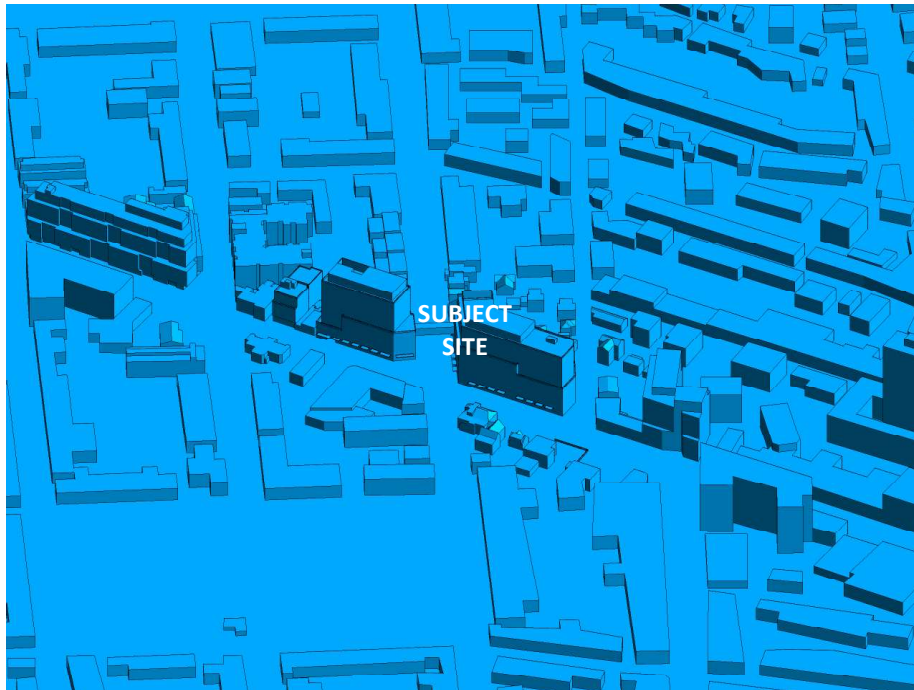
**FIGURE 2C: COMPUTATIONAL MODEL, EXISTING MASSING, NORTH PERSPECTIVE**



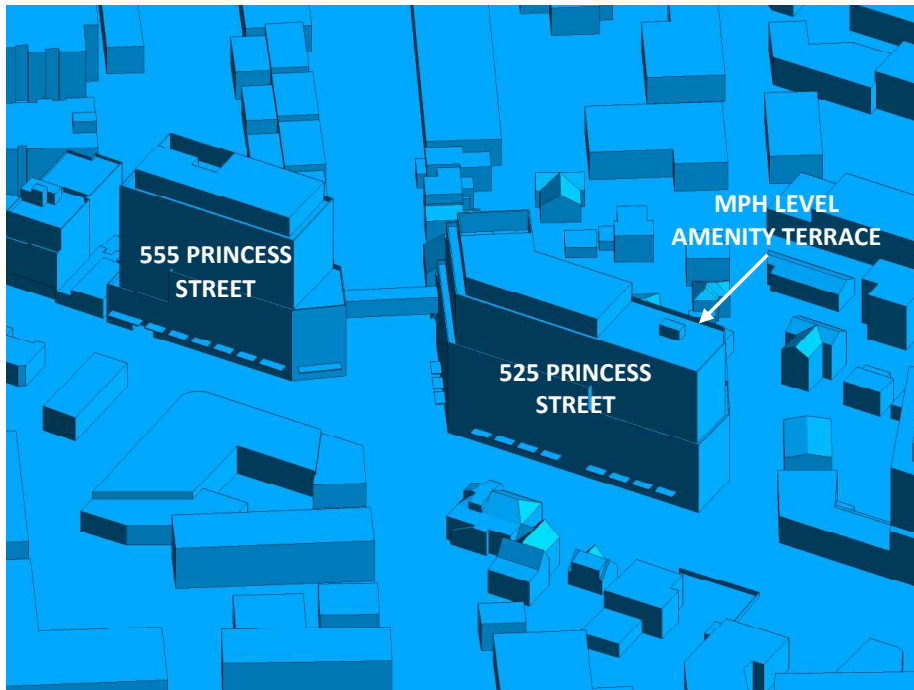
**FIGURE 2D: CLOSE-UP VIEW OF FIGURE 2C**



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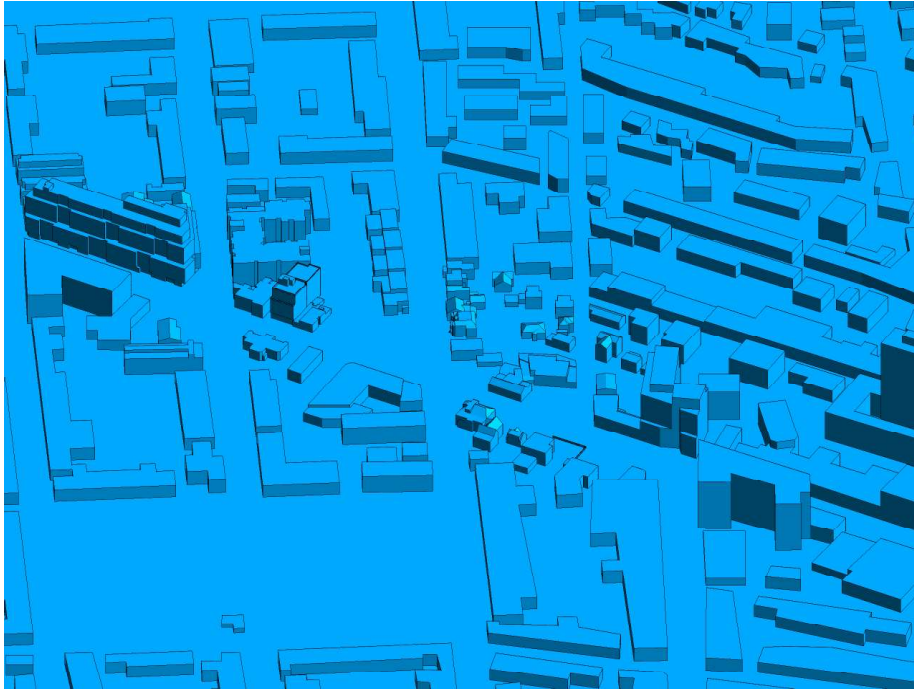
**FIGURE 2E: COMPUTATIONAL MODEL, PROPOSED MASSING, SOUTH PERSPECTIVE**



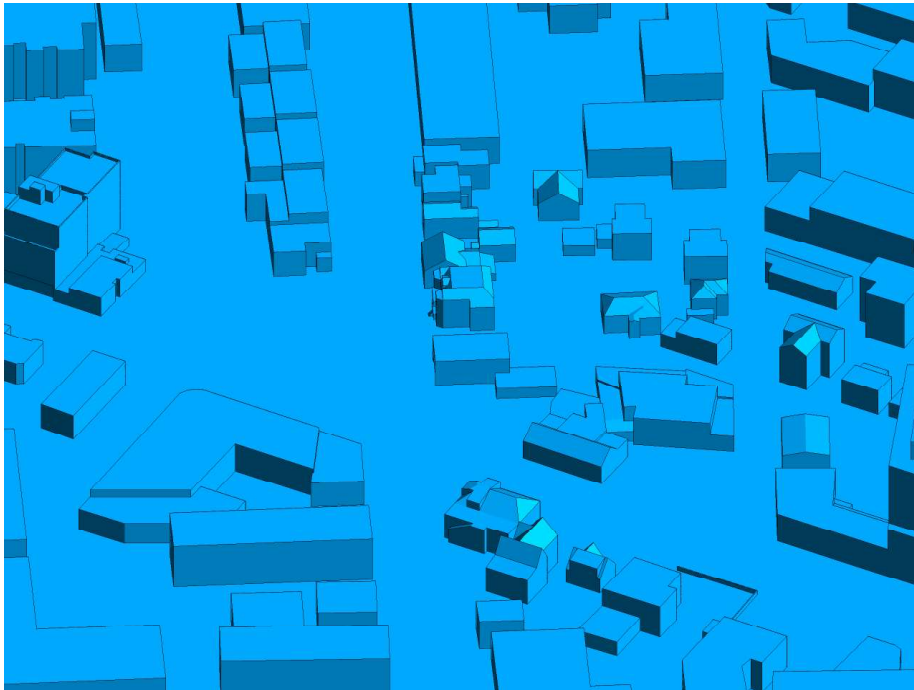
**FIGURE 2F: CLOSE-UP VIEW OF FIGURE 2E**



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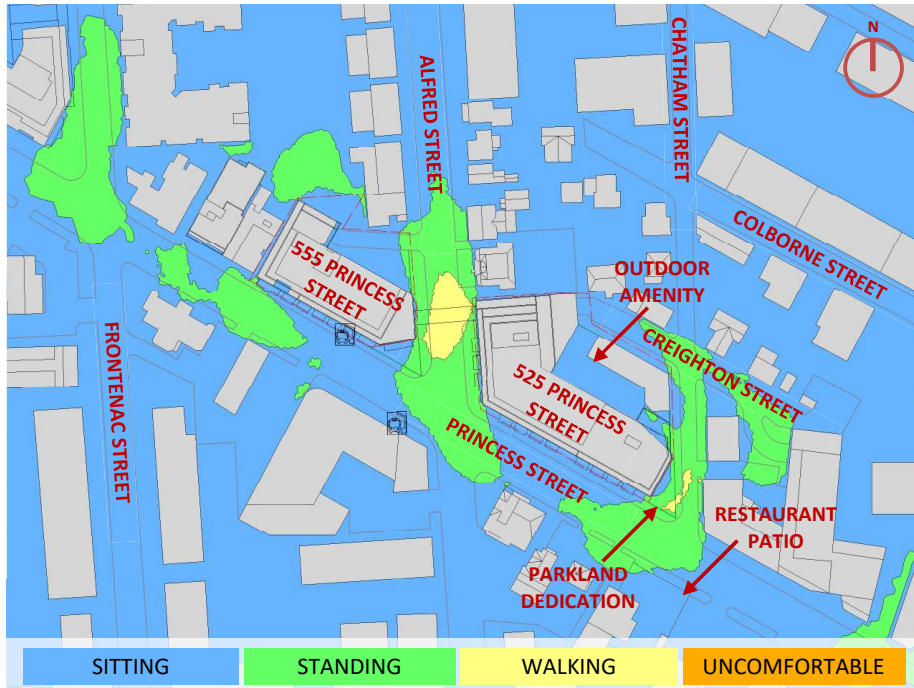
**FIGURE 2G: COMPUTATIONAL MODEL, EXISTING MASSING, SOUTH PERSPECTIVE**



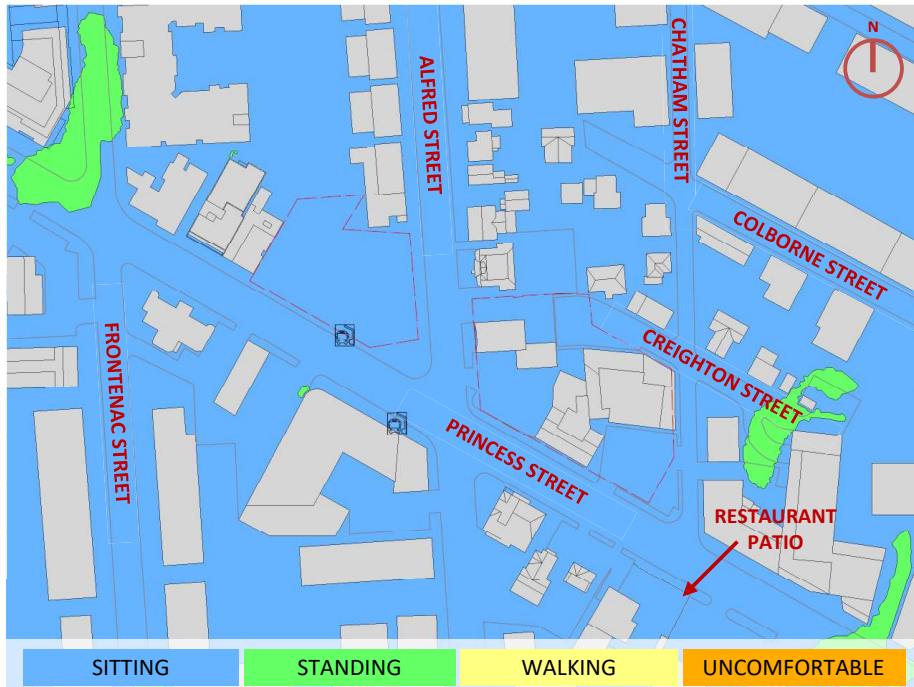
**FIGURE 2H: CLOSE-UP VIEW OF FIGURE 2G**



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**FIGURE 3A: SPRING – PROPOSED MASSING – WIND COMFORT, GRADE LEVEL**



**FIGURE 3B: SPRING – EXISTING MASSING – WIND COMFORT, GRADE LEVEL**



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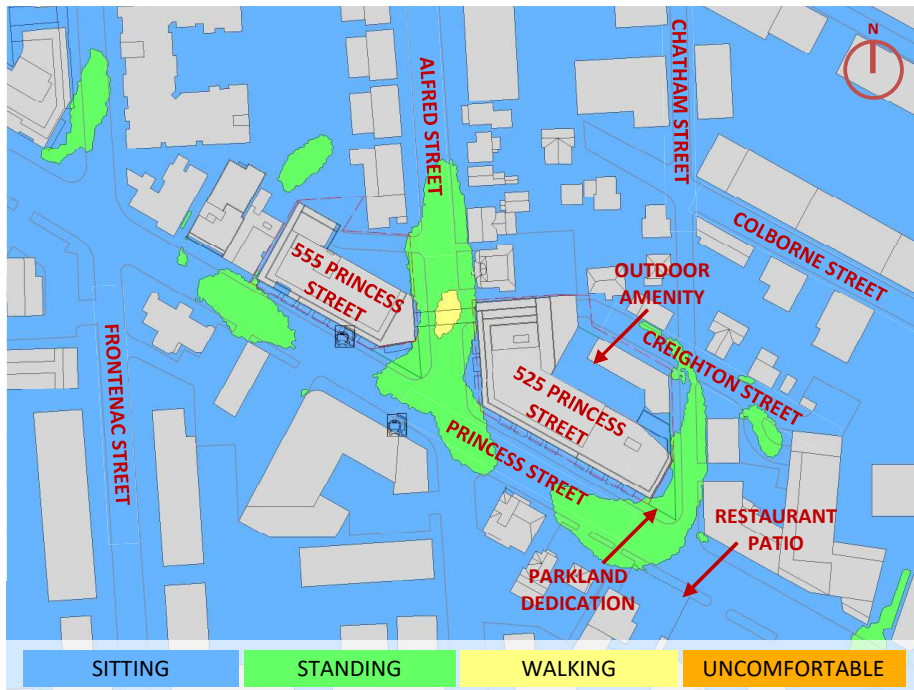


FIGURE 4A: SUMMER – PROPOSED MASSING – WIND COMFORT, GRADE LEVEL

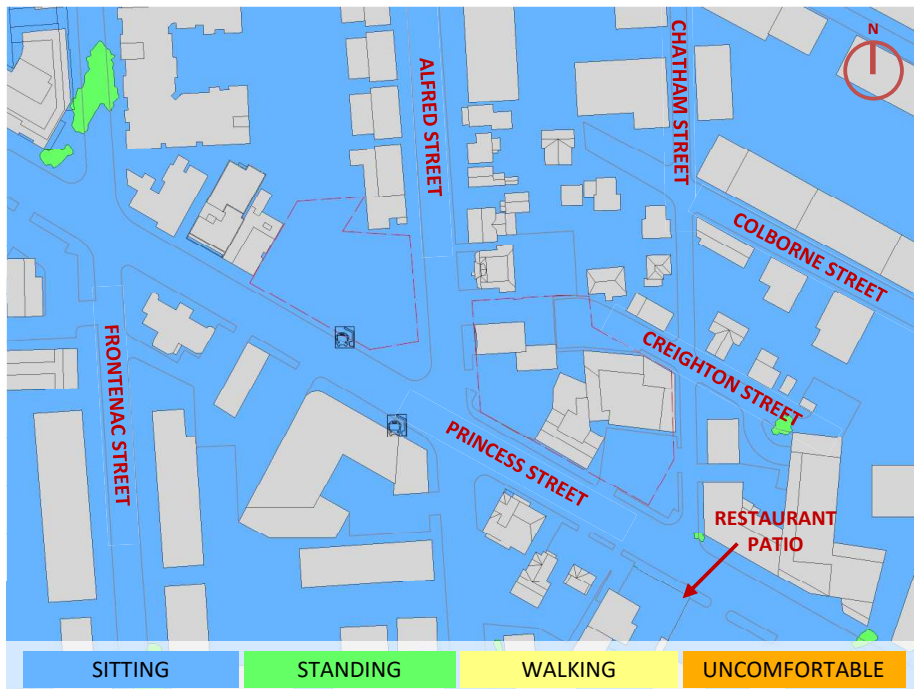


FIGURE 4B: SUMMER – EXISTING MASSING – WIND COMFORT, GRADE LEVEL



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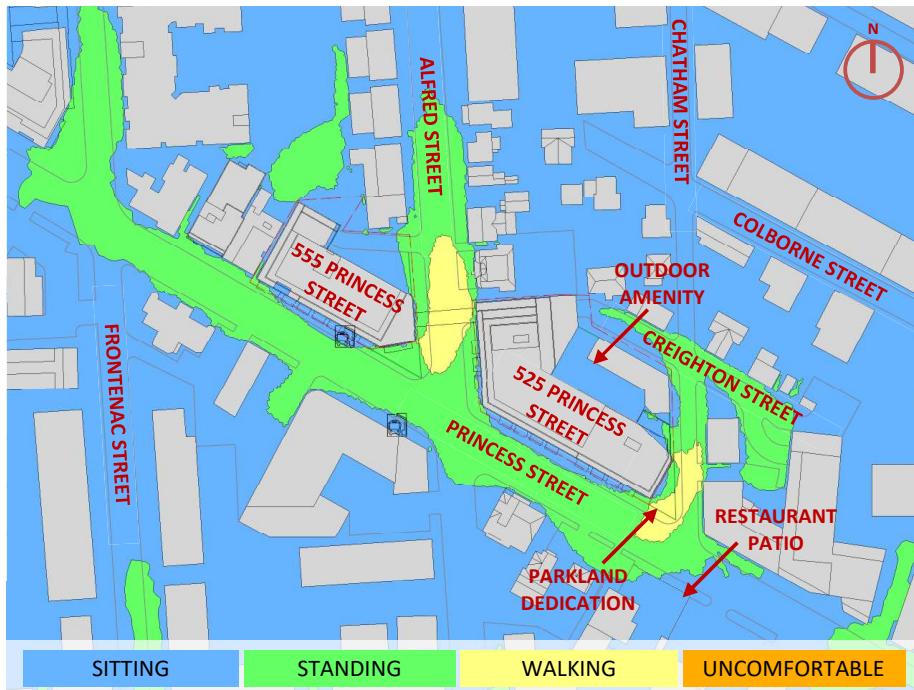


FIGURE 5A: AUTUMN – PROPOSED MASSING – WIND COMFORT, GRADE LEVEL

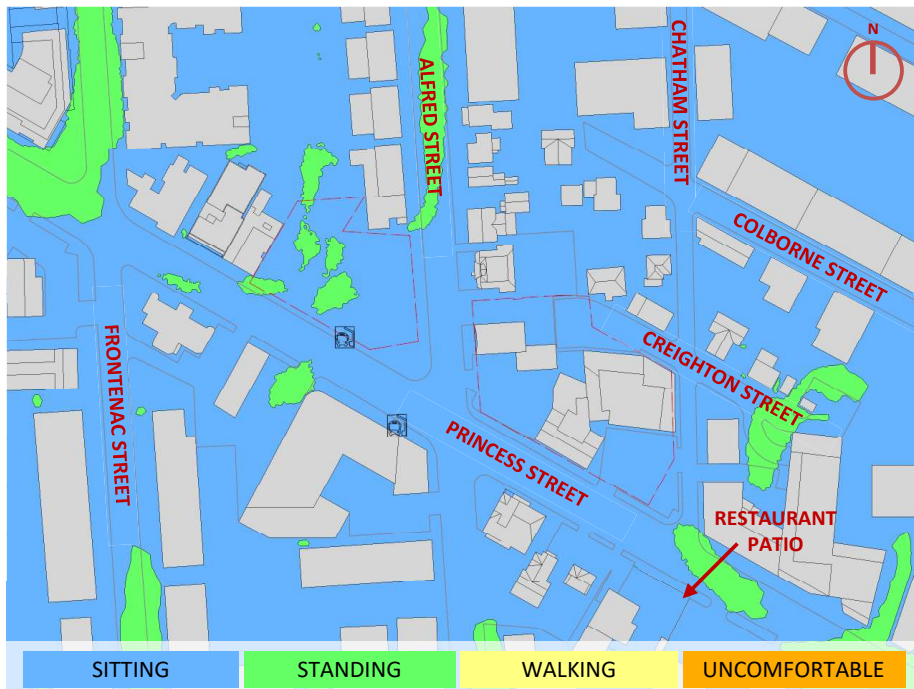


FIGURE 5B: AUTUMN – EXISTING MASSING – WIND COMFORT, GRADE LEVEL



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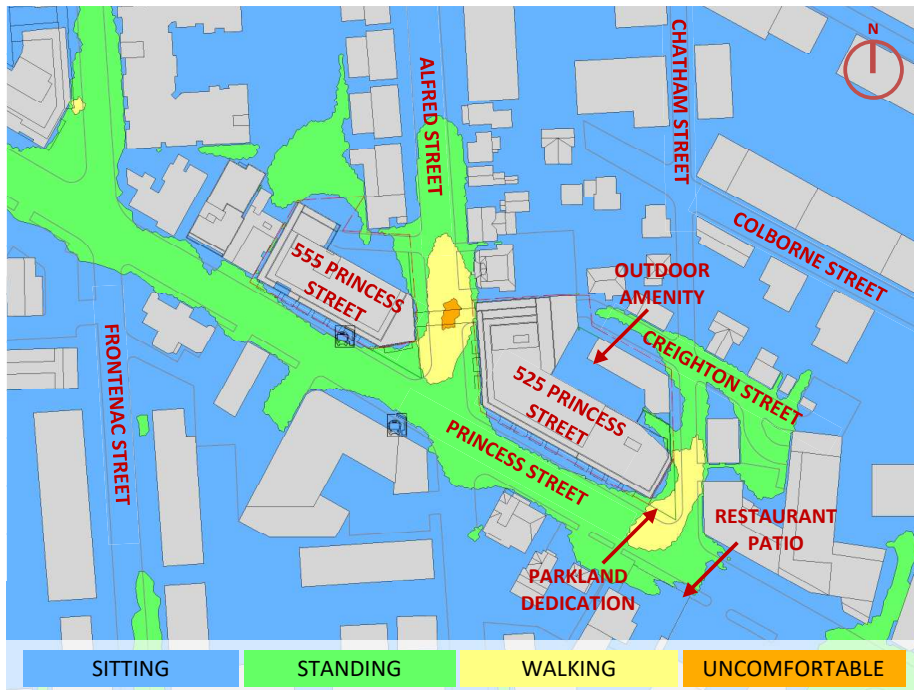


FIGURE 6A: WINTER – PROPOSED MASSING – WIND COMFORT, GRADE LEVEL

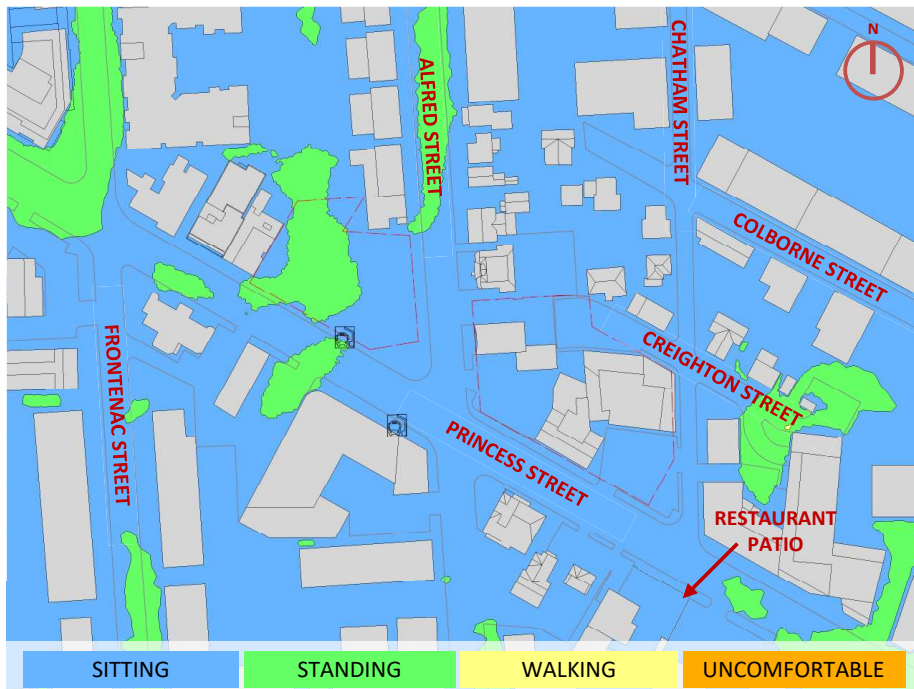


FIGURE 6B: WINTER – EXISTING MASSING – WIND COMFORT, GRADE LEVEL



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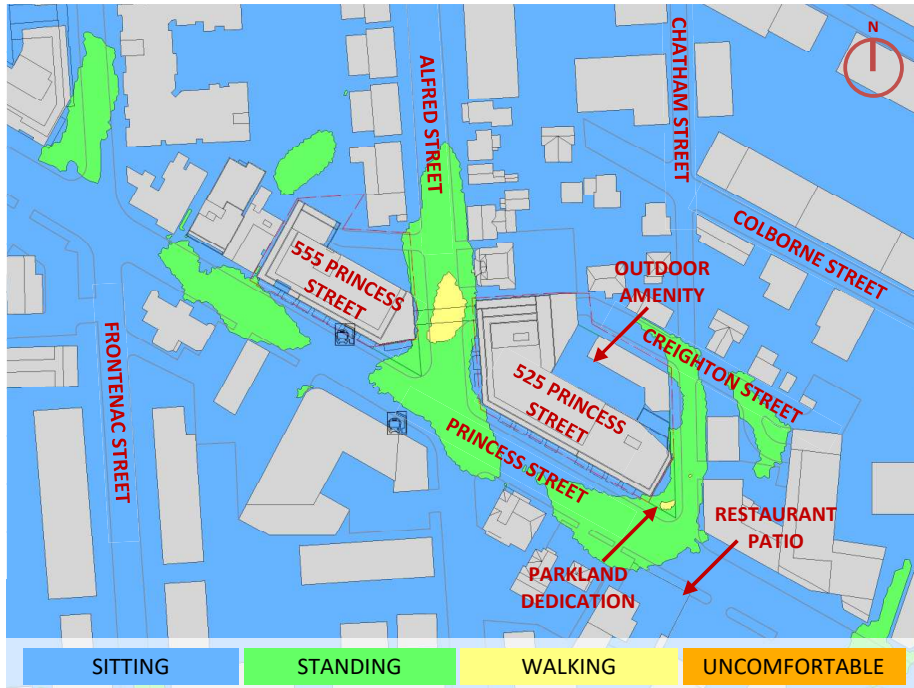


FIGURE 7A: TYPICAL USE PERIOD – PROPOSED MASSING – WIND COMFORT, GRADE LEVEL

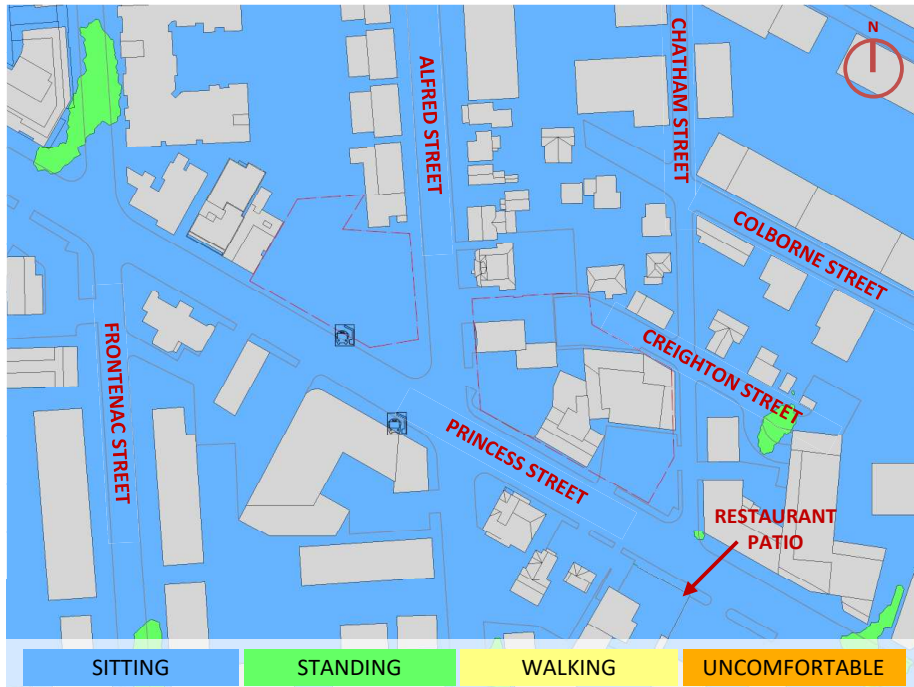


FIGURE 7B: TYPICAL USE PERIOD – EXISTING MASSING – WIND COMFORT, GRADE LEVEL



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**FIGURE 8A: SPRING – WIND COMFORT, 525 PRINCESS STREET AMENITY TERRACE**



**FIGURE 8B: SUMMER – WIND COMFORT, 525 PRINCESS STREET AMENITY TERRACE**



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**FIGURE 8C: AUTUMN – WIND COMFORT, 525 PRINCESS STREET AMENITY TERRACE**



**FIGURE 8D: WINTER – WIND COMFORT, 525 PRINCESS STREET AMENITY TERRACE**

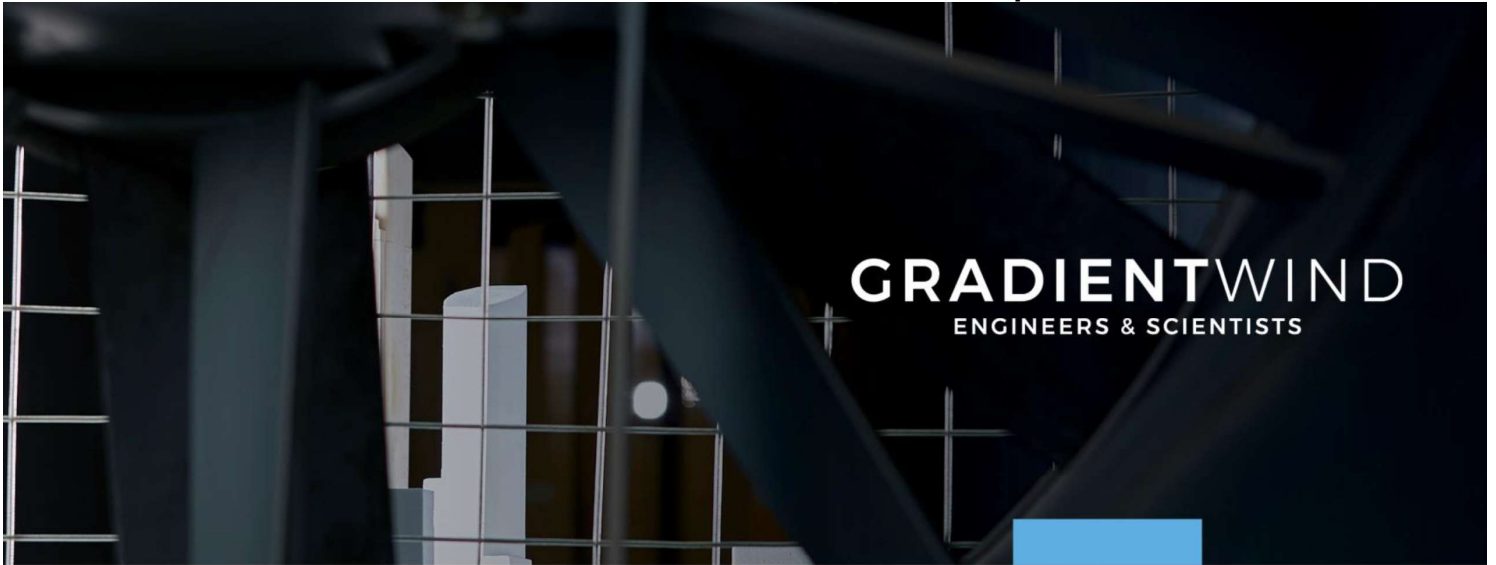


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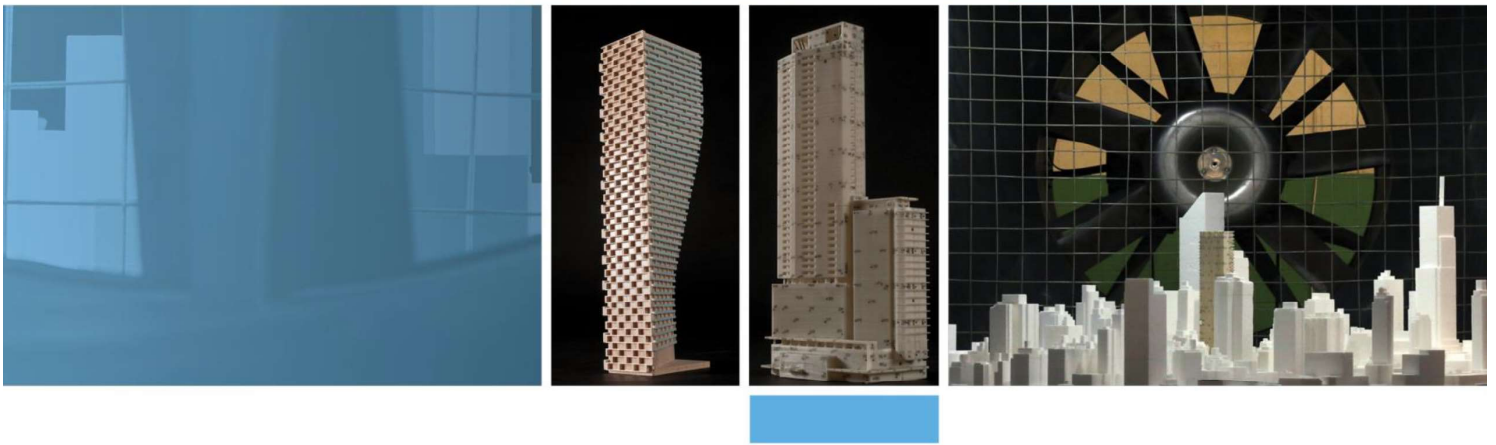
**FIGURE 9: TYPICAL USE – WIND COMFORT, 525 PRINCESS STREET AMENITY TERRACE**





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## APPENDIX A

### SIMULATION OF THE ATMOSPHERIC BOUNDARY LAYER

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## SIMULATION OF THE ATMOSPHERIC BOUNDARY LAYER

The atmospheric boundary layer (ABL) is defined by the velocity and turbulence profiles according to industry standard practices. The mean wind profile can be represented, to a good approximation, by a power law relation, Equation (1), giving height above ground versus wind speed [1], [2].

$$U = U_g \left( \frac{Z}{Z_g} \right)^\alpha \quad \text{Equation (1)}$$

where  $U$  = mean wind speed,  $U_g$  = gradient wind speed,  $Z$  = height above ground,  $Z_g$  = depth of the boundary layer (gradient height), and  $\alpha$  is the power law exponent.

For the model,  $U_g$  is set to 6.5 metres per second (m/s), which approximately corresponds to the 40% mean wind speed for Kingston based on historical climate data and statistical analyses. When the results are normalized by this velocity, they are relatively insensitive to the selection of gradient wind speed.

$Z_g$  is set to 540 m. The selection of gradient height is relatively unimportant, so long as it exceeds the building heights surrounding the subject site. The value has been selected to correspond to our physical wind tunnel reference value.

$\alpha$  is determined based on the upstream exposure of the far-field surroundings (that is, the area that is not captured within the simulation model).

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Table 1 presents the values of  $\alpha$  used in this study, while Table 2 presents several reference values of  $\alpha$ . When the upstream exposure of the far-field surroundings is a mixture of multiple types of terrain, the  $\alpha$  values are a weighted average with terrain that is closer to the subject site given greater weight.

**TABLE 1: UPSTREAM EXPOSURE (ALPHA VALUE) VS TRUE WIND DIRECTION**

Wind Direction (Degrees True)	Alpha Value ( $\alpha$ )
0	0.24
22.5	0.24
45	0.23
67.5	0.22
90	0.22
112.5	0.21
135	0.21
157.5	0.21
180	0.21
202.5	0.22
225	0.23
247.5	0.24
270	0.24
292.5	0.25
315	0.24
337.5	0.23

**TABLE 2: DEFINITION OF UPSTREAM EXPOSURE (ALPHA VALUE)**

Upstream Exposure Type	Alpha Value ( $\alpha$ )
Open Water	0.14-0.15
Open Field	0.16-0.19
Light Suburban	0.21-0.24
Heavy Suburban	0.24-0.27
Light Urban	0.28-0.30
Heavy Urban	0.31-0.33

The turbulence model in the computational fluid dynamics (CFD) simulations is a two-equation shear-stress transport (SST) model, and thus the ABL turbulence profile requires that two parameters be defined at the inlet of the domain. The turbulence profile is defined following the recommendations of the Architectural Institute of Japan for flat terrain [3].

$$I(Z) = \begin{cases} 0.1 \left( \frac{Z}{Z_g} \right)^{-\alpha-0.05}, & Z > 10 \text{ m} \\ 0.1 \left( \frac{10}{Z_g} \right)^{-\alpha-0.05}, & Z \leq 10 \text{ m} \end{cases} \quad \text{Equation (2)}$$

$$L_t(Z) = \begin{cases} 100 \text{ m} \sqrt{\frac{Z}{30}}, & Z > 30 \text{ m} \\ 100 \text{ m}, & Z \leq 30 \text{ m} \end{cases} \quad \text{Equation (3)}$$

where  $I$  = turbulence intensity,  $L_t$  = turbulence length scale,  $Z$  = height above ground, and  $\alpha$  is the power law exponent used for the velocity profile in Equation (1).

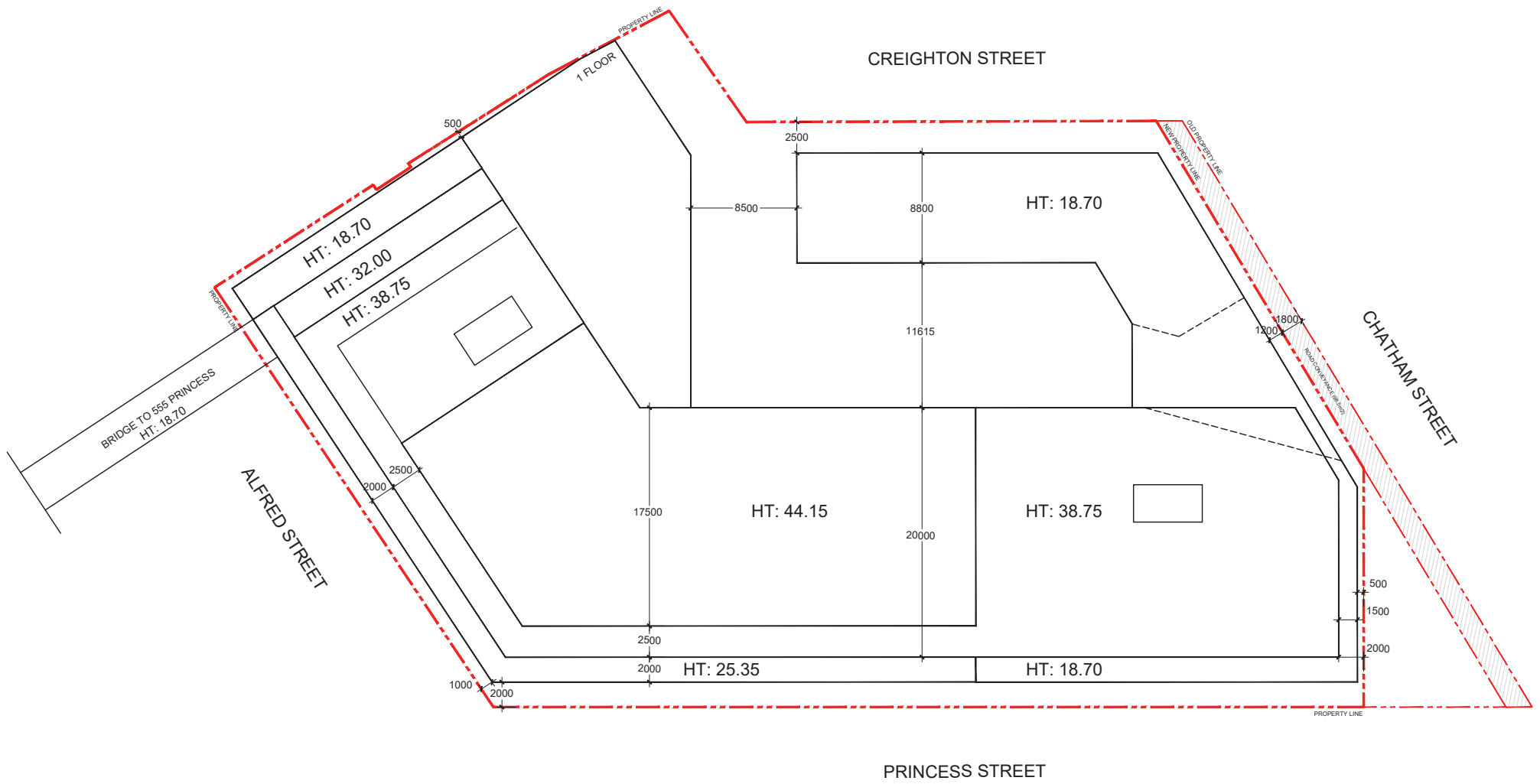
Boundary conditions on all other domain boundaries are defined as follows: the ground is a no-slip surface; the side walls of the domain have a symmetry boundary condition; the top of the domain has a specified shear, which maintains a constant wind speed at gradient height; and the outlet has a static pressure boundary condition.

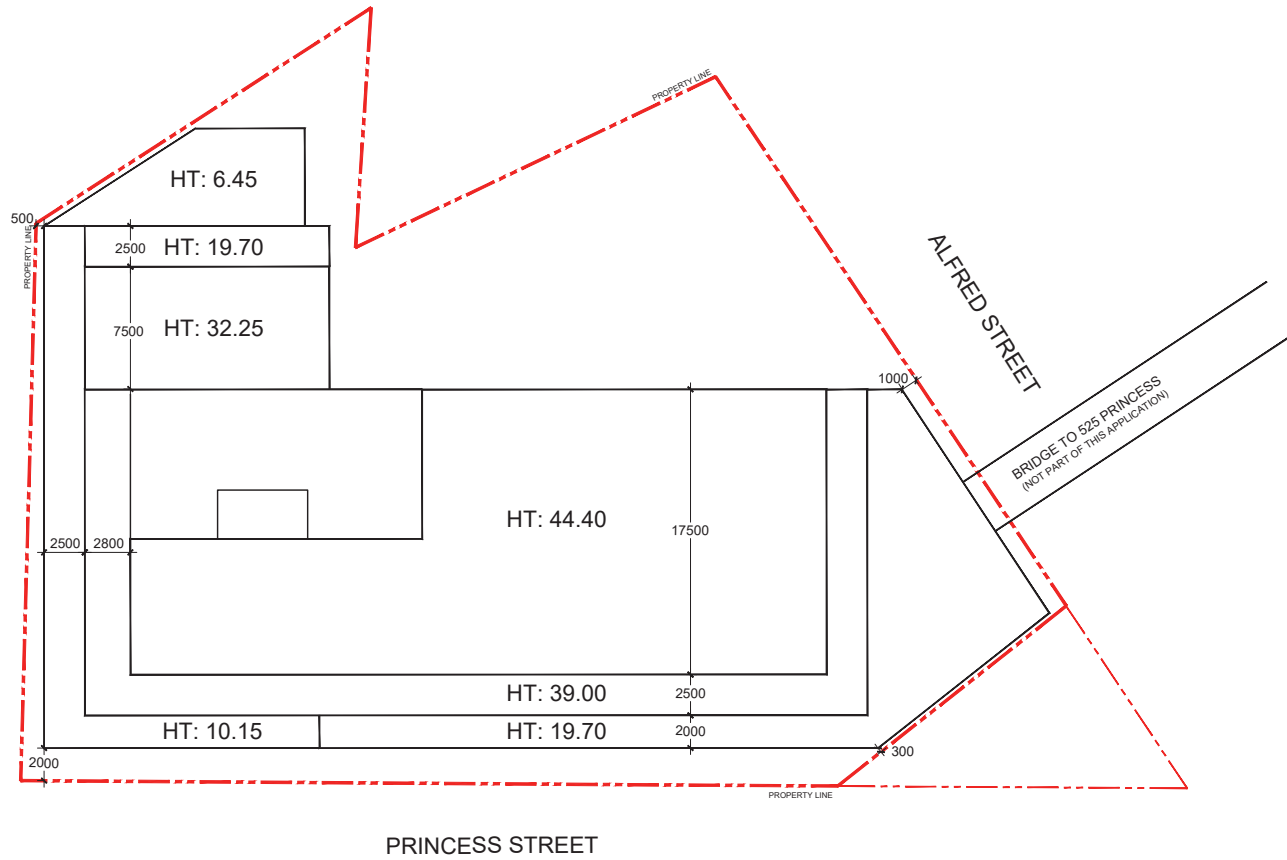
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**REFERENCES**

- [1] P. Arya, "Chapter 10: Near-neutral Boundary Layers," in *Introduction to Micrometeorology*, San Diego, California, Academic Press, 2001.
- [2] S. A. Hsu, E. A. Meindl and D. B. Gilhousen, "Determining the Power-Law Wind Profile Exponent under Near-neutral Stability Conditions at Sea," vol. 33, no. 6, 1994.
- [3] Y. Tamura, H. Kawai, Y. Uematsu, K. Kondo, and T. Okhuma, "Revision of AIJ Recommendations for Wind Loads on Buildings," in *The International Wind Engineering Symposium, IWES 2003*, Taiwan, 2003.







Site Photographs – January 8, 2026



Figure 1: View of 555 Princess Street from the northwest corner of Princess Street and Alfred Street with views of 362 Alfred Street (background, right), 501 Frontenac Street (background, middle) and 575 Princess Street (background, left).



Figure 2: Partial views of 525 Princess Street (right) and 555 Princess Street (left) from in front of 362 Alfred Street.



Figure 3: View of 525 Princess Street from the northwest corner of Princess Street and Alfred Street with views 495 Princess Street (background, left) and 333 University Street (background, right) visible.



Figure 4: View of existing building addressed 525 Princess Street, currently vacant.



Figure 5: View of west parking lot at 525 Princess Street, with building addressed 347-349 Alfred Street visible in centre background.



Figure 6: View of 525 Princess Street from the northwest corner of Princess Street and Chatham Street.



Figure 7: View of 525 Princess Street from the south side of Creighton Street.

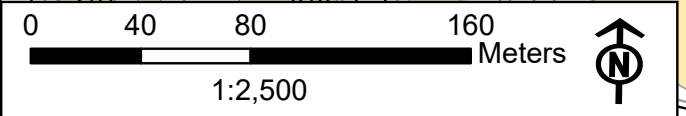
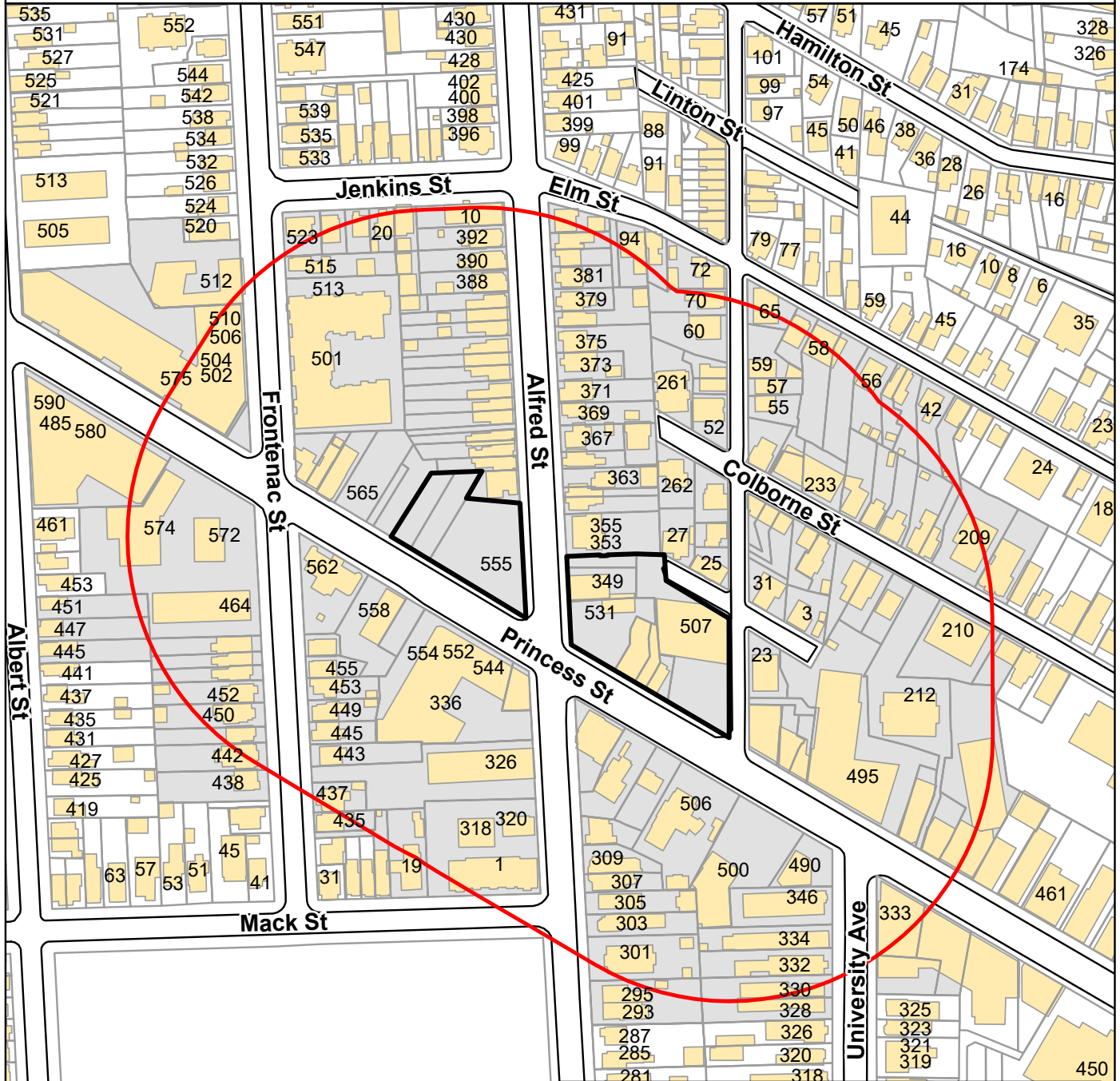


Figure 8: View of Creighton Street with 525 Princess Street on left.



Planning Committee  
**Public Notification Map**  
Address: 525 - 555 Princess Street  
File Number: D14-013-2025

- Subject Lands
- Property Boundaries
- Proposed Parcels
- 120m Public Notification Boundary
- 180 Properties in Receipt of Notice (MPAC)



**Clendening, Ian**

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**From:** Clendening, Ian  
**Sent:** November 25, 2025 3:52 PM  
**To:** [REDACTED]  
**Cc:** Birdi, Chanti  
**Subject:** RE: Follow up information from our Meeting (525/555 Princess Street - Application D14-013-2025)

Hi John,

Sorry for the delay in responding due to my vacation. I hope that you found the Public Meeting informative.

In response to your follow-up questions to my initial email, both Planning and Engineering/Public Works will have leading roles at different stages of the project. At the current Zoning By-Law amendment stage, Planning is reviewing the overall proposal (i.e., both buildings and the connective walkway) against the policies of the Official Plan (including urban design criteria) and are gathering feedback from technical reviewers and public input. This process is intended to determine whether the proposal represents 'good planning', and is consistent with Provincial Policy and the City's Official Plan.

Technical reviewers have indicated support in principal for the bridge, with details (such as snow clearance and maintenance requirements) to be determined through the encroachment permit process. The recommendation report for the Zoning By-Law amendment is intended to include a recommendation to authorize the Manager of Development Engineering to enter into the encroachment agreement, as the Encroachment By-Law does not currently contemplate this type of structure. One report with both Zoning By-law amendment and encroachment permit authority is intended to give the Committee, Council and public a fuller picture of the proposal and process, as opposed to separate reports.

Following finalization of the Zoning By-Law amendment and towards the end of the Site Plan Control process, the Development Engineering team would lead the encroachment permit process.

I do understand the concerns for the 'public benefit' as this relates to the City's road allowance. In a certain respect the provision of this pedestrian bridge would reduce congestion along the sidewalk as residents could traverse between the two buildings without the need to cross the intersection. I will concede that opinions will never be unanimous about aesthetics, but I think that this bridge feature could be seen as providing a placemaking 'public benefit' to the area as this intersection is afforded special treatment as one of a few intersections where greater height is permitted.

Kindly,



**Ian Clendening (he/him/his)**

Senior Planner  
Planning Services

City of Kingston  
Located at: 1211 John Counter Boulevard,  
216 Ontario Street Kingston, ON K7L 2Z3  
613-546-4291 extension 3126  
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The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** John Grenville [REDACTED]  
**Sent:** November 13, 2025 1:38 PM  
**To:** Clendening, Ian <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>; [REDACTED]  
**Cc:** Birdi, Chanti <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>  
**Subject:** RE: Follow up information from our Meeting (525/555 Princess Street - Application D14-013-2025)

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Hi Ian – Thanks for your email. A couple of questions:

1. Why has Engineering decided “that the proposal be looked at as a comprehensive whole such that Council can consider the proposal as intended by the applicant when determining whether to approve the amendments sought.” What is Engineering’s role? What is the Planning Dept’s role? I thought that it was the role of the Planning Department to make the recommendation to the Planning Committee based on their review and consultation with other departments, such as Engineering. Your email suggests otherwise.
2. Based on the Technical Review has the Engineering Department provided any feedback, posed any questions or made any comments on the proposed bridge?
3. Clearly there is a private benefit in allowing the developer to use the air space above the public right-of-way to construct a bridge. What is the public benefit? Until the public benefit is established and communicated, how can the proposal be evaluated?

I look forward to hearing from you. John

John Grenville, [REDACTED]  
24 Jenkins Street, Kingston, ON K7K 1N3  
[REDACTED]

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**From:** Clendening, Ian <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>  
**Sent:** November-12-25 4:30 PM  
**To:** [REDACTED]

Cc: Birdi,Chanti <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>

Subject: Follow up information from our Meeting (525/555 Princess Street - Application D14-013-2025)

Hi Everyone,

First of all, I want to thank you again for attending the meeting with Chanti, James and myself to go through the revisions to the development at 525/555 Princess Street.

During the meeting, I committed to getting back to you with regards to certain items including more detailed information on the type of units being constructed.

The tables below provide a break down by unit type for each of the two buildings in the current proposal and the previous proposal (organized differently, but with key information highlighted for ease of reference).

I can note that the revised plans have an overall reduction of 119 units between the two buildings.

**Current Proposal 525 Princess Street** (east of Alfred St.)

UNIT BREAKDOWN & COUNT	
PROPOSED	
STUDIO	157 (49.7%)
1-BEDROOM	49 (15.5%)
2-BEDROOM	62 (19.6%)
INCL. 2BR TOWNH.	-
3-BEDROOM	48 (15.2%)
INCL. 3BR TOWNH.	13
TOTAL	316

**Current Proposal 555 Princess Street** (west of Alfred St.)

UNIT BREAKDOWN & COUNT	
PROPOSED	
STUDIO	85 (49.4%)
1-BEDROOM	28 (16.3%)
2-BEDROOM	34 (19.8%)
3-BEDROOM	25 (14.5%)
TOTAL	172

**Previous Proposal 525 Princess Street** (east of Alfred St.)

UNIT COUNTS					
	STUDIO	1 BED	2 BED	3 BED	TOTAL
2ND FLOOR:	24	11	03	06	44
3RD FLOOR:	25	09	05	06	45
4TH FLOOR:	24	10	05	04	43
5TH FLOOR:	16	08	05	04	33
6TH FLOOR:	22	10	06	02	40
7TH FLOOR:	21	09	07	02	39
8TH FLOOR:	21	09	07	02	39
9TH FLOOR:	20	06	07	01	34
10TH FLOOR:	25	03	08	01	37
	198	75	53	28	354
	(56%)	(21%)	(15%)	(8%)	
					+ 21 WALKUP APARTMENTS (1-BEDROOMS) + 6 STACKED TOWNHOMES (5 1-BEDROOMS, 1 2-BEDR)
					<b>381 UNITS TOTAL</b>

**Previous Proposal 555 Princess Street** (west of Alfred St.)

UNIT COUNTS					
	STUDIO	1 BED	2 BED	3 BED	TOTAL
2ND FLOOR:	19	03	06	01	29
3RD FLOOR:	20	04	07	01	32
4TH FLOOR:	20	04	07	01	32
5TH FLOOR:	14	01	04	02	21
6TH FLOOR:	20	01	06	00	27
7TH FLOOR:	20	01	06	00	27
8TH FLOOR:	20	01	06	00	27
9TH FLOOR:	06	00	04	00	10
10TH FLOOR:	12	00	09	00	21
	151 (66.8%)	15 (6.6%)	55 (24.3%)	05 (2.2%)	226 UNITS TOTAL

I recognize that the unit counts tie in with your concern for parking.

When addressing the proposed reduction in parking for the site to 0.2 spaces per dwelling unit, which was approved through Minor Variance Application D13-064-2023, the applicant submitted a Parking Brief that built upon the initial Traffic Impact Study (attached as requested) submitted for the Zoning By-law Amendment. The report spoke specifically to the topic of ‘Young Professional and Family Demographic Parking Rates’ stating that:

The peak parking demand at 117-175 Park St was observed to be 0.38 spaces per unit. However, it was highlighted that legacy parking behaviour does not necessarily predict future parking behaviour, particularly considering transit and active travel improvements within a revitalized Williamsville Main Street area.

and further,

Based on a comparison between general urban / suburban parking demand ratios and dense multi-use urban of parking demand rates as published in Parking Generation Manual – 5th Edition (Institute of Transportation Engineers, January 2019), future parking demand among the young professional and family demographic was projected to be approximately 0.25 spaces per unit.

Through the revised proposal, the applicant intends to provide a modest increase over the required parking ratio with 0.21 spaces per unit (i.e., 5 spaces beyond what would otherwise be required at the 0.2 spaces per unit ratio).

In regards to concerns for the frontage along Princess and the potential ‘valley’ effect, I have provided below the height maps for each of the properties. Rather than use minimum side yard setbacks or front yard setbacks, the height maps were used to tie the buildings specified maximum heights the areas specified in the graphic. In order to deviate from the exact built form established by the initial zoning by-law, the applicant intends to update these height maps.

The image on the **left** shows the currently approved plan which would allow for a setback of between 1 and 1.4 metres for most of the buildings frontage the current proposal is on the **right**. Through the revised proposal, staff have been able to work with the developer to achieve a minimum 2 metre setback which increases to 3 metres at the east end of the 555 Princess Street Property.



**Clendening, Ian**

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**From:** Birdi, Chanti  
**Sent:** October 29, 2025 11:23 AM  
**To:** [REDACTED]  
**Cc:** [REDACTED]; Clendening, Ian  
**Subject:** Re: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

Hi John,

The Community Benefits payments will be taken during the future Hold Lift application. The benefits are secured through Holding Overlay Numbers H8 and H41 and payment must be provided before these Holds are lifted. This generally occurs towards the end of the site plan process.

The funds are then placed in a Community Benefits account and there would be future consultation for any new parkland programming on purchased lands. The location and programming of lands under these future funds are not known at this time.

Regarding encroachment permit fees, a new fee will need to be established as the Fees and Charges By-law does not contemplate this type of structure. As a first of its kind, Engineering and Public Works staff will need to review potential implications and propose a new annual fee to cover any additional maintenance or application processing associated with the structure. The fee is not known at this time.

Thank you for the questions and looking forward to meeting this afternoon,  
Chanti

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**From:** John Grenville [REDACTED] >  
**Sent:** Saturday, October 25, 2025 6:01 PM  
**To:** Birdi, Chanti <cbirdi@cityofkingston.ca>  
**Cc:** [REDACTED]

**Subject:** RE: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

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Hello Chanti – The draft ZBA in the planning justification does not contain any reference to the community benefit that was approved by Council in May 2022. I assume that this means that the community benefit has been paid or is payable at some time in the future. Could you confirm that status of the community benefit and, if unpaid, let me know when it will be paid. I am also interested in the status of the search for parkland on Princess Street that will fulfill the community benefit commitment.

John

John Grenville, [REDACTED]  
24 Jenkins Street, Kingston, ON K7K 1N3  
[REDACTED]

---

**From:** John Grenville [REDACTED]  
**Sent:** October-24-25 10:16 AM  
**To:** 'Birdi,Chanti' <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>  
**Cc:** [REDACTED]  
**Subject:** RE: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

Hello Chanti - Thanks very much for the detailed information. I am interested in the encroachment fee that will be payable to the City for the elevated pedestrian bridge. I have read the encroachment by-law and the fees by-law and don't see anything that covers a permanent bridge across a municipal street. Can you provide me with an indication of the annual fee or direct me to the staff person who is responsible for encroachment permits.

Thanks for your help. John

John Grenville, [REDACTED]  
24 Jenkins Street, Kingston, ON K7K 1N3  
[REDACTED]

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**From:** Birdi,Chanti <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>  
**Sent:** October-22-25 11:59 AM  
**To:** [REDACTED]  
**Cc:** [REDACTED]; Bar,James <[jbar@cityofkingston.ca](mailto:jbar@cityofkingston.ca)>; Clendening,Ian <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>  
**Subject:** Re: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

Hi John,

Thank you for your questions.

The file for D35-001-2019 does contain plans ranging from 2019 to 2022. The 2019 file number reflects when the application was initially submitted and it's very common for plans to be resubmitted following the first technical review cycles. The approval was granted in 2022.

Within the current submission (D14-013-2025), the Planning Justification Report prepared by Fotenn refers to the "pedestrian bridge" in relation to amenity space, shared programming and urban design. The intent is to provide residents with a weather-protected connection between the two buildings to allow shared use of amenity space in each building.

The feasibility and supportability of the skyway is being reviewed as part of this submission and, if supported, would be secured through an encroachment agreement between the City and property owner alongside the Site Plan Control stage. The Ministry of Transportation (MTO) does not have jurisdiction over roads in this area and approvals would lie with the City. The Encroachment By-Law No. 2004-107 can be found at [this link](#). An encroachment agreement under this By-law would come with requirements (such as maintenance requirements, clearance sign requirements, etc.) and an annual fee.

Regarding other applicable law, I understand from our Building Services reviewer that the buildings and skywalk would be reviewed under Part 3 of the Building Code, however, it should be noted that the technical review is still underway. Building Services may have additional comments or recommendations following full review. The Building Permit itself would not be issued until the zoning, site plan and encroachment agreement are all in effect.

Hope this helps provide some initial direction to your review. Please let me know if you have any other questions as you work through the submission materials.

Thank you,  
Chanti



**Chanti Birdi, RPP, MCIP (she/her)**

Intermediate Planner

Growth & Development

City of Kingston

Located at: 1211 John Counter Boulevard

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The City of Kingston acknowledges that we are on the traditional homeland of the Anishinaabe, Haudenosaunee and the Huron-Wendat, and thanks these nations for their care and stewardship over this shared land.

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**From:** John Grenville [REDACTED] >  
**Sent:** Tuesday, October 21, 2025 4:42 PM

To: Birdi,Chanti <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>; [REDACTED]

Cc: Clendening,Ian <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>; Bar,James <[jbar@cityofkingston.ca](mailto:jbar@cityofkingston.ca)>

Subject: RE: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

You don't often get email from [REDACTED]. [Learn why this is important](#)

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Hi Chanti - I forgot to ask – is there anything on DASH that explains the need that is being fulfilled by the skywalk? The question is – why is public space (the area above the street) being requested for private use? What is the public benefit in allowing the use of public space for private benefit?

John

John Grenville, [REDACTED]

24 Jenkins Street, Kingston, ON K7K 1N3

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**From:** John Grenville [REDACTED] >

**Sent:** October-21-25 2:45 PM

**To:** 'Birdi,Chanti' <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>; [REDACTED]

**Cc:** 'Clendening,Ian' <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>; 'Bar,James' <[jbar@cityofkingston.ca](mailto:jbar@cityofkingston.ca)>

**Subject:** RE: Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

Hello Chanti – I went to DASH with the file number that you provided - D35-001-2019, but could only find files under Supporting Information, dating to 2022 at the latest. Am I in the right place?

John

John Grenville, [REDACTED]

24 Jenkins Street, Kingston, ON K7K 1N3

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**From:** Birdi, Chanti <[cbirdi@cityofkingston.ca](mailto:cbirdi@cityofkingston.ca)>

**Sent:** October-20-25 3:51 PM

**To:** [REDACTED]

**Cc:** Clendening, Ian <[iclendening@cityofkingston.ca](mailto:iclendening@cityofkingston.ca)>; Bar, James <[jbar@cityofkingston.ca](mailto:jbar@cityofkingston.ca)>

**Subject:** Invitation to Review Revised Development Proposal (D14-013-2025 - 525 & 555 Princess Street)

Good afternoon,

I am reaching out in advance of an upcoming Community Meeting for a re-zoning application for the properties at 525 & 555 Princess Street.

Recently, the City received an application which proposes to modify the built forms initially approved on May 17, 2022 through application D35-001-2019. Approval of the 2019 application used a 'height map' to restrict development of the sites to the specific setbacks and building elevations put forward in the application. Since that time, the property has changed ownership and the new owners are seeking a further amendment to the Zoning By-law which would alter the previously approved building design.

Key elements of the proposal remain the same as what was initially approved including the 10-storey built form and the mixed-use nature of the development with ground floor commercial. One unique element which the proposed development seeks to allow through the amendment is the inclusion of a 'skyway bridge' linking the two buildings at the fourth floor, over Alfred Street.

The concept plans and elevations are attached and the full submission materials, including plans and reports, are available on the City's DASH portal ([accessible at this link](#)).

In advance of the Community Meeting scheduled for November 20, 2025, we would like to invite you to attend a virtual meeting with City staff as an opportunity to go over the proposed development and answer any questions that you may have.

If you are interested in meeting, I welcome you to indicate your availability using a Doodle poll at: <https://doodle.com/group-poll/participate/egXPl1ka>. Please select your preferred time by this **Friday, October 24<sup>th</sup>**. If you have any issues accessing the Doodle poll, please do not hesitate to contact me for assistance.

If you cannot attend this meeting, you are welcome to join the Community Meeting on November 20th at 6pm at Planning Committee to ask questions and provide comments. The purpose of the Community Meeting is to gather feedback and no staff recommendation or decision is made at this meeting.

If there are any other Association members who have not been included on this email and may wish to join, please feel free to forward this email or provide their contact information so we can reach out to them.

Thank you and looking forward to meeting with you,

Chanti



**Chanti Birdi, RPP, MCIP (she/her)**

Intermediate Planner

Growth & Development

City of Kingston

Located at: 1211 John Counter Boulevard

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