



**City of Kingston
Report to Planning Committee
Report Number PC-25-035**

To: Chair and Members of the Planning Committee
From: Paige Agnew, Commissioner, Growth & Development Services
Resource Staff: Tim Park, Director, Planning Services
Date of Meeting: September 18, 2025
Subject: Recommendation Report
File Number: D14-008-2025
Address: 33 Compton Street
District: 7 – Kingscourt-Rideau
Application Type: Zoning By-Law Amendment
Owner: Compton Street Developments Inc.
Applicant: Fotenn – Planning and Design

Council Strategic Plan Alignment:

Theme: 1. Support Housing Affordability

Goal: 1.1 Promote increased supply and affordability of housing.

Executive Summary:

The following is a report recommending approval to the Planning Committee regarding an application for a zoning by-law amendment submitted by Fotenn – Planning and Design, on behalf of Compton Street Developments Inc., with respect to the subject site located at 33 Compton Street.

The purpose of this application is to facilitate the development of a total of 40 new homes on the subject lands, which are currently vacant and undeveloped, as a mix of stacked townhouse buildings and freehold townhouse buildings. For the stacked townhouses, two separate buildings each containing 16 residential units are proposed on the western side of the lot at a height of three

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and a half storeys. For the freehold townhouses, a total of 8 residential units are proposed on the eastern side of the lot at a height of two storeys each.

The stacked townhouse development is proposed to include a parking lot containing a total of 32 parking spaces with an additional two visitor parking spaces, one short-term delivery parking space, and one car-share space. Furthermore, a total of 35 bicycle parking spaces will be provided on the site. For the freehold townhouses, each of these homes will be developed with driveway access from Compton Street and a private garage suitable for vehicle or bicycle parking.

Approval of this Zoning By-Law Amendment application will enable stacked townhouses as a permitted use for the property and establish other zoning standards which will facilitate the proposed development of townhouses and stacked townhouses. Following an approval of this application, a Site Plan Control application will be required for the development of the stacked townhouses, and a Part Lot Control Exemption application will be required to facilitate land division of the planned freehold townhouses and make these homes available for separate ownership. It is anticipated that a Final Plan of Condominium application will also be required to make the stacked townhouse units available for separate ownership. It is the applicant's intention that all new homes created through this development will be for market rate home ownership.

The proposed development supports the City's 2023-2026 Strategic Plan objective of promoting the increased supply of all housing forms. Additionally, the proposal is consistent with the specific goals of the Rideau Heights Regeneration Plan, approved by City Council in 2015. One of the numerous objectives of this neighbourhood revitalization plan is to introduce market housing into an area consisting of predominantly rent-geared-to-income (RGI) housing units, with the goal of providing a more balanced mix of housing for this community. The subject property was envisioned for the development of market rate housing as part of Phase 2 of the Rideau Heights Regeneration Strategy.

Staff have reviewed all materials submitted in support of the application and are satisfied that the proposal meets all policy tests and represents good land use planning in the public interest.

Recommendation:

That the Planning Committee recommends to Council:

That the application for a zoning by-law amendment (File Number D14-008-2025) submitted by Fotenn – Planning and Design, on behalf of Compton Street Developments Inc., for the property municipally known as 33 Compton Street, be approved; and

That Kingston Zoning By-Law Number 2022-62, as amended, be further amended, as per Exhibit A (Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62) to Report Number PC-25-035; and

That Council determines that in accordance with Section 34(17) of the *Planning Act*, no further notice is required prior to the passage of the by-law; and

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That the amending by-law be presented to Council for all three readings.

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Authorizing Signatures:

p.p.

ORIGINAL SIGNED BY COMMISSIONER

**Paige Agnew, Commissioner,
Growth & Development Services**

ORIGINAL SIGNED BY CHIEF ADMINISTRATIVE OFFICER

**Lanie Hurdle, Chief
Administrative Officer**

Consultation with the following Members of the Corporate Management Team:

Jennifer Campbell, Commissioner, Community Services	Not required
Neil Carbone, Commissioner, Corporate & Emergency Services	Not required
David Fell, President & CEO, Utilities Kingston	Not required
Desirée Kennedy, Chief Financial Officer & City Treasurer	Not required
Jenna Morley, City Solicitor	Not required
Ian Semple, Commissioner, Transportation & Infrastructure	Not required

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Options/Discussion:**Statutory Public Meeting**

This recommendation report forms the basis of a statutory public meeting at Planning Committee. Anyone who attends the statutory public meeting may present an oral submission, and/or provide a written submission on the proposed application. Also, any person may make written submissions at any time before City Council makes a decision on the application.

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of Kingston to the Ontario Land Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of Kingston before the by-law is passed, the person or public body is not entitled to appeal the decision. If a person or public body does not make oral submissions at a public meeting, or make written submissions to the City of Kingston before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

Planning Committee will consider the recommendations in this report and make its recommendation to City Council at this meeting.

Anyone wishing to be notified of Council's decision on the subject application must submit a written request to:

Victoria McCutcheon, Planner
The Corporation of the City of Kingston
Planning Services
216 Ontario Street
Kingston, ON K7L 2Z3
613-546-4291 extension 3237
vmccutcheon@cityofkingston.ca

Background and Decision Date

In accordance with By-Law Number 2007-43, this application was subject to a pre-application. The applicant was provided with a pre-application report on December 21, 2023.

Following the pre-application process, a complete application was submitted by the applicant and was deemed to be complete as of May 9, 2025. A Community Meeting was held at Planning Committee on June 19, 2025. A summary of the feedback received at the Community Meeting is provided in the Public Comments section of this Report.

In accordance with the *Planning Act*, this application is subject to a decision by Council on or before August 7, 2025, which is 90 days after a complete application was received. In the absence of a decision by Council in this timeframe, the applicant may exercise their right to

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appeal to the Ontario Land Tribunal (OLT). The application has exceeded this timeframe as the applicants have been working to satisfy all technical review comments.

Site Characteristics

The subject lands are located at the northwest corner of Compton Street and Wilson Street, within the Rideau Heights area of Kingston (Exhibit B – Key Map). The site has an area of approximately 5700 square metres with 118 metres of frontage onto the north of Compton Street and 22 metres of frontage onto the west of Wilson Street. The property is currently vacant and undeveloped, following the demolition of the former Oakwood Daycare in 2015.

The surrounding lands are developed with a variety of residential, open space, institutional and commercial land uses. Directly to the south and west of the subject property are a number of residential townhouses, including a number of RGI units owned by the Kingston and Frontenac Housing Corporation. Additional residential properties abut the property to the north, along Conacher Drive. Three public parks are located in walking distance to the property: Headway Park directly to the south, Shannon Park slightly further to the south-west, and Markers Acres Park to the north. To the south-east of the subject lands, a mix of commercial and institutional uses are provided at the plaza located at 263 Weller Avenue (Exhibit C – Neighbourhood Context Map).

A Kingston Hydro substation is located directly adjacent to the north-east of the subject property. Two hydro easements associated with this substation are located on the subject lands, as shown on the Concept Plan for this development (Exhibit H – Concept Plan).

The site is also located in walking distance to several transit stops which are serviced by Kingston Transit Routes 1, 2, and 7. In terms of pedestrian infrastructure, both sides of Compton Street and Wilson Street are developed with sidewalks.

The subject property is currently designated as Residential in the Official Plan and zoned as Urban Multi-Residential Zone 4 (URM4) in the Kingston Zoning By-law. In addition to the URM4 zone, the property is also subject to Legacy Exception Overlay L466 which was established as part of the planning approvals associated with the Rideau Heights Regeneration strategy.

Proposed Application and Submission

The proposed Zoning By-law Amendment (ZBA) is proposed in order to establish permitted uses and appropriate zoning standards which will facilitate the redevelopment of the vacant property with a total of 40 residential units. The entire development is comprised of four blocks (Block A, Block B, Block C, and Block D) as shown on the Concept Plan (see Exhibit H – Concept Plan).

Block A and Block B will be developed as separate stacked townhouse buildings, at a height of three and a half storeys. Each of these stacked townhouse buildings will contain 16 units. The floor plans for the stacked townhouse buildings are still preliminary at this time, however, the applicant has indicated that the units within these multi-residential buildings will be a mix of two-bedroom and three-bedroom units.

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A parking lot for the two stacked townhouse buildings is planned immediately to the west of the buildings. The applicant has proposed a parking ratio of one parking space per unit. As such, the parking lot is anticipated to include 32 parking spaces, including two accessible parking spaces. In addition to these, two visitor parking spaces, one car-share space, and one short-term delivery parking space will be provided as part of the lot.

A snow storage area is proposed at the northern end of the parking lot. An outdoor garbage storage area would be provided in the western edge of the parking lot, screened by a 1.8-metre-tall fence and accessed by a pedestrian pathway which cuts through the center of the parking lot.

A total of 35 bike parking spaces have been proposed for the stacked townhouses including 29 long-term bike parking spaces and 6 short-term bike parking spaces. Twenty-one long-term horizontal bike parking spaces and a bike maintenance area will be provided in a secure, enclosed bike storage building located on the west side of the site between the two stacked townhouse buildings. Nine additional long-term bike parking spaces will be provided as vertical bicycle storage spaces within the ground-floor stacked townhouse units. The 6 short-term bike parking spaces will be provided in outdoor bike racks.

Amenity areas for the stacked townhouses will be provided in the form of a common exterior amenity area, located between the two buildings. Each unit will also have either a private balcony or an exterior patio area for at-grade units.

Approximately 39% of the site will consist of landscaped open space. Landscaping will include pathway connections to existing sidewalk infrastructure along Compton Street, general landscaped areas, and exterior amenity space. The proposal is anticipated to require the removal of private trees on the site. Forestry staff have reviewed the proposal and indicated that a Tree Inventory, Tree Planting Plan, and Tree Preservation Plan should be provided at the Site Plan Control stage. Forestry staff have identified that existing tree coverage on the property will need to be replaced as part of the redevelopment. The submitted Tree Planting Plan should include large shade trees to replace any canopy loss resulting from the proposed development.

New Canada Post mailboxes will also be provided on the west side of the site, abutting the Block A stacked townhouse.

A future Part Lot Control exemption application will be required to facilitate the land division that is required to accommodate the development of freehold townhouses on Blocks C and D. The applicant is proposing to develop the eastern portion of the property with two rows of two-storey townhouses which will accommodate a total of 8 individual units with driveway entrances along Compton Street.

In support of the application, the applicant has submitted the following:

- Concept Plan and a supplemental revision
- Planning Justification Report
- Preliminary floor plans and architectural drawings

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- Bicycle storage floor plans
- Reference Plan
- Noise Impact Study
- Preliminary Site Servicing and Stormwater Management Report; and
- Owner Authorization Form

All submission materials are available online through the Development and Services Hub (DASH) at the following link, [DASH](#), using “Look-up a Specific Address”. If there are multiple addresses, search one address at a time, or submission materials may also be found by searching the file number.

Provincial Policy Statement

The Provincial Planning Statement (2024) provides policy direction on matters of provincial interest related to land use planning and development, which are intended to be complemented by local policies addressing local interests. The PPS recognizes Ontario as a vast, fast-growing province and directs municipalities to build homes that respond to changing market needs and local demand. The PPS recognizes the need for compact, transit-supportive designs and finding ways to optimize investments in infrastructure and public service facilities.

Section 2.2 directs Planning authorities to provide for an appropriate range and mix of housing options and densities that meet the projected needs of current and future residents. The PPS also indicates through Section 2.3.1 that settlement areas should be focus of growth and development.

The proposed development would introduce a mix of new residential uses to an underutilized lot within an existing residential neighbourhood. The proposed density of 70 dwelling units per net hectare will exceed the minimum target identified through the Official Plan. The site’s proximity to active transportation and transit infrastructure along Compton Street encourages the use of multi-modal transportation to access the site. The proposed development is also anticipated to be accommodated by existing municipal water services and municipal sewers.

The development has been proposed in a compact form that utilizes existing land and infrastructure resources within the urban boundary. Based on the above, the proposed Zoning By-Law amendment is consistent with the Provincial Planning Statement. A detailed review of the applicable policies is attached in Exhibit D.

Official Plan Considerations

The subject property is designated Residential in the Official Plan and located within a Housing District as shown on Schedule 2 - City Structure. As per Section 2.6.4 of the Official Plan, new low-rise residential buildings are appropriate in existing low-rise residential areas. Housing Districts are planned to continue to mature and adapt while ensuring land use compatibility can be maintained.

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The proposed development of two 3.5 storey stacked townhouses, and two rows of 2-storey townhouses is considered a low-rise development. The proposed height and orientation of the four blocks, with the taller buildings pushed further back into the site, is compatible with the surrounding neighbourhood. The development provides pedestrian pathway connections across the site to existing active transportation and transit infrastructure along Compton Street. Alongside the provision of on-site parking and bike parking options, the development encourages the use of multi-modal transportation to access the site.

There are no known natural or cultural heritage resources on or in proximity to the site. The proposed stacked townhouses are appropriately setback from the adjacent residential property at 41 Compton Street, with proposed walkways and an existing parking area on the adjacent site, providing separation from the adjacent property. The proposal is not anticipated to create any negative shadowing, overlook, or visual intrusion. The redevelopment and intensification of an underutilized site within the urban boundary is encouraged by the Official Plan. The built form is appropriately scaled to be compatible with the surrounding low-rise residential neighbourhood.

Further landscaping buffering considerations will be evaluated through the Site Plan Control process. Feasibility has been demonstrated for noise and stormwater services. The applicant will be required to obtain a partial hold removal for the Municipal Servicing Allocation Policy Overlay to secure servicing capacity for the site.

A detailed review of the applicable policies is attached in Exhibit F.

Zoning By-Law Discussion

The property is subject to Urban Multi-Residential Zone 4 (URM4) and Legacy Exception L466. This application proposes to introduce a site-specific exception overlay to permit the proposed stacked townhouse use and establish site-specific performance standards related to both the stacked townhouses and townhouse dwellings. The URM4 parent zone will be maintained and provisions established under the Legacy Exception will be altered or transferred into the Exception Overlay E206.

Permitted Uses

This application proposes to introduce stacked townhouses as a permitted use on the subject property. The property's URM4 zoning is intended to accommodate multi-family residential development as well as a variety of institutional uses. The permitted residential uses include apartment buildings, houses, and townhouses. Stacked townhouses are not permitted within the URM4 zone as of right where they will be sold individually.

The proposed development seeks to introduce a mix of townhouses and stacked townhouses on a lot that exceeds the minimum lot area requirement. Permission to introduce stacked townhouses as a permitted use will contribute to a mix of housing options to satisfy the diverse needs of the neighbourhood at a height and density that remains compatible with surrounding low-rise residential uses.

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Lot Area & Lot Frontage

As stacked townhouses are not a contemplated use within the URM4 zone, a minimum lot area and minimum lot frontage requirement has not been established. The application proposes to establish a minimum lot area of 1920 square metres and a minimum lot frontage of 18 metres for stacked townhouses. The application proposes to establish a minimum lot area of 165 square metres and a minimum lot frontage of 6 metres for townhouses.

The requested changes are required to acknowledge the existing site conditions and facilitate the future land division of the freehold townhouses onto individual lots through a subsequent Part Lot Control Exemption application. The property has a total lot area of approximately 5717 square metres with approximately 124 metres of lot frontage on Compton Street and approximately 27 metres of lot frontage on Wilson Street.

The intent of the minimum lot area provision is to ensure a functional building envelope can be established and all requirements to support the use can be accommodated on site. The proposed lot area for the stacked townhouses seeks to recognize the proposed orientation of one stacked townhouse building behind the other, recognizing the need for an appropriate amount of space to meet parking, setback, landscaped open space, and walkway requirements on the site. The proposed lot area for the townhouses recognizes for a more compact built form that respects the setback, landscaping, and site functionality requirements established through this application. The proposed changes to the minimum lot area requirements will allow for an appropriately sized building envelope to be established.

The intent of the minimum lot frontage provision is to ensure adequate space for utility infrastructure and functional access to individual lots. 33 Compton Street's larger lot size allows for greater flexibility in accommodating stacked townhouses and townhouses in a functional and compatible manner. The proposed lot frontages will not impede site access or utility connections. The proposed townhouse frontages will allow for functional individual driveways that meet the minimum width requirement and 1.2-metre-wide walkways to be accommodate on site. The proposed lot frontage for the stacked townhouses will also be able to accommodate a functional drive aisle and 1.8-metre-wide sidewalk connecting to existing sidewalk infrastructure on Compton Street.

Front Setback

The proposal involves a front setback reduction from 7.5 metres to 4.1 metres for stacked townhouses and a front setback reduction from 7.5 metres to 6.2 metres for townhouses. The intent of the front setback provision is to maintain adequate landscaped open space, allow for a consistent streetwall, accommodate front yard amenities such as driveway and walkway areas, and prevent encroachment into the City's right-of-way.

As stacked townhouses are not permitted within the URM4 zone and, as such, it is worth noting that the established standard of 7.5 metres was not created with this use in mind. The proposed stacked townhouses will be oriented with entrances on the eastern and western sides of the building with pedestrian pathways and a drive aisle connecting to Compton Street. The

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proposed front setback reduction will still allow a functional drive aisle and 1.8-metre-wide walkway to be established along Compton Street, connecting to unit entrances and a parking area located further back into the property.

The proposed front setback reduction from 7.5 metres to 6.1 metres for the townhouses is minor in nature and is not anticipated to create an inconsistent streetwall that detracts from the neighbourhood character. The proposed setback will allow the proposed development to have a more prominent presence along Compton Street. The reduced setback will not impede front yard amenities from being accommodated in a functional manner including individual driveways and 1.2-metre-wide walkways providing access from each unit to Compton Street.

The property will be able to meet and exceed the landscaped open space requirements with 39% landscaped open space provided on the lot. Encroachment into the City's right of way is not anticipated.

Maximum Density

The proposal requires relief to permit an increase in density on the site from 60 dwelling units per net hectare, permitted by Legacy Exception L466, to 70 dwelling units per net hectare. The purpose of the maximum density provision is to ensure available servicing and infrastructure to accommodate an increase in residents on a site and to prevent overdevelopment.

The Preliminary Site Servicing and Stormwater Management Report (March 2025) indicates an intent and capacity to connect to existing municipal servicing. Utilities Kingston has reviewed the application and not raised any concerns with the site's water and wastewater capacity. An updated servicing plan, gas load summary, and water meter specifications will be reviewed at the Site Plan Control stage. The applicant will also be required to submit a Partial Hold Removal application as required by the Municipal Servicing Allocation Policy to secure servicing capacity prior to building permit issuance.

The proposed development will remain compatible with the immediate neighbourhood context. The proposal has been reviewed by Transportation Staff, and it was determined that the addition of 40 units is not anticipated to result in negative traffic impacts. The site is located within proximity to active transportation and transit infrastructure, encouraging the use of multi-modal transportation to access the site. The proposed density will be accommodated at an appropriate height and built form that is consistent with surrounding low-rise neighbourhood and provides a mix of housing options.

Stacked Townhouses- Amenity Area

The proposal involves a reduction in amenity space from 18.5 square metres per dwelling unit to 16 square metres per dwelling unit. The intent of the minimum amenity area provision is to ensure adequate space to serve the common leisure and recreation needs of residents in multi-residential buildings while also contributing to landscaped open space on the subject property.

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The proposed reduction in amenity area is not anticipated to detract from the ability of residents to access leisure and recreational opportunities. Proposed amenity area on the property includes an exterior common area (210 square metres) and private balconies and patios, split between the stacked townhouse units (303 square metres). Staff have noted recommendations for provision of lighting, outdoor seating, and tree planting within the communal area that will be further reviewed through the Site Plan Control process.

The property is also located within close proximity to Headway Park, Shannon Park, and Markers Acres Park which include well-used amenities such as a toboggan hill in Headway Park and a splash pad in Shannon Park. The Rideau Heights Regeneration Strategy identifies future plans for the improvements to both Headway Park and Shannon Park through the integration of improved recreational amenities.

The amenity area reduction will also not impede the ability of the property to meet and exceed landscaped open space requirements. The proposal will accommodate 39% landscaped open space on site consisting of general landscaping, amenity areas, and pedestrian walkways. Landscaping details will be reviewed through the Site Plan Control process.

Stacked Townhouses- Minimum Drive Aisle Width

The proposal requires relief to permit a reduced minimum drive aisle width from 6.7 metres to 6.0 metres. Under Section 7.4.1 of the Kingston Zoning By-Law, for parking spaces accessed perpendicular to a drive aisle, the drive aisle is required to be 6.7 metres for the portion of the drive aisle used to access a parking space and 6.0 metres for any internal circulation road that is not directly used to access a parking space.

The intent of the minimum drive aisle width provision is to provide additional space to accommodate the turning radius required to pull in and out of parking spaces. The proposed parking area will integrate a two-way 6.0-metre-wide drive aisle providing direct access to perpendicular parking spaces. No internal circulation roads are proposed. The proposed reduction in drive aisle width is not anticipated to negatively impact the functionality of the parking area.

The existing Legacy Exception L466 permits reduced standard parking space lengths of 2.6 metres wide and 5.2 metres long. The proposal does not intend to carry this provision over to the new Exception Overlay and has proposed the standard 2.6 metre wide and 5.5-metre-long parking spaces, identified within the Kingston Zoning By-Law. The proposal also integrates the required 1.5-metre-wide turnaround area at the rear of the parking area to assist with navigating in and out of the parking lot.

Stacked Townhouses- Long Term Bike Parking Location

The proposal requires relief to permit 8 long-term bicycle parking spaces to be located within ground-floor stacked townhouse units. The Kingston Zoning By-law does not permit bicycle parking spaces to be located within the dwelling units of new residential buildings. The intent of

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this provision is to mitigate accessibility challenges that may accompany the storage of bicycles in units that require stairs or elevators to access.

The proposed relief will only apply to ground floor stacked townhouse units which do not require access via stairs. A dedicated bike storage closet will be provided within ground floor units to accommodate vertical storage. Ground floor units have direct access by way of a concrete sidewalk to Compton Street, ensuring unimpeded access from the street to bike storage options. The proposed relief will also allow a smaller bike storage building to be provided externally, freeing up additional landscaped open space on the site.

Stacked Townhouses- Balconies

Relief is requested to permit an increased balcony coverage along the horizontal length of the main walls of the second floor of the stacked townhouse buildings. The proposal requests 100% balcony coverage along the east-facing wall and 50% balcony coverage along the west-facing wall. Relief is requested to accommodate external amenity space for second-floor units.

The intent of the maximum permitted coverage provision is to mitigate against intrusive overlook onto adjacent residential properties. The east wall of the stacked townhouses will be facing the parking lot of the Kingston Hydro substation, and the west wall faces the adjacent residential property at 41 Compton Street, which along this property line is mostly a parking lot area and the side wall of a residential building. As such, the increase in coverage of the east wall is not anticipated to result in any negative privacy impacts. The increase in coverage on the west wall from 45% to 50% is minor in nature and is not anticipated to be a significant change that will impact the ability of adjacent residential property and the ability of residents to enjoy their exterior amenity space. The increase in permitted coverage from 45% will also allow for additional private amenity area to be provided in a way that does not detract from available landscaped open space on site.

Stacked Townhouses- Other

The minimum interior setback, maximum height, and minimum parking ratio, minimum width of horizontal bike parking spaces provisions of Legacy Exception 466 will be carried over to the new Exception Overlay. The proposed stacked townhouses will be able to comply with these requirements.

Townhouses- Rear Setback

The proposal involves a reduction of the rear setback from 7.5 metres to 6.0 metres for the proposed townhouses. The intent of the minimum rear setback requirement is to mitigate against intrusive overlook on adjacent properties, ensure an appropriate amount of amenity space can be provided and maintain landscaped open space on site.

The proposed reduction of the rear setback is not anticipated to result in any intrusive overlook concerns. The proposed townhouses will be located on the east side of the property, abutting the Kingston Hydro substation which is set back 25 metres from the shared lot line. A retaining

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wall has also been proposed along a portion of the rear lot line, providing some screening from the adjacent property.

The proposed development will be able to meet and exceed the 30% landscaped open space requirement with a rear yard amenity area available to each townhouse unit. There is no minimum amenity area requirement for freehold townhouses.

Townhouses- Exterior Setback

The proposal involves a reduction of the exterior setback requirement applicable to townhouses from 7.5 metres to 4.0 metres. The intent of the minimum exterior setback provision is to maintain an appropriate amount of landscaped open space and avoid encroachment into the sight triangle and City's right of way.

One proposed townhouse unit is located at the corner of Wilson Street and Compton Street. The unit is setback 4.29 metres along the length of the exterior wall with a 4.0 metre setback at the corner of the front porch. The exterior setback reduction will not result in development within the required sight triangle. As the lot lines along Compton Street and Wilson Street do not intersect at a single point, the sight triangle is determined by measuring the hypothetical point of intersection at the extension of the front lot line and exterior lot line. As such, a sight triangle with 4.5 metres along Wilson Street and 4.5 metres along Compton Street can be protected without encroachment. No encroachment into the City's right of way is anticipated to result from the proposed reduction.

The site will be able to meet and exceed the landscaped open space requirement with the proposed reduction. Functional rear yard access can also be maintained with the proposed reduction.

Townhouses- Maximum Driveway Width

The proposal involves an increase in the maximum permitted driveway width for townhouses from 'the lesser of 6.0 metres or 40% of the length of the applicable street line' to 'the lesser of 6.0 metres or 65% of the applicable street line', provided that the minimum driveway width is 2.6 metres. Given the proposed minimum lot frontage of 6.0 metres, this would mean a maximum permitted driveway width of 3.9 metres for the smallest lots.

The submitted concept plan indicates proposed driveway widths of 3.68 metres for Block C townhouses and 3.81 metres for Block D townhouses. The requested increase will assist with accommodate future lot division through a subsequent part lot control application of townhouses into compact lots.

The intent of the maximum driveway width provision is to ensure that the streetscape and neighbourhood character will not be adversely affected, boulevard trees can be planted, and street parking remains viable. The driveway widening will still allow the property to exceed the required 30% landscaped open space on site. The additional increase in driveway width is minor

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in nature and is not anticipated to set an undesirable precedent for the neighbourhood, given that the 6.0 metre width cap is being maintained.

Long-term street parking is not available along Compton Street, as street parking is limited to 12 consecutive hours. As such, the proposed extension is not anticipated to detract from the availability of long-term parking options. Opportunities for increased planting will be evaluated at the Site Plan Control stage.

Table 1:

A detailed summary of the proposed changes to the zoning provisions, along with a rationale for each change, is provided above. See the table below for a full list of proposed zoning amendments to be provided as part of the new Exception Overlay E206 applicable to the subject property.

Provision	Requirement	Proposed E206 Exception Overlay	Same as Legacy Exception L466?
Table 12.1.2 Permitted Uses - URM4 Zone	<ul style="list-style-type: none"> • Apartment building • House • Townhouse • Community center • Day care center • Elementary school • Library • Museum • Place of worship 	In addition to the uses permitted by the applicable zone, a stacked townhouse is a permitted use.	No
Provisions applicable to stacked townhouses :			
Table 12.5.1.1 Minimum lot area (square metres)	a) House: 335.0 per lot b) Semi-detached house, townhouse: 270.0 per lot	1,920.0	No
Table 12.5.1.2	18.0	18.0	No

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Minimum lot frontage (metres)			
Table 12.5.1.4 Minimum front setback (metres)	7.5	4.1	No
Table 12.5.1.5 Minimum rear setback (metres)	7.5	6.0	No
Table 12.5.1.7 Minimum interior setback (metres)	d) 5.4	4.0	Yes
Table 12.5.1.3 Maximum height (storeys)	N/A	4 storeys	Yes
Section 4.3.1 Amenity Area per residential unit (square metres)	18.5	16.0	No
Table 7.4.1 Minimum drive aisle width (metres)	6.7	6.0	No
Table 7.1.1.1 Minimum parking ratio	PA4: Minimum 0.8 to a maximum of 1.0 per dwelling unit	Minimum parking ratio is 0.7 parking spaces per dwelling unit.	Yes

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Section 7.3 Long term bike parking location	No specific applicable zoning provision.	A maximum of 8 long-term bike spaces are permitted within the interior of the stacked townhouse units for the ground floor only.	No
Section 4.20.1.2 Balconies	A maximum of 45% of the length of each face of the main wall of each storey may be occupied by balconies.	100% of the horizontal length of the face of the eastern wall of each storey may be occupied by balconies. 50% of the horizontal length of the face of the western wall of each storey may be occupied by balconies.	No
Provisions applicable to townhouses :			
Minimum lot area (square metres)	Table 12.5.1.1 b) Semi-detached house, townhouse: 270.0 per lot	165	No
Minimum lot frontage (metres)	Table 12.5.1.2 18.0	6.0	No
Minimum front setback (metres)	Table 12.5.1.4 7.5	6.2	No
Minimum rear setback (metres)	Table 12.5.1.5 7.5	6.0	No
Minimum exterior setback (metres)	Table 12.5.1.6 7.5	4.0	No
Maximum driveway width	Section 7.4.8.5 The maximum width of a driveway is the lesser of: a) 6.0 metres; or b) 40% of the length of the street line	The maximum width of a driveway is 65% of the length of the street line frontage.	No

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Other provisions:			
Minimum width of horizontal bike spaces	Section 7.3.19.1 Must have minimum dimensions of 0.6 metres wide and 1.8 metres long with a minimum vertical clearance of 1.9 metres.	Minimum size of a horizontal bike space is 0.3 metres wide and 1.8 metres long with 1.9 metres of vertical clearance.	Yes
Maximum density		70 dwelling units per net hectare. For the purpose of calculating density, all lands subject to Exception E206 shall be treated as one lot	No

Other Applications

The proposed development will require a Site Plan Control application, Part Lot Control application and Partial Hold Removal application. A Plan of Condominium application will be required if the stacked townhouses will be condo units.

Technical Analysis

This application has been circulated to external agencies and internal departments for review and comment. All comments on the proposal have been addressed and no outstanding issues with this application remain at this time.

Public Comments

The following is a summary of the public input received to date, including a summary of the feedback received at the Community Meeting on June 19, 2025.. All original written public comments are available in Exhibit K of this report.

Comment: Concern that proposal is an overbuild in terms of density.

Response:

The site has a total lot area of approximately 0.57 hectares. The proposed density of 70 dwelling units per net hectare would equate to the development of 40 dwelling units. The density will be split between two 16-unit stacked townhouse buildings and 8 freehold townhouses. Site parking will be split between individual driveways for the freehold townhouses and a central parking lot for the stacked townhouses. Given the site’s proximity to transit and active transportation infrastructure, multi-modal transportation will be encouraged to access the site.

September 18, 2025

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Comment: Concern for privacy of nearby townhouses

Response:

Planning Staff are not anticipating negative privacy impacts from the proposed stacked townhouses on the adjacent residential properties at 16 Compton Street and 41 Compton Street. The application proposes a 4.0 metre interior setback and a 4.1 metre front setback for the stacked townhouses. The proposed retaining wall, pathway and adjacent parking lot will provide an appropriate degree of separation from the adjacent residential property. Second-floor balconies will also be restricted to 50% coverage on the western wall to mitigate against intrusive overlook. Opportunities to introduce landscaping along the western lot line will be explored through the Site Plan Control process.

Comment: Concern with increased traffic due to number of children playing nearby

Response:

Transportation Staff have provided clarification that a Traffic Impact Study (TIS) is requested when 100+ trips are anticipated in either the morning or afternoon peak period. A TIS may also be requested when there are concerns related to roadways, intersection or access in general. For the proposed development, 100+ trips were not anticipated during peak periods and Transportation did not express concerns related to existing transportation infrastructure in the immediate area as a result of the development.

Planning Staff have requested additional information on whether there are any planned traffic calming measures or pedestrian connections across this portion of Compton Street. Transportation Staff have indicated that there are not immediate plans for new crossings in this area but would consider these recommendations as part of the Fire Lane road reconstruction project.

Effect of Public Input on Draft By-Law

Staff have reviewed comments submitted by members of the public, as above. Comments related to privacy and density have been carefully reviewed and staff are of the opinion that the proposed density and setbacks are appropriate for the site, as indicated within this report. Concerns related to traffic have been reviewed in consultation with the City's transportation department.

Conclusion

The proposal represents a compatible residential development at an appropriate height and density for the surrounding low-rise neighbourhood context. The development supports Council's goals related to intensification within the urban boundary while encouraging the use of multi-modal transportation options. The proposed zoning by-law amendment is consistent with the Provincial Policy Statement, conforms to the relevant policies of the Official Plan and represents good land use planning within the public interest.

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Existing Policy/By-Law:

The proposed amendment was reviewed against the policies of the Province of Ontario and City of Kingston to ensure that the changes would be consistent with the Province's and the City's vision of development. The following documents were assessed:

Provincial*Planning Act*

Provincial Planning Statement, 2024

Municipal

City of Kingston Official Plan

Zoning By-Law Number 2022-62

Notice Provisions:

Pursuant to the requirements of the *Planning Act*, notice of the statutory public meeting was provided 20 days in advance of the public meeting in the form of a sign posted on the subject property and by mail to 81 property owners (according to the latest Assessment Rolls) within 120 metres of the subject property. In addition, a courtesy notice placed in The Kingston Whig-Standard on August 29, 2025.

If the application is approved, a Notice of Passing will be circulated in accordance with the provisions of the *Planning Act*.

At the time of writing of this report, no pieces of written public correspondence have been received, and all planning related matters have been addressed within the body of this report. Any public correspondence received after the publishing of this report will be included as an addendum to the Planning Committee agenda.

Climate Risk Considerations

The proposed development will add additional housing units within the City in a walkable and transit-oriented area. Vehicle and bike parking is being provided in accordance with the provisions of the Kingston Zoning By-law.

The development of the site would require storm water management controls introducing additional water features to the area and tree re-planting will be reviewed through the future site plan control application.

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Indigenization, Inclusion, Diversity, Equity & Accessibility (IIDEA) Considerations

The stacked townhouse portion of the site will be subject to a future Site Plan Control application. Through technical review of the Site Plan Control application, compliance with the accessible parking requirements of the Kingston Zoning By-Law 2022-62 will be confirmed. No reduction in accessible parking requirements are proposed as part of the current application. The site layout will also be reviewed by the Municipal Accessibility Advisory Committee during technical review the Site Plan Control application.

Financial Considerations:

None

Contacts:

James Bar, Manager, Development Approvals, 613-546-4291 extension 3213

Victoria McCutcheon, Planner, 613-546-4291 extension 3237

Other City of Kingston Staff Consulted:

None

Exhibits Attached:

- Exhibit A Draft By-Law and Schedule A to Amend Zoning By-Law Number 2022-62
- Exhibit B Key Map
- Exhibit C Neighbourhood Context
- Exhibit D Consistency with the Provincial Policy Statement
- Exhibit E Official Plan, Land Use Map
- Exhibit F Conformity with the Official Plan
- Exhibit G Zoning By-Law Number 2022-62 Map
- Exhibit H Concept Plan, Floor Plans, and Elevations
- Exhibit I Site Photographs
- Exhibit J Public Notification Map
- Exhibit K Public Comments

September 18, 2025

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By-Law Number 2025-XX

A By-Law to Amend By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (Removal of Legacy Exception ‘L466’ and Introduction of Exception Number ‘E206’ (17 and 33 Compton Street))

Passed: [Meeting Date]

Whereas the Council of The Corporation of the City of Kingston enacted By-Law Number 2022-62, “Kingston Zoning By-Law Number 2022-62” (the “Kingston Zoning By-Law”);

Whereas the Council of The Corporation of the City of Kingston deems it advisable to amend the Kingston Zoning By-Law;

Therefore be it resolved that the Council of The Corporation of the City of Kingston hereby enacts as follows:

1. By-Law Number 2022-62 of The Corporation of the City of Kingston, entitled “Kingston Zoning By-Law Number 2022-62”, is amended as follows:
 - 1.1. Schedule E – Exception Overlay is amended to remove Legacy Exception L466 and to add Exception E206, as shown on Schedule “A” attached to and forming part of this By-Law.
 - 1.2. By adding the following Exception Number E206 in Section 21 – Exceptions, as follows:

“**E206.** Despite anything to the contrary in this By-law, the following provisions apply to the lands subject to this Exception:

 - a) In addition to the **uses** permitted by the applicable Zone, the following **use** is permitted:
 - i. **Stacked townhouse**
 - b) **Stacked townhouses** must comply with the following provisions:
 - i. The minimum **lot area** is 1,920 square metres;
 - ii. The minimum **lot frontage** is 18 metres;
 - iii. The minimum **front setback** is 4.1 metres;
 - iv. The minimum **rear setback** is 6 metres;
 - v. The minimum **interior setback** is 4 metres;
 - vi. The maximum **height** is 4 **storeys**;
 - vii. The minimum **landscaped open space** is 30%;

- viii. A minimum of 16 square metres of **amenity area** is required per **unit**;
 - ix. The minimum width of a **drive aisle** is 6 metres;
 - x. The minimum parking ratio is 0.7 **parking spaces** per **dwelling unit**;
 - xi. A maximum of 8 **long-term bike spaces** are permitted within the interior of the **stacked townhouse units** and are permitted in the ground floor **units** only;
 - xii. 100% of the horizontal length of the face of the eastern wall of each **storey** may be occupied by **balconies**; and
 - xiii. A maximum of 50% of the horizontal length of the face of the western wall of each **storey** may be occupied by **balconies**.
- c) **Townhouses** must comply with the applicable Zone provisions, except:
- i. The minimum **lot area** is 165 square metres;
 - ii. The minimum **lot frontage** is 6 metres;
 - iii. The minimum **front setback** is 6.2 metres;
 - iv. The minimum **rear setback** is 6 metres;
 - v. The minimum **exterior setback** is 4 metres; and
 - vi. The maximum width of a **driveway** is the lesser of:
 - 1. 6.0 metres; or
 - 2. 65% of the length of the applicable **street line** frontage.
- d) The minimum size of a **horizontal bike space** is 0.3 metres wide and 1.8 metres long with 1.9 metres of vertical clearance; and
- e) The maximum **density** is 70 **dwelling units** per net hectare. For the purpose of calculating **density**, all lands subject to Exception E206 shall be treated as one lot.”
2. This By-Law shall come into force in accordance with the provisions of the *Planning Act*.

Given all Three Readings and Passed: [Meeting Date]

Janet Jaynes
City Clerk


Bryan Paterson
Mayor



**Schedule 'A'
to By-Law Number**

Address: 17 and 33 Compton St
File Number: D14-008-2025

**Kingston Zoning By-Law 2022-62
Schedule E - Exception Overlay**

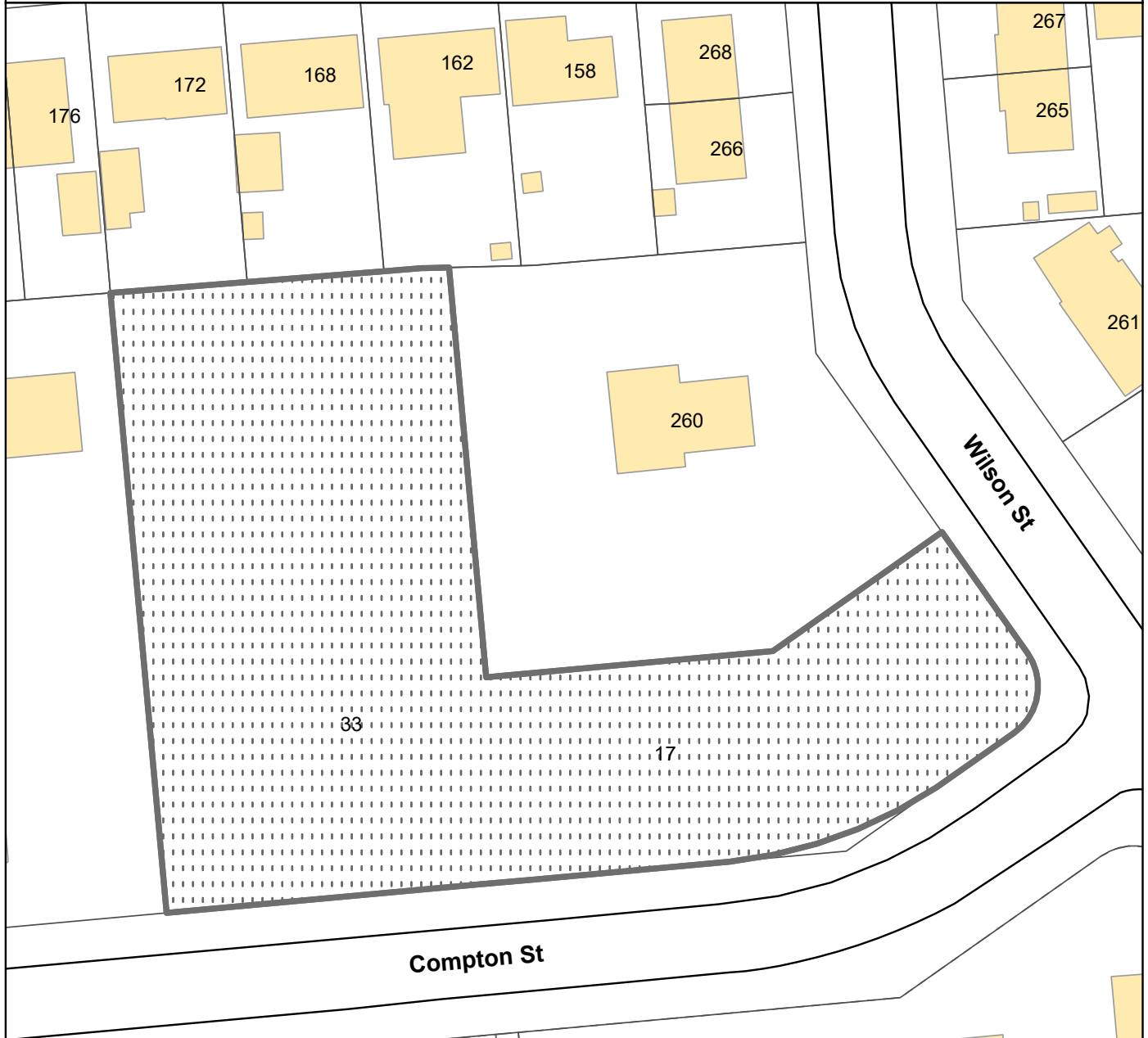
 Lands to be removed from L466
and added as E206

Certificate of Authentication

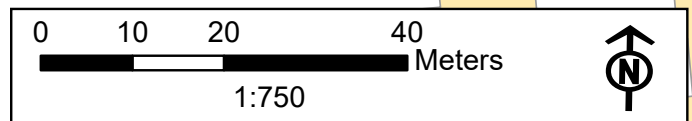
This is Schedule 'A' to By-Law Number _____, passed this _____ day of _____ 2025.

Mayor

Clerk



Prepared By: ncameron
Prepared On: 2025-05-21

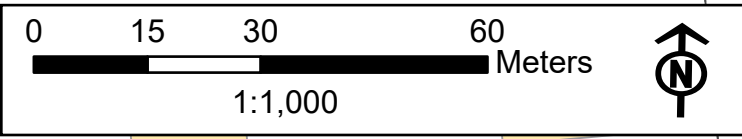
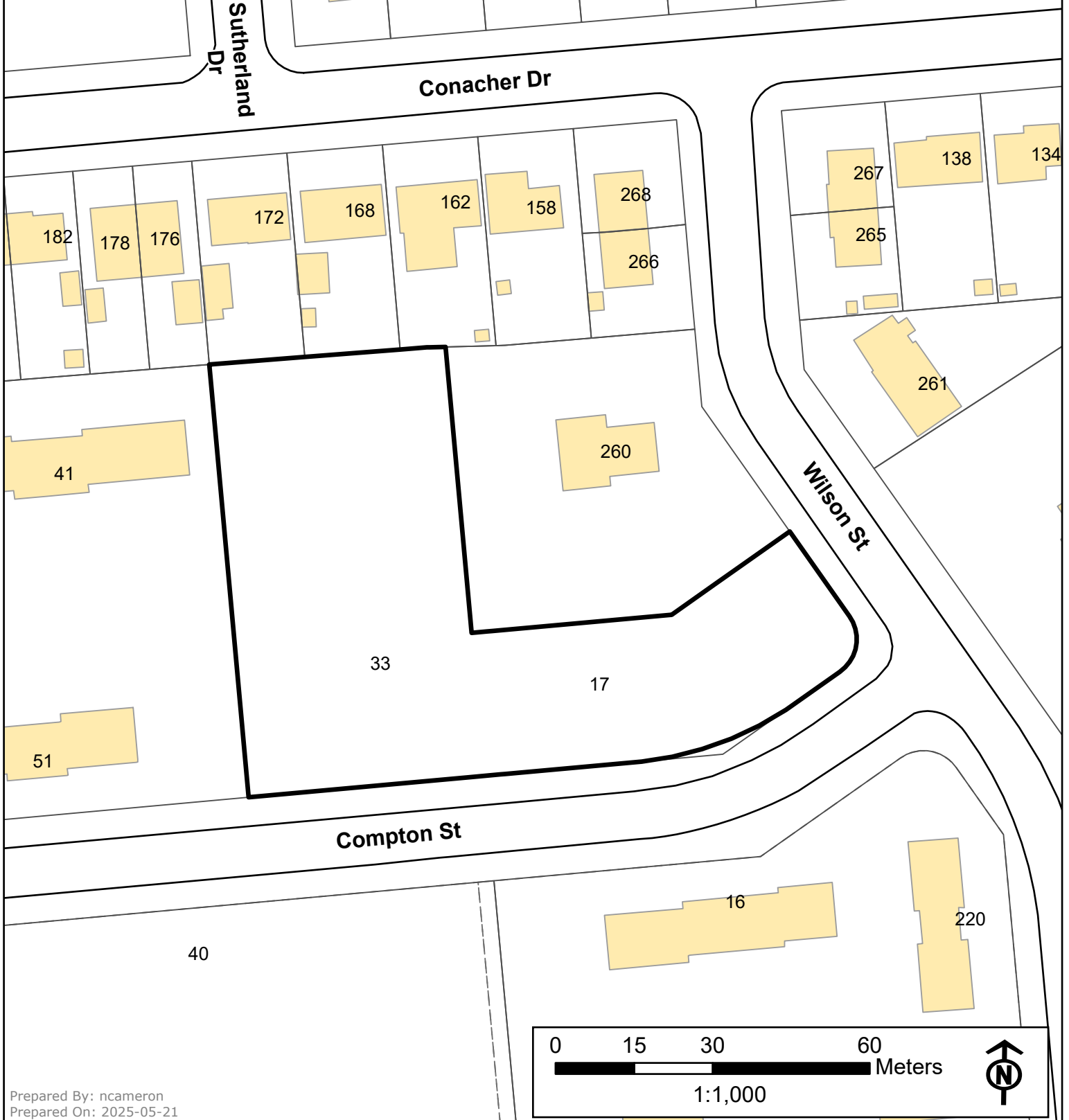
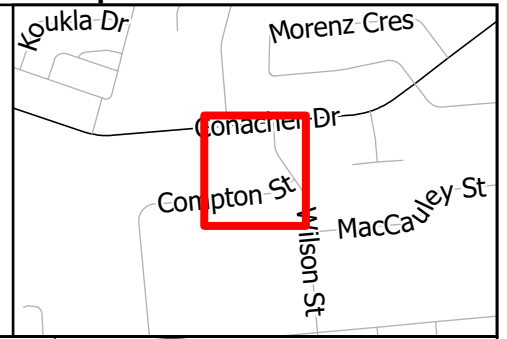


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Planning Committee
Key Map
 Address: 33 Compton Street
 File Number: D14-008-2025

Subject Lands






Prepared By: ncameron
 Prepared On: 2025-05-21

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City of Kingston
Neighbourhood Context
 Address: 33 Compton Street
 File Number: D14-008-2025

-  Subject Lands
-  Property Boundaries
-  Proposed Parcels



Prepared By: ncameron
 Prepared On: 2025-05-21

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Demonstration of How the Proposal is Consistent with the Provincial Planning Statement (2024)

Policy	Commentary
2.1 Planning for People and Homes	
<p>2.1.4 To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:</p> <p>a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and</p> <p>b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.</p>	<p>The proposed development is located on Residential-designated lands within an existing Housing District. The property is zoned Urban Multi-Residential Zone 4 (URM4) which accommodates a variety of multi-residential housing types.</p> <p>The proposed development seeks to introduce an alternative built form that will contribute to the range of housing types options available within the existing neighbourhood. The proposal will be required to connect to existing municipal servicing on Compton Street.</p>
<p>2.1.6 Planning authorities should support the achievement of complete communities by:</p> <p>a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, long-term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;</p> <p>b) improving accessibility for people of all ages and abilities by addressing land</p>	<p>The proposal introduces new residential development to a vacant lot within an existing residential neighbourhood. The development will incorporate a mix of compact housing options including both townhouses and stacked townhouses to support the diverse needs of future residents.</p> <p>The site is located along existing transit routes 1 and 7, with a bus stop located in front of the site. The site is also located within walking distance of nearby open space and institutional uses including Headway Park, Shannon Park, Markers Acres Park, and Rideau Heights Public School. The site can be accessed by a variety of transportation methods including transit and active transportation with bike</p>

Policy	Commentary
<p>use barriers which restrict their full participation in society; and</p> <p>c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.</p>	<p>parking infrastructure and personal vehicle parking available on site.</p> <p>The development will be required to comply with the standards of the Accessibility for Ontarians with Disabilities Act (AODA). The proposal will also be reviewed by the Municipal Accessibility Advisory Committee (MAAC) through the Site Plan Control process. The creation of compact housing forms assists with providing a wider variety of housing options.</p>
2.2 Housing	
<p>2.2.1 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:</p> <p>b) permitting and facilitating:</p> <p>1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and</p> <p>2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;</p> <p>c) promoting densities for new housing which efficiently use land, resources,</p>	<p>See 2.1.6</p> <p>The proposed density of 70 dwelling units per net hectare will be achieved through a mix of townhouses and stacked townhouses to meet the diverse needs of future residents in the area. The proposed density is appropriate for the subject property which has an area of approximately 5717 square metres (0.57 hectares). The proposal seeks to efficiently utilize existing land resources and municipal infrastructure, including water and sanitary services, municipal roads and active transportation infrastructure to support the proposed development.</p>

Policy	Commentary
<p>infrastructure and public service facilities, and support the use of active transportation; and</p> <p>d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations</p>	
2.3 Settlement Areas and Settlement Area Boundary Expansions	
<p>2.3.1</p> <p>1. Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.</p> <p>2. Land use patterns within settlement areas should be based on densities and a mix of land uses which:</p> <p>a) efficiently use land and resources;</p> <p>b) optimize existing and planned infrastructure and public service facilities;</p> <p>c) support active transportation</p> <p>d) are transit-supportive, as appropriate; and</p> <p>3. Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.</p>	<p>The site is located within the Urban Boundary where growth and development are to be focused. The proposal seeks to utilize existing municipal infrastructure and land resources to develop a vacant lot within an existing residential neighbourhood at a compatible density.</p> <p>The site is located in proximity to sidewalks along Compton Street and Wilson Street. The proposal also includes the provision of 29 long-term bike parking spaces and 6 short-term bike parking spaces on site. The site is located along transit Route 1 and 7 with a transit stop in front of the subject property.</p> <p>Through the Official Plan, the City of Kingston has identified a minimum residential density for existing built-up residential areas of 22 dwelling units per net hectare. At a proposed density of 70 dwelling units per net hectare, achieved through a mix of housing options, the proposal exceeds this minimum target.</p>

Policy	Commentary
4. Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.	
2.9 Energy Conservation, Air Quality and Climate Change	
2.9.1 Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that: a) support the achievement of compact, transit-supportive, and complete communities	The proposed development involves the development of a vacant lot located within an identified Housing District. The site is located within close proximity to transit stops along Compton Street and Conacher Drive and sidewalk infrastructure providing alternative means of transportation.
3.1 General Policies for Infrastructure and Public Service Facilities	
3.1.1 Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs. Planning for infrastructure and public service facilities shall be coordinated and integrated with land use planning and growth management so that they: c) are available to meet current and projected needs.	The proposed development is anticipated to be accommodated by existing municipal servicing.
3.6 Sewage, Water, and Stormwater	
3.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.	The proposed development is anticipated to be accommodated by existing municipal servicing with connections to the municipal water main, storm sewer, and sanitary sewer on Compton Street. Utilities Kingston has indicated that wye connections will be required with long sweeps for sanitary services. An updated servicing plan will be provided and reviewed through the Site Plan Control process.

Policy	Commentary
<p>3.6.8 Planning for stormwater management shall:</p> <p>a) be integrated with planning for sewage and water services and ensure that systems are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle;</p> <p>b) minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;</p> <p>c) minimize erosion and changes in water balance including through the use of green infrastructure;</p> <p>d) mitigate risks to human health, safety, property and the environment;</p> <p>e) maximize the extent and function of vegetative and pervious surfaces;</p> <p>f) promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and</p> <p>g) align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale</p>	<p>The proposed development will utilize a flow regulator and connect to the existing stormwater infrastructure along Compton Street. The proposal will restrict stormwater flows to pre-development levels. Stormwater projections included within the submitted report aim to ensure that the development remains functional despite anticipated effects of climate change.</p> <p>The submitted Servicing and Stormwater Management Report (March 2025) has been reviewed by the City's Stormwater Technologists and have confirmed that appropriate stormwater controls have been proposed for the site.</p>
<p>4.1 Natural Heritage</p>	
<p>4.1.1 Natural features and areas shall be protected for the long term.</p>	<p>There are no known natural heritage features on or in proximity to the site.</p>
<p>4.6 Cultural Heritage and Archeology</p>	
<p>4.6.1 Protected heritage property, which may contain built heritage resources or cultural heritage landscapes, shall be conserved.</p>	<p>The subject property has been cleared of archeology.</p>

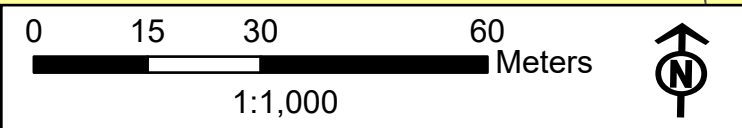
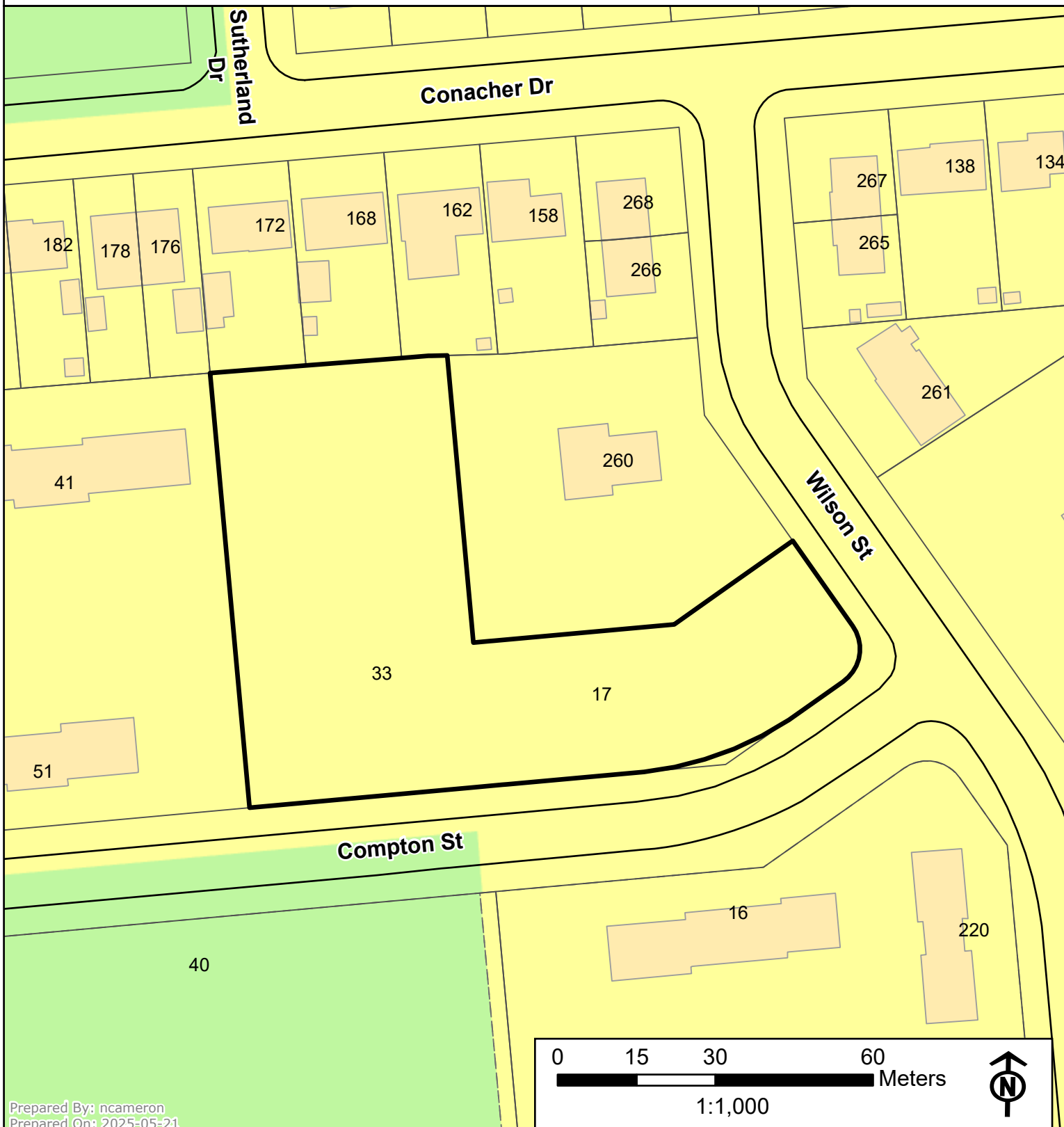
Policy	Commentary
4.6.2 Planning authorities shall not permit development and site alteration on lands containing archaeological resources or areas of archaeological potential unless the significant archaeological resources have been conserved	See Section 4.6.1 above.
4.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property unless the heritage attributes of the protected heritage property will be conserved.	There are no known cultural heritage resources on or in proximity to the site.
5.1 General Policies for Natural and Human-Made Hazards	
5.1.1 Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no known natural or human-made hazards which have been identified on the site.



Planning Committee
Official Plan
Existing Land Use
 Address: 33 Compton Street
 File Number: D14-008-2025

Open Space
 Residential

Subject Lands



Prepared By: ncameron
 Prepared On: 2025-05-21

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Demonstration of Conformity to the Official Plan

Policy	Category	Review
<p>2.1.1 Most growth will occur within the Urban Boundary, shown on Schedule 2, where development will be directed to achieve greater sustainability through:</p> <ul style="list-style-type: none"> a. appropriate (minimum) densities; b. land use patterns that foster transit and active transportation; c. enhanced access to public amenities and spaces for all residents, visitors and workers; d. opportunities for sharing resources such as parking, utilities, and the land base for locally grown produce, in the form of urban agriculture, as well as educational, recreational or cultural assets; e. direction of new development and key land uses to areas where they can best result in sustainable practices; f. promotion of employment opportunities and alliances that enhance local skills, educational resources and the use of local products, including food; 	<p>Strategic Policy Direction, Focus of Growth – Urban Areas</p>	<p>The subject lands are located within the City’s defined urban boundary where additional density is desirable.</p> <p>The application proposes an increase in density to 70 dwelling units per net hectare on a vacant lot within an existing Housing District. The proposed increase in density is proposed in a location that meets the criteria of the Official Plan and will contribute to the neighbourhood in a compatible manner.</p> <p>The subject lands are located in proximity to transit and active transportation infrastructure. There is a transit stop located on Compton Street in front of the subject property, serviced by Transit Routes 1 and 7. The site is also located within walking distance of Conacher Drive, providing access to Transit Route 2.</p> <p>Compton Street has existing sidewalk infrastructure on both sides of the street. The site is within walking distance of the Weller Kingston Community Health Centre as well as other open space and institutional uses including Headway Park, Shannon Park, Markers Acres Park, the Evangel Pentecostal Church and Rideau Heights Public School. These sites can be accessed by existing sidewalk networks along Compton Street, Wilson Street, and Maccauley Street without the need to cross an arterial road. The site is also located within 800 metres of commercial uses.</p>

Policy	Category	Review
<p>g. maximized use of investments in infrastructure and public amenities;</p> <p>h. strategies that will revitalize both neighbourhoods and employment areas, and rehabilitate brownfield sites for re-use;</p> <p>i. parks that are planned to be accessed by urban residents within a ten minute walk and situated in locations that lessen the need for pedestrians to cross an arterial road or major highway;</p> <p>j. where possible, the preservation of mature trees for shade and their other beneficial ecological and community effects;</p> <p>k. climate positive development;</p> <p>l. promotion of green infrastructure to complement infrastructure;</p> <p>m. encouraging a mix of land uses that provide for employment, education, personal service and convenience retail in close proximity to residential land uses, subject to compatibility matters as outlined in Section 2.7; and,</p>		<p>The subject property will be required to connect to existing municipal water and sewer services.</p> <p>The development will incorporate pedestrian pathways that connect to existing sidewalk infrastructure and on-site bike parking facilities to support low-carbon modes of travel. The site does not contain and is not located in proximity to any natural heritage features.</p> <p>The proposal does involve the removal of site trees. Existing tree cover will be required to be replaced as part of the redevelopment. A tree planting plan with the inclusion of large shade trees to replace any canopy loss will be required at the Site Plan Control stage.</p>

Policy	Category	Review
<p>n. an ecosystem approach to protecting the natural heritage system.</p>		
<p>2.1.4 In reviewing development applications, the City will promote sustainability through:</p> <p>a. encouragement of green building design to reduce greenhouse gases by adopting:</p> <ul style="list-style-type: none"> • energy efficient construction; • renewable sources of energy for lighting and heating; • natural lighting; • design that reduces water consumption; • design which minimizes discharge into the sanitary sewers; and • design which reduces or eliminates discharge into the storm sewers through incorporating stormwater management practices including low impact design and stormwater re-use. <p>b. design, landscaping, and streetscaping practices that promote protection from undesirable sun, wind, or other conditions and reduces the negative effects of urban summer heat;</p>	<p>Strategic Policy Direction, Focus of Growth-Development Review</p>	<p>As indicated in the Servicing and Stormwater Management Report (March 2025), the site has been designed to control post-development run-off to pre-development levels through the proposed onsite oversized pipe system and parking lot storage with a Tempest flow regulator device. Peak flows can be attenuated to pre-development flows through this on-site storage. The above-mentioned report has been reviewed by the City’s Stormwater Technicians who have confirmed feasibility to provide adequate stormwater controls for the site to support the proposed development.</p> <p>The proposal includes the provision of 39% landscaped open space which includes green space, concrete sidewalks, and communal amenity areas. Landscaping plans will be provided at the Site Plan Control stage.</p> <p>Existing tree cover on the property will need to be replaced as part of the redevelopment. A Tree Inventory and Tree Preservation Plan will be required at the Site Plan Control stage. Forestry has requested the inclusion of large shade trees to replace any canopy loss.</p> <p>The proposal encourages a multi-modal approach to transportation through the provision of pathways</p>

Policy	Category	Review
<p>c. design, landscaping, and streetscaping practices that reduce the quantity of impermeable surfaces;</p> <p>d. construction and operational practices that minimize waste and maximize re-use of resources;</p> <p>e. practices that conserve or recycle materials, energy, or other resources;</p> <p>f. design which promotes a reduction of automobile trips, active transportation and transit, including secured public access to bicycle storage and parking;</p> <p>g. the creation of a mix of uses that support increased access to healthy foods;</p> <p>h. the use of materials that have been extracted or recovered locally;</p> <p>i. design that reduces municipal costs associated with the provision of infrastructure and municipal service delivery over the long term;</p> <p>j. development that generates sufficient tax revenue to pay for the increased services (e.g., solid waste collection, fire and police services, snow</p>		<p>connecting to existing sidewalk infrastructure and bike parking. The site is located along transit routes and within walking distance of a range of open space, institutional, and commercial uses.</p> <p>The proposal includes a mix of housing types to accommodate diverse housing needs.</p> <p>The subject property is located within the urban boundary and will be connected to existing municipal services.</p>

Policy	Category	Review
<p>clearing, etc.) that the City has to provide; and,</p> <p>k. development that suits the demographic and/or socio-economic needs of the community.</p>		
<p>2.2.4 The Urban Boundary shown by the dashed line on Schedule 2 has been established to recognize the substantially built up areas of the City where major sewer, water and transportation infrastructure has been planned. The land within the Urban Boundary will be the focus of growth and development in the City and contains sufficient land to accommodate the projected growth for a planning horizon of 2036. The Area Specific Phasing area within the Urban Boundary is subject to site-specific urban growth management policies. The Special Planning Area sites are also within the Urban Boundary and are now committed to a substantial land use but could accommodate future growth</p>	<p>City Structure-Urban Boundary</p>	<p>The property is located within the City's defined urban boundary and will be fully serviced by municipal water and sewer services. The proposal will facilitate the development of 40 dwelling units within an existing Housing District.</p>
<p>2.2.5 Housing Districts are planned to continue to mature and adapt as the City evolves in a manner that ensures land use compatibility while supporting the construction of new housing. Re-investment and</p>	<p>City Structure-Housing Districts</p>	<p>The subject lands are located within an existing Housing District. The proposal seeks to facilitate residential development on a vacant lot at a compatible density.</p>

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<p>upgrading will be encouraged through minor infilling and minor development. Housing Districts will be designated for residential uses of different types, but will also contain areas of open space, community facilities and commercial uses.</p>		
<p>2.3.1 The focus of the City's growth will be within the Urban Boundary, shown on Schedule 2, where adequate urban services exist, or can be more efficiently extended in an orderly and phased manner, as established by this Plan. Kingston's Water Master Plan and Sewer Master Plan will guide the implementation of the infrastructure planning</p>	<p>Principles of Growth- Growth Focus</p>	<p>The property is located within the City's defined urban boundary and will be connected to existing municipal services through wyed connections with long sweeps for sanitary services for the stacked townhouses. A Gas Load Summary will be reviewed at the Site Plan Control stage.</p>
<p>2.3.2 In 2013, residential density within the City's Urban Boundary was 25.7 units per net hectare. The City intends to increase the overall net residential and non-residential density within the Urban Boundary through compatible and complementary intensification, the development of underutilized properties and brownfield sites, and through the implementation of area specific policy directives tied to Secondary Planning Areas</p>	<p>Principles of Growth- Intensification</p>	<p>The proposed development represents compatible intensification in a complementary built form within the urban boundary. The proposal seeks to increase residential density on a vacant lot in an existing residential neighbourhood.</p> <p>The development proposes 40 new residential units at a density of 70 dwelling units per net hectare. The two three-and-a-half storey stacked townhouse buildings will each contain 16 units while there will be eight individual two-storey townhouse dwellings.</p>

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and Specific Policy Areas, as illustrated in Schedule 13		
<p>2.3.11 In order to implement the Strategic Direction of the Kingston Transportation Master Plan, active transportation will be aggressively promoted with greater emphasis on pedestrians, cyclists and transit, and accessibility for all residents and visitors.</p>	Principles of Growth-Transportation	<p>The proposal will encourage multi-modal transportation through its proximity to transit stops, pathways connecting to existing sidewalk infrastructure and on-site bicycle parking. The site is also located within walking distance of open spaces, institutional and commercial uses.</p>
<p>2.3.18 Through the prevention and removal of barriers for persons with disabilities, and the application of universal design principles, the City supports and promotes opportunities for all people to access the City and make contributions as citizens. The application of universal design principles in development and renovation is promoted. The City also encourages owners of private properties with public access to do the same.</p>	Principles of Growth-Accessibility	<p>The proposed development will be required to meet accessibility requirements under AODA through the Building Permit review process. Through the Site Plan Control process, the application will also be reviewed by the Municipal Accessibility Advisory Committee (MAAC) as well as Building and Planning staff for compliance with the City's accessibility guidelines.</p> <p>One Type A and one Type B accessible parking space will be provided on the site, in accordance with zoning requirements for accessible parking spaces.</p>
<p>2.4.1 The City supports sustainable development of a compact, efficient, urban area with a mix of land uses and residential unit densities that optimize the efficient use of land in order to:</p> <p>a. reduce infrastructure and public facility costs;</p>	Phasing of Growth-Vision	<p>The proposal will develop a vacant parcel of land within an existing Housing District in the Urban Boundary. The development will be serviced existing municipal infrastructure. The applicant will be required to provide wye connections along Compton Street with long sweeps for sanitary services.</p>

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<p>b. reduce energy consumption and greenhouse gas emissions;</p> <p>c. support active transportation and viable public transit;</p> <p>d. conserve agriculture and natural resources within the City; and</p> <p>e. reduce reliance on private vehicles.</p>		<p>The proposal will increase density at a compatible level of 70 dwelling units per net hectare within an existing neighbourhood. The development encourages the use of multi-modal transportation to access the site through active transportation infrastructure and nearby transit options. The development will introduce bike parking and new pathways to the site connecting to existing sidewalk infrastructure along Compton Street and Wilson Street.</p> <p>The proposal will introduce new housing options to an existing neighbourhood, making efficient use of a vacant parcel of land. No natural heritage features are anticipated to be impacted by the proposed development.</p>
<p>2.4.3 It is the intent of this Plan to achieve an increase in the City's net urban residential densities through promoting intensification and requiring minimum densities for residential development. It is also the intent of the City that intensification be focused within Centres and Corridors and in specific policy areas.</p>	<p>Phasing of Growth-Residential Density</p>	<p>The proposal will contribute to an increase in the City's net urban residential density and exceeds the minimum density established by the Official Plan.</p>
<p>2.4.4 New residential development and new secondary plans are subject to the following policies and minimum densities:</p> <p>a) for the existing built-up residential areas, a net urban</p>	<p>Phasing of Growth-Minimum Residential Density</p>	<p>The proposed density of 70 dwelling units per net hectare (40 units) exceeds the minimum density requirement for an existing built-up area of 22 dwelling units per net hectare.</p>

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<p>residential density of 22 dwelling units per net hectare is established as the overall minimum density, except where specifically increased in subsections (b), (c), and (d) below;</p>		
<p>2.4.5 The City has established the following minimum targets for intensification to occur within the Urban Boundary.</p> <p>a) It is the intent of the City that 40 percent (%) of new residential development occur through intensification.</p>	<p>Phasing of Growth-Intensification Targets</p>	<p>The development constitutes intensification of an underutilized site through the development of two stacked townhouse buildings and eight townhouse dwellings. The development will contribute to the City's goal of 40% of new residential development to occur through intensification.</p>
<p>2.4.6 Urban development within the City will proceed in a planned and orderly manner. The Order of Development will be as follows:</p> <p>a) lands located within the Urban Boundary that have servicing capacity currently in place, including infill opportunities, brownfield sites and other vacant or under-utilized properties have the first priority for development;</p>	<p>Phasing of Growth-Urban Boundary</p>	<p>The subject lands are located within the urban boundary and propose a mix of housing types on an underutilized site. The development will be connected to existing municipal services through wye connections.</p>
<p>2.5.8 Where intensification is encouraged, increased densities will only be approved when it has been determined by the City that servicing capacity exists or that capacity expansions are</p>	<p>Phasing of Municipal Infrastructure and Transportation-Servicing Capacity</p>	<p>A Site Servicing and Stormwater Management Report (March 2025) was submitted by the applicant. It was determined that wye connections will need to be provided by the applicant along Compton Street with long sweeps for sanitary</p>

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<p>imminent to accommodate additional development.</p>		<p>services. Servicing will be further evaluated through the Site Plan Control process.</p> <p>As per the Municipal Servicing Allocation (MSA) Policy, the applicant will be required to obtain a partial hold removal prior to Building Permit issuance to secure servicing capacity.</p>
<p>2.5.11 The use of transit will be supported and encouraged through the development of mixed use areas and mixed use buildings, the development of Corridors and more intense mixed use Centres, and through the increase of densities within newer areas, compatible uses and infill with complementary uses, and appropriate development of under-utilized and brownfield sites.</p>	<p>Phasing of Municipal Infrastructure and Transportation-Strategic Direction to Promote Active Transportation</p>	<p>The subject site is serviced by Transit Routes 1 and 7 with a transit stop located on Compton Street, in front of the site.</p> <p>The development of the underutilized site with a mix of housing options has been proposed at a compatible density that will be appropriate within the existing residential neighbourhood.</p>
<p>2.6.1 Low-rise residential buildings are the predominant form of housing in the City's urban neighbourhood system, both in number of units and physical area. They represent low profile, ground oriented types of housing including, but not limited to, houses, semi-detached dwellings, duplexes, triplexes, rowhouses, townhouses, stacked townhouses and accessory houses. Low-rise buildings</p>	<p>Residential Development- Low-Rise, Mid-Rise, and High-Rise Residential Building Types</p>	<p>The proposed development consists of low-rise housing types including two 3.5 storey stacked townhouse buildings and two rows of two-storey townhouse dwellings. The proposed development will be compatible with the existing low-rise residential area.</p>

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are generally up to 3 or 4 storeys in height.		
2.6.4 New low-rise residential buildings are appropriate in existing low-rise residential areas and in greenfield areas that have been planned for low-rise residential through the policies of this Plan	Residential Development-Appropriate Locations for New Low-Rise, Mid-Rise, and High-Rise Residential	The subject property is located within an existing low-rise residential area primarily consisting of 2-storey residential buildings. The proposed development will be appropriately located within the existing neighbourhood.
2.7.1 Development and/or land use change must demonstrate that the resultant form, function and use of land are compatible with surrounding land uses.	Land Use Compatibility Principles- Compatible Development and Land Use Change	The application demonstrates that the proposed development will be compatible with the surrounding residential neighbourhood, in terms of form and function. A detailed review has been provided under Section 2.7.6.
2.7.2 The demonstration of compatible development and land use change must consider the potential for adverse effects and matters that have the potential to negatively impact the character, planned function and/or ecological integrity of an area, and the health and safety of humans. Where there exists a potential for negative impacts, a land use compatibility study, focused specifically on the identified land use compatibility matters, will be required.	Land Use Compatibility Principles- Compatible Development and Land Use Change	The proposed development will be compatible with surrounding land uses as detailed in Section 2.7.3. A Planning Justification Report and Noise Impact Study were submitted with the application, which speaks to land use compatibility and the potential for adverse effects in conformity with Section 2.7.2.
2.7.3 The land use compatibility matters to be considered under Section	Land Use Compatibility Principles- Land	The proposed development of two 3.5 storey stacked townhouse buildings and 2-storey townhouse dwellings are compatible and consistent with low-rise built form of

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<p>2.7.2 include, but are not limited to:</p> <ul style="list-style-type: none"> a. shadowing; b. loss of privacy due to intrusive overlook; c. increased levels of light pollution, noise, odour, dust or vibration; d. increased and uncomfortable wind speed; e. increased level of traffic that can disrupt the intended function or amenity of a use or area or cause a decrease in the functionality of active transportation or transit; f. environmental damage or degradation; g. diminished service levels because social or physical infrastructure necessary to support a use or area are overloaded; h. reduction in the ability to enjoy a property, or the normal amenity associated with it, including safety and access, outdoor areas, heritage or setting; i. visual intrusion that disrupts the streetscape or buildings; j. degradation of cultural heritage resources; 	<p>Use Compatibility Measures</p>	<p>the existing neighbourhood. The subject site abuts the Kingston Hydro sub-station to the northeast, two-storey townhouses to the west, and one to two-storey single-detached dwellings to the north. The proposed buildings are appropriately setback from all lot lines and are not anticipated to result negative shadowing impacts on the rear yards of adjacent properties. Shadows will be largely contained to the subject lands at an acceptable level.</p> <p>The townhouses and stacked townhouses are not expected to result in a loss of privacy for adjacent residential properties due to intrusive overlook. The stacked townhouses are appropriately setback from the eastern and rear lot lines, with walkways and a concrete toe wall separating the two buildings from the adjacent residential property.</p> <p>The stacked townhouses area proposed to have 100% coverage of the second floor east-facing wall and 50% coverage of the second-floor west wall, facing 41 Compton Street. As the east wall is facing the parking lot of the adjacent property and has been appropriately setback 4.18 metres from the eastern interior lot line, this is not anticipated to create any negative privacy impacts on adjacent residential buildings.</p> <p>There will be short-term impacts associated with site-construction. Once occupied, the proposed use is not anticipated to increase levels of light pollution, noise, odour, dust, or</p>

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<p>k. architectural incompatibility in terms of scale, style, massing and colour; or,</p> <p>l. the loss or impairment of significant views of cultural heritage resources and natural features and areas to residents.</p>		<p>vibration. A Noise Study was submitted as part of the application. The traffic noise impacts and proposed air conditioning systems would meet the NPC-300 noise criteria. The predicted noise impacts from external stationary sources on the development would also meet the NPC-300 noise criteria for a Class 1 Area. The study concluded that noise control measures would not be required for the proposed development.</p> <p>The proposed buildings at heights of 3.5 storeys and 2 storeys are not anticipated to affect wind speed in the area.</p> <p>A Traffic Impact Study was not required for the proposed development. While the proposal seeks to increase maximum residential density on the site, the scale of development is not anticipated to disrupt the functionality of traffic in the area. As the site is in proximity to transit and active transportation infrastructure, a parking ratio of 0.7 parking spaces per dwelling unit has been proposed.</p> <p>There are no known cultural heritage or natural heritage features on or adjacent to the subject site.</p> <p>A preliminary review of the submitted Site Servicing Report by Utilities Kingston has demonstrated that the proposed development will be connected to existing municipal services through wye connections. An updated servicing plan will be</p>

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		<p>reviewed through the Site Plan Control process. The applicant will be required to obtain a partial hold removal prior to Building Permit issuance for Municipal Servicing Allocation to secure their capacity. The site is well-located in proximity to adjacent social infrastructure and physical infrastructure along Compton Street and is not anticipated to result in diminished service levels.</p> <p>As indicated in the Preliminary Site Servicing and Stormwater Management Report, a flow regulator will be used to restrict flows to pre-development levels. Stormwater projections have been provided for 5 and 100-year storm events to mitigate against adverse effects on neighbouring properties from future stormwater events.</p> <p>The site is located within an existing residential neighbourhood, adjacent to a mix of residential and open space uses. The proposed development is not anticipated to negatively impact the ability of surrounding properties to enjoy their properties or disrupt the existing streetscape through visual intrusion. While the proposed development will alter the existing streetscape, the development has been proposed at a compatible height that will be appropriately setback from adjacent lot lines.</p>
<p>2.7.4 Mitigation measures may be used to achieve development and land use</p>	<p>Land Use Compatibility</p>	<p>As indicated in Section 2.7.3, the proposed development will be compatible with the existing</p>

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<p>compatibility. Such measures may include one or more of the following:</p> <ul style="list-style-type: none"> a. ensuring adequate setbacks and minimum yard requirements; b. establishing appropriate transition in building heights, coverage, and massing; c. requiring fencing, walls, or berming to create a visual screen; d. designing the building in a way that minimizes adverse effects; e. maintaining mature vegetation and/or additional new landscaping requirements; f. controlling access locations, driveways, service areas and activity areas; and, g. regulating location, treatment and size of accessory uses and structures, lighting, parking areas, garbage storage facilities and signage 	<p>Principles-Mitigation Measures</p>	<p>residential neighbourhood. The following mitigation measures have been implemented within the proposal:</p> <ul style="list-style-type: none"> • The proposed building heights of 3.5 stories and 2 stories are consistent with the existing low-rise neighbourhood. The taller 3.5 storey buildings have been appropriately positioned further back into the lot while the two-storey buildings will face Compton Street. • The proposed setback reductions will be appropriate for the site, given the low-rise built form. Adjacent rear yards and parking areas provide additional separation from residential uses. • The proposal will involve the removal of trees from the subject property. Through the subsequent Site Plan Control application, the applicant will be required to provide a Landscaping Plan as well as a Tree Inventory, Tree Planting Plan, and Tree Preservation Plan. Forestry have indicated that existing tree cover on the property will need to be replaced as part of the redevelopment. Large shade trees will need to be included within the Tree Planting Plan to replace any canopy loss that occurs from the proposed development. • Site Access will be limited to Compton Street with individual

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		<p>driveway entrances for the townhouses and a larger parking area for the stacked townhouses.</p> <ul style="list-style-type: none"> An outdoor garbage storage area will be provided at the western edge of the parking lot for the stacked townhouses and will be screened by 1.8 metre high fencing, accessible by way of a pedestrian pathway.
<p>2.7.6 Only development proposals that meet the long-term needs of the intended users or occupants will be supported. Proponents, whether developing individual buildings on a single site, or multiple buildings being built at one time or phased over time, will be required to demonstrate to the satisfaction of the City that the functional needs of the occupants or users will be met by providing:</p> <p>a. suitable scale, massing and density in relation to existing built fabric;</p> <p>b. appropriate landscaping that meets or improves the characteristic green space amenity of the site and surroundings and enhances the City's tree planting program;</p> <p>c. adequate land area and appropriate site configuration</p>	<p>Land Use Compatibility Principles- Functional Needs</p>	<p>The proposed density of 70 dwelling units per net hectare exceeds the maximum permitted density within the URM4 zone. As the height and scale of the building is appropriate for the existing low-rise residential neighbourhood and can be accommodate by existing servicing, the proposed density and built form will be compatible for the proposed location.</p> <p>The site will have an appropriate amount of landscaped open space which includes both communal and private amenity areas as well as pathways connecting to existing sidewalk infrastructure on Compton Street. A Landscaping Plan, Tree Preservation Plan, Tree Planting Plan and Tree Inventory will be required to be submitted for review at the Site Plan Control stage.</p> <p>The site is appropriately sized to accommodate the proposed density and building heights without negatively impacting site functionality. Site access has been limited to Compton Street with</p>

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<p>or provision for land assembly, as required;</p> <p>d. efficient use of municipal services, including transit;</p> <p>e. appropriate infill of vacant or under-utilized land; and,</p> <p>f. clearly defined and safe:</p> <ul style="list-style-type: none"> • site access; • pedestrian access to the building and parking spaces; • amenity areas; • building entry; and, • parking and secure and appropriate bicycle facilities. 		<p>pedestrian pathways providing connections to existing sidewalk and transit infrastructure, including a transit stop in front of the site.</p> <p>The proposal involves the development of a vacant lot with low-rise residential uses. The proposed development will connect to existing municipal water and sanitary services.</p> <p>Parking for the stacked townhouses will be provided in a surface parking lot to the east of the buildings while parking for the freehold townhouses will be provided by way of individual driveways on Compton Street. A bicycle storage enclosure for the stacked townhouses will be provided on the west side of the property between the two stacked townhouse buildings with pathway access from Compton Street. Bicycle storage for the freehold townhouses will be provided within individual dwelling units.</p> <p>Individual unit entrances will be provided for the stacked townhouses on the west and east sides of the buildings. The freehold townhouses will have main entrances facing Compton Street.</p> <p>Amenity area for the stacked townhouses will be provided by way of an exterior common area, private balconies and terraces. Private amenity areas for the freehold townhouses will be available in the front and rear yards. Additional detail</p>

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		on amenity spaces will be provided at the Site Plan Control Stage.
<p>3.3.1 The predominant use of land in a Residential designation will be for various forms of housing contemplated by the low-rise, mid-rise and high rise residential policies in Section 2.6. Specific uses are permitted in accordance with Section 3.2.</p>	<p>General Residential Policies- Permitted Uses</p>	<p>The proposed development consists of low-rise residential uses including two 3.5 storey stacked townhouses and two rows of 2-storey townhouses.</p>
<p>3.3.3 New development is encouraged and expected to incorporate “green building features” as recommended in Section 2.1.4 of this Plan, and must comply with the policies of all other sections of this Plan</p>	<p>General Residential Policies- Green Building Design Features</p>	<p>The incorporation of green building design features will be reviewed through the Site Plan Control process.</p>
<p>3.3.5 Sections 2.6 and 2.7 identify the various types of low-rise, mid-rise and high-rise residential building types, including the appropriate locations for each building type and the land use compatibility considerations that must be demonstrated for new low-rise, mid-rise and high-rise residential developments. Where a new residential building is proposed in a Residential designation, it must comply with Sections 2.6 and 2.7, and all other applicable policies of this Plan.</p>	<p>General Residential Policies- New Low-Rise, Mid-Rise and High-Rise Buildings</p>	<p>The proposed development of 3.5-storey stacked townhouses and 2-storey townhouses is appropriate and compatible with the existing low-rise residential neighbourhood. A review of compliance with Sections 2.6 and 2.7 has been provided above.</p>

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<p>4.1.1 New development will proceed only if the City is satisfied that adequate services, roads, and utilities are available, or can be made available, to serve the proposal adequately. In determining the adequacy of servicing, utility systems, or the transportation system, the City will consider not only the proposal, but also the potential for development that exists in the same service area.</p>	<p>Infrastructure and Transportation- New Development</p>	<p>A Preliminary Site Servicing and Stormwater Management Report (March 2025) was submitted by Forefront Engineering. The Report was reviewed by Utilities Kingston and it was determined that wyeed connections along Compton Street will need to be provided by the applicant with long sweeps for sanitary services. The applicant will be required to provide an updated servicing plan, a Gas Load Summary, and Water Meter Sizing Form to be reviewed through the Site Plan Control process.</p> <p>As per the Municipal Servicing Allocation Policy, the applicant will also be required to apply for a partial hold removal for their servicing capacity prior to Building Permit issuance.</p>
<p>4.3.1 Stormwater management techniques must be used in the design and construction of all new development to control both the quantity and quality of stormwater runoff. The degree of control will depend on the conditions in the downstream receiving water bodies. This is to minimize the negative impacts of development on the downstream receiving water bodies, the aquatic environment, and fish habitat.</p>	<p>Stormwater Management- Purpose</p>	<p>The proposed development will utilize a flow regulator to manage stormwater runoff on site. Stormwater controls will be introduced to address quality and quantity on site and ensure that post-development flows to not exceed pre-development levels.</p> <p>Final details related to stormwater management will be secured through Site Plan Control approvals.</p>
<p>4.3.4 For urban infill development projects, the</p>	<p>Stormwater Management-</p>	<p>A Preliminary Stormwater Management Report has been</p>

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<p>City will require the preparation of a stormwater management report to address the impacts of additional lot coverage or new uses of the site on the quality and quantity of water. Proponents must endeavour to improve the management of stormwater from the existing development areas.</p>	<p>Quality and Quantity of Water</p>	<p>provided and reviewed with the Zoning By-Law Amendment application. The City's stormwater technicians have reviewed the report and confirmed that it has demonstrated that it is feasible to provide adequate stormwater controls for the site.</p> <p>Final details related to stormwater management will be secured through Site Plan approvals.</p>
<p>4.6.3 The reconstruction of existing roads and the construction of new roads within settlement areas are to include safe, convenient and accessible pedestrian facilities, such as sidewalks, corner ramps, pedestrian signals and crosswalks of universal design. The enhancement of roadways, sidewalks, sidewalk safety barriers, and transit facilities to maximize mobility and access for all will be required in all construction and reconstruction projects.</p>	<p>Transportation-Pedestrian Facilities</p>	<p>Changes within the right of way such as those related to new access points will need to be constructed to municipal standards.</p> <p>Due to the degree of road cuts on Compton Street, it will be required that the surface asphalt be milled and reinstated from the western limits to the eastern limits of the road works. This will be reviewed through the Site Plan Control process.</p>
<p>4.6.59 Front yard parking in residential areas has the potential to affect the visual amenity of the streetscape and the residential character of a neighbourhood. It is the intention of this Plan to preserve the character of existing neighbourhoods and maintain the designed character of new neighbourhoods. The</p>	<p>Transportation-Front Yard Parking in Residential Areas</p>	<p>Parking for freehold townhouses will be provided in driveways that meet the provisions of the zoning by-law. Entrance permits will be required for new driveways.</p>

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<p>following policies apply to front yard parking for low-rise, mid-rise and high-rise residential development:</p> <p>a. New and existing low-rise residential development with at-grade access, such as single-detached dwellings, semi-detached dwellings, and linked and row houses, will not have front yard parking except for parking in a permitted driveway as per the provisions of the zoning by-law; and</p>		
<p>4.7.8 All new multi-residential, commercial, industrial and institutional development must incorporate appropriate vaults, central storage, collection areas and other facilities and enclosures for the storage of garbage, recycling, organics and other waste materials in order to facilitate sustainable waste management practices. All such facilities for the storage of garbage, recycling, organics, and other waste materials must be shown on site plan control applications, as required under Section 9 of this Plan.</p>	<p>Solid Waste Management- Requirements for New Development</p>	<p>An outdoor garbage storage area will be provided for the stacked townhouses at the western edge of the parking lot, screened by 1.8 metre-high fencing and accessible by way of pedestrian pathways.</p>
<p>5.21 The City of Kingston recognizes the importance of noise management. Any proposed development that has a sensitive use within the</p>	<p>Protection of Health and Safety- Noise Study</p>	<p>A Noise Study has been submitted with the Zoning By-Law Amendment application in support of the proposed development. As described in Section 2.7, no noise control</p>

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<p>potential influence area as described in the Province’s D-6 Guideline or between the 25 to 30 NEF contours requires a detailed noise study to the satisfaction of the City. The study must be conducted by a qualified person in accordance with Provincial guidelines or any such further guidance or requirement implemented by the City, as applicable, address all sources of noise affecting the site, and include recommendations for mitigation to meet the applicable noise criteria.</p>		<p>measures were recommended for the proposal.</p> <p>The following Warning Clause C will be required to be included within the Site Plan Control Agreement:</p> <p><i>“This dwelling unit has been designed with the provision for adding central air conditioning at the occupant’s discretion. Installation of central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment.”</i></p>
<p>8.2 The Design Guidelines for New Communities establish the following guiding principles that should be used to ensure the development of successful communities:</p> <ul style="list-style-type: none"> a. foster attractive communities and a sense of place; b. create compact, accessible, mixed use communities; c. provide a variety of housing types; d. provide access and visibility to open spaces; 	<p>Urban Design-Guiding Principles for Development of New Communities</p>	<p>The proposed development will be compatible within the existing low-rise residential neighbourhood. The proposed townhouses are intended to be located on separately conveyable lots through a future Part Lot Control application.</p> <p>The development will contribute new housing options to an existing residential neighbourhood and will address the streetscape, with townhouses facing Compton Street.</p> <p>The proposal integrates exterior amenity areas and pathway connections to existing active transportation and transit infrastructure, encouraging multi-modal access to the site. New entrances on Compton Street will connect to on-site parking and bike-parking.</p>

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<p>e. encourage environmentally sustainable development;</p> <p>f. create a street network for active transportation and transit;</p> <p>g. integrate and highlight cultural heritage resources; and,</p> <p>h. encourage spaces, services and facilities that highlight arts and culture in a manner that generates and sustains cultural vitality</p>		<p>The site is not located in proximity to cultural or natural heritage features.</p>
<p>8.3 The Design Guidelines for Residential Lots establish the following guiding principles that should be used to ensure new residential development is integrated into the existing built fabric, and is conducive to active transportation:</p> <p>a. foster developments that are context appropriate;</p> <p>b. foster attractive developments which add to the existing sense of place;</p> <p>c. provide a variety of housing types;</p> <p>d. ensure compact, accessible mixed use development;</p> <p>e. encourage environmentally sustainable development; and,</p>	<p>Urban Design-Guiding Principles for Development of Residential Lots</p>	<p>Please see Sections 2.6, 2.7.6, and 8.2.</p>

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f. integrate and highlight cultural heritage resources		
<p>8.4 Through the review of development proposals, construction of public works, or the preparation and approval of area plans, the City will promote the provision of barrier-free access and safety by:</p> <p>a. providing for age-friendly needs and the requirements of people with disabilities, and others requiring access supports through improved amenities such as parking, benches, and washrooms, clear signage, visual or auditory indicators, and other means as appropriate;</p> <p>b. improving public security through enhanced lighting, visibility of public areas, provision of entrance locations in well-traveled areas, and ease of access for emergency personnel or vehicles;</p> <p>c. clearly defining building entrances and avoiding designs that would create areas that are hidden from public view and thus potentially available for criminal activity;</p> <p>d. arranging public uses and amenities within a convenient walking distance;</p>	Urban Design-Accessibility and Safety	<p>The proposed development will introduce new housing types to an existing residential neighbourhood to meet the diverse needs of residents. The low-rise built form will integrate well with the existing neighbourhood.</p> <p>The application complies with zoning requirements for barrier-free paths of travel and accessible parking. The final build-out will be subject to Site Plan Control. Through this process, a Site Plan Accessibility Checklist will be required to be submitted. Site accessibility will be reviewed by Planning and Building staff as well as the Municipal Accessibility Advisory Committee (MAAC). The final building design will be required to comply with AODA requirements.</p>

Policy	Category	Review
<p>e. providing adequate walkway widths, visually permeable materials and structures, and landscaping elements that do not obstruct sightlines in the design of streetscapes, transportation facilities, or public buildings and places; and,</p> <p>f. promoting safe environments by applying Crime Prevention Through Environmental Design (CPTED) concepts and principles in the design of buildings, site layout and landscaping of development sites</p>		
<p>8.5 Through the review of development proposals, construction of public works, or the preparation and approval of area plans, the City shall maintain or enhance the character of valued streetscapes, community areas, and landscapes by:</p> <p>a. preserving human scale in locations that are pedestrian-oriented and establishing an appropriate street wall height by controlling building heights through an implementing zoning by-law, requiring building step-backs, having entrances at street level, providing street</p>	<p>Urban Design- Streetscapes and Public Spaces</p>	<p>The proposed development involves the build-out of a vacant lot with freehold townhouses facing Compton Street and new pathway connections from the site to existing sidewalk infrastructure.</p> <p>The proposed low-rise development will be compatible with the built form of the surrounding neighbourhood with exterior amenity areas, new bicycle parking and on-site parking as well as landscaping to appropriately connect to the existing neighbourhood.</p> <p>Site design will be further evaluated through the Site Plan Control process.</p>

Policy	Category	Review
<p>furniture, and other means as appropriate;</p> <p>b. providing shade through natural or built means to provide comfortable outdoor environments and provide protection from ultraviolet radiation;</p> <p>c. protecting views to the water, City Hall and other significant buildings or landscapes;</p> <p>d. siting new buildings and structures in a manner that repeats and complements the siting and spacing of existing buildings, structures or landscaped areas in order to continue a pattern that is characteristic of surrounding neighbourhoods and heritage areas;</p> <p>e. the strategic use of building separation, landscaping and buffers to mitigate inharmonious elements of the built or natural environment, such as railways, service areas, or incompatible uses;</p> <p>f. designing public spaces or requiring the design of common spaces in private projects to have a clear sense of definition, and provide sufficient amenity, accessibility and security to</p>		

Policy	Category	Review
<p>encourage public use and linkage to other public areas;</p> <p>g. preserving and enhancing the context of special buildings, streetscapes, landscapes and sites that have been identified as having architectural, or cultural heritage value or interest; and,</p> <p>h. encouraging innovative methods to minimize the visual impact of utility features, either by containing utility features within streetscape elements or by screening them from view.</p>		
<p>8.6 The City requires the design of new development to be visually compatible with surrounding neighbourhoods and areas of cultural heritage value or interest through its site plan control review, preparation of zoning standards, and urban design guidelines, as appropriate, that address the following:</p> <p>a. siting, scale and design of new development in relation to the characteristics of the surrounding neighbourhood or the significant cultural heritage resources including, scale, massing, setbacks, access or landscape treatments;</p> <p>b. protecting natural heritage features and areas and</p>		<p>The proposed townhouse dwellings are intended to be located on separately conveyable lots which will be reviewed through a subsequent Part Lot Control application. The dwellings will be context-appropriate and provide new housing options to an existing residential neighbourhood. See Sections 2.6 and 2.7.3.</p>

Policy	Category	Review
<p>cultural heritage landscapes through the siting, design and review of new development;</p> <p>c. promoting innovation in building design to create an interesting and varied built environment, to increase sustainability by improving energy efficiency, and to deliver barrier-free accessibility;</p> <p>d. achieving compatibility in land use and with a predominant architectural style, street pattern or site arrangement where that style or arrangement forms a valuable component of the existing neighbourhood or the cultural heritage value or interest of the identified area. Section 2.7 provides additional policy in this regard; and,</p> <p>e. encourage spaces, services and facilities that highlight arts and culture in a manner that generates and sustains cultural vitality.</p>		
<p>9.5.9 When considering an application to amend the zoning by-law, the Planning Committee and Council will have regard to such matters as:</p>	<p>By-Laws- Planning Committee/Council Considerations</p>	<p>The proposal conforms to applicable sections of the Official Plan as reviewed through this table. No Official Plan Amendment is required.</p> <p>The proposal represents a compatible intensification of an underutilized lot within the urban boundary. The recommended zoning by-law implements the density and</p>

Policy	Category	Review
<p>a. conformity of the proposal with the intent of the Official Plan policies and schedules;</p> <p>b. compatibility of the proposal with existing uses and zones, sensitive uses, the natural heritage system, cultural heritage resources, and compatibility with future planned uses in accordance with this Plan;</p> <p>c. compatibility of proposed buildings or structures with existing buildings and structures, with zoning standards of adjacent sites, with any future planned standards as provided in this Plan, and with any urban design guidelines adopted by the City for the area;</p> <p>d. the extent to which the proposal is warranted in this location and the extent to which areas zoned for the proposed use are available for development;</p> <p>e. the suitability of the site for the proposal, including its ability to meet all required standards of loading, parking, open space or amenity areas;</p> <p>f. the suitability of the density relative to the neighbourhood and/or district, in terms of units per hectare, bedrooms per hectare, floor space</p>		<p>use provisions for the future stacked townhouses and freehold townhouses.</p>

Policy	Category	Review
<p>index, and/or employees per hectare, as applicable;</p> <p>g. the impact on municipal infrastructure, services and traffic;</p> <p>h. comments and submissions of staff, agencies and the public; and,</p> <p>i. the degree to which the proposal creates a precedent</p>		



Planning Committee
Existing Zoning
Kingston Zoning By-Law 2022-62
 Address: 33 Compton Street
 File Number: D14-008-2025

Subject Lands

Schedule 1 Zoning Map

Zone

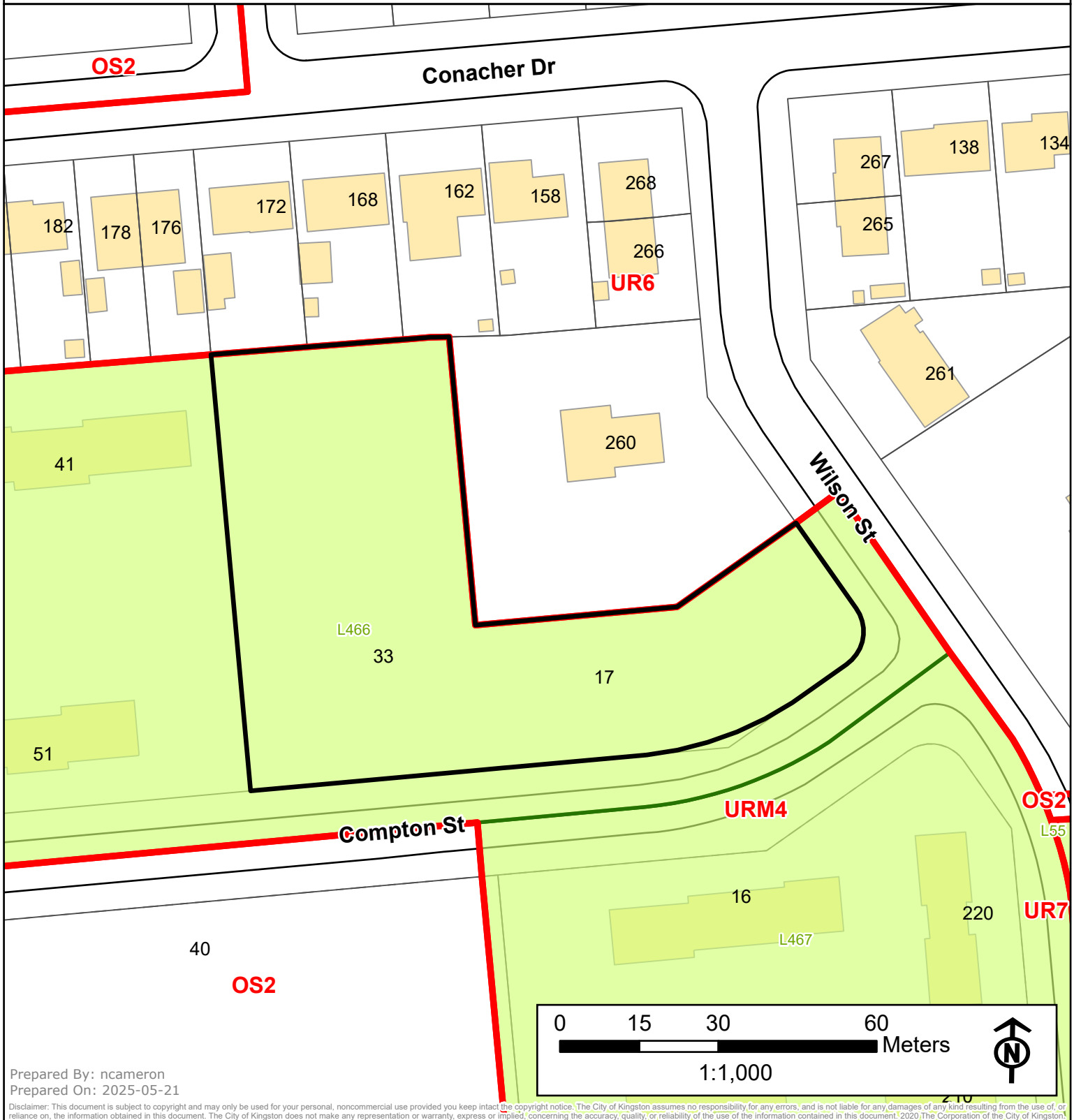
Schedule E - Exception Overlay

Legacy Exceptions (LXXX)

Exceptions (EXXX)

Schedule F - Holding Overlay

Holding Overlay (HXXX)



Prepared By: ncameron
 Prepared On: 2025-05-21

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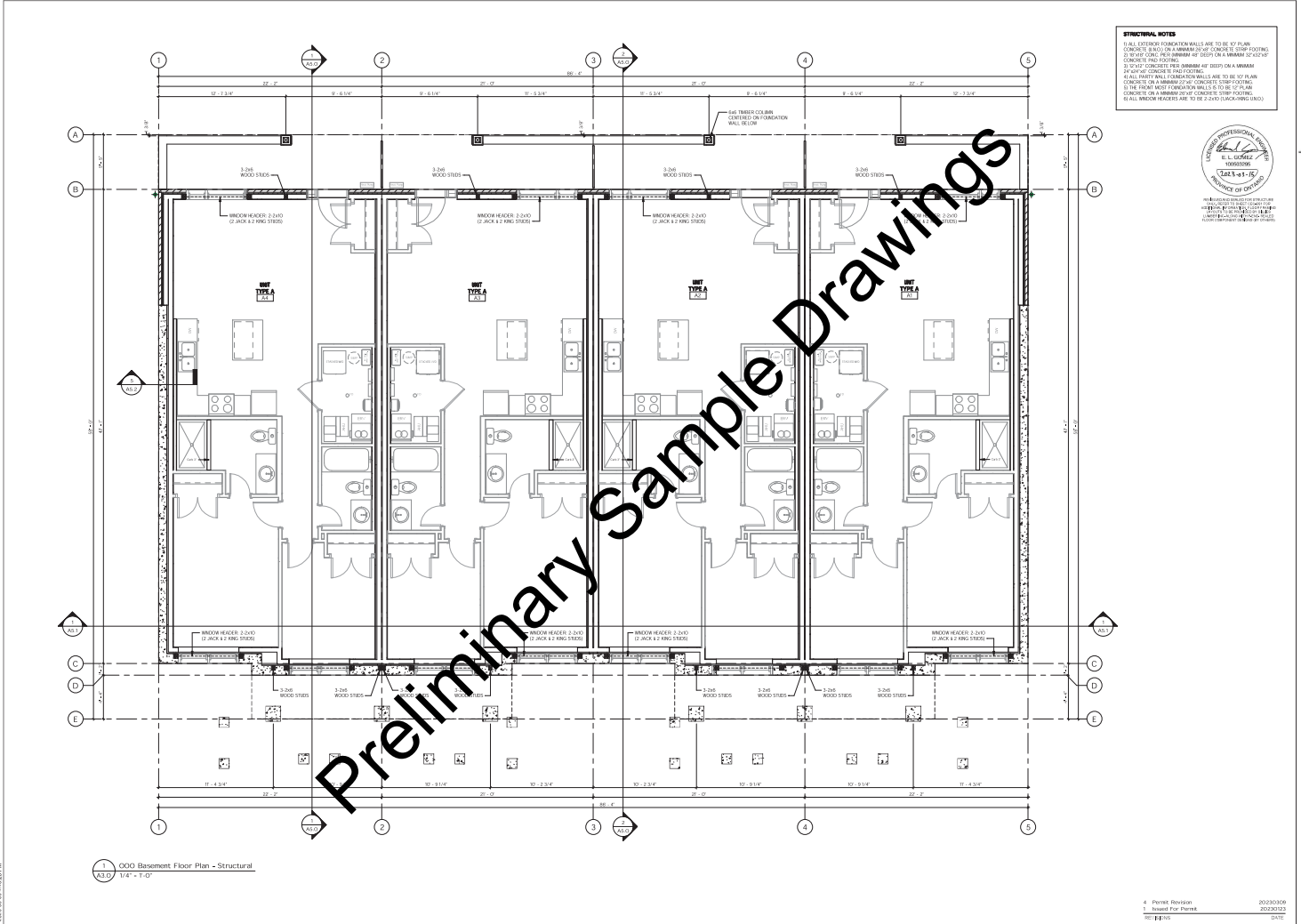
STATISTICS REQUIRED	NOTES PROPOSED
Zoning: URM4 with L466	
Total Site Area: 5,741.00 m ²	
Site Area Street Towns: 1,687.77 m ²	
Site Area Stacked Towns: 3,853.63 m ²	
Lot Frontage: 16 m	6.1 m STREET TOWNS / 49.22 m STACKED TOWNS
Lot Area: 270 m ² STREET TOWNS / N/A STACKED TOWNS	167 m ² STREET TOWNS / N/A STACKED TOWNS
Building Coverage: 30% STREET TOWNS	28.16% (1674.1 m ²)
Landscaping / Open Space: 30% STACKED TOWNS	52.06% (892.69 m ²)
	34.90% (1,250.89 m ²)
Number of Units:	32 STACKED TOWNS (CONDO)
	8 STREET TOWNS (FREEHOLD)
Density:	70.2 upha
Building Height: 2 storeys town / 3.5 storeys stacked town	
Amenity Area Stacks: 18.5 m ² PER UNIT x 38 = 592 m ²	513m ² = (210 m ² Amenity + 303m ² Balconies)
Min. Setbacks: 7.5 m	6.2 m STREET TOWNS / 4.1 m STACKED TOWNS
Front Yard: 4.0 m	4.0 m STREET TOWNS / N/A STACKED TOWNS
Ext. Side Yard: 3.0 m STREET TOWNS / 5.4m STACKED TOWNS	3.0 m STREET TOWNS / 4.18 m STACKED TOWNS
Int. Sideyard: 4.0 m	6.65 m STREET TOWNS / 7.53m STACKED TOWNS
Rear Yard: 0.7 SPACES / DWELLING UNIT @ 0.7 = 5.6 (Street town)	8 spaces
Parking Required: 0.7 SPACES / DWELLING UNIT @ 0.7 = 25.2 (Stacked Town)	45 spaces including 2 Barrier Free, 1 Car Share (CS), 1 Short Term Deliver (STD)
Parking Provided:	
Type A (0.4m x 5.5m): STREET TOWNS / 5.4m STACKED TOWNS	1 space
Type B (2.7m x 5.5m): STREET TOWNS / 5.4m STACKED TOWNS	1 space

SITE PLAN

BLK E, EE, G, GG, OGG,
PL 1592, FR229498 KINGSTON
KINGSTON ON,
33 COMPTON STREET

025 JULY 9, 2025 Drawn By: CES
 Scale: 1:200 Model No: 2025040

REVISIONS



STRUCTURAL NOTES

1. ALL EXTERIOR FOUNDATION WALLS ARE TO BE 12" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 2. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 3. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 4. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 5. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 6. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 7. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 8. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 9. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING. 10. ALL INTERIOR WALLS ARE TO BE 8" THICK CONCRETE BLOCK ON A MINIMUM OF 12" CONCRETE STEEP FOOTING.



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Professional Association of Architects
REGISTERED ARCHITECT
PROJECT: HURON CREEK - LA LOMA
UNITS 25-40, GRAVEL RIDGE TRAIL, KITCHENER
STRUCTURAL - BASEMENT PLAN

DESIGNED BY	20200309
CHECKED BY	20200313
DATE	20200313
PROJECT	20200313
DATE	20200313

4 - Permit Revision
1 - Issued For Permit
EFT

20200309
20200313

STRUCTURAL NOTES

- ALL EXTERIOR FOUNDATION WALLS ARE TO BE OF PLAN CONCRETE (C15) ON A MINIMUM 2'-0" DEEP CONCRETE FOOTING. 12" W/ 2#4 @ 12" O.C. MINIMUM 4" CLEAR ON A MINIMUM 12" W/ 2#4 OF CONCRETE FOOTING.
- 3" W/ 2#4 CONCRETE FLOOR BRIMMERS ARE TO BE ON A MINIMUM 4" W/ 2#4 CONCRETE FLOOR SLAB.
- ALL INTERIOR WALLS ARE TO BE OF 1/2" PLUMBOCK CONCRETE ON A MINIMUM 12" W/ 2#4 CONCRETE FLOOR FOOTING. ALL EXTERIOR FOUNDATION WALLS TO BE OF PLAN CONCRETE ON A MINIMUM 2'-0" DEEP CONCRETE FOOTING. ALL BRIMMERS ARE TO BE 2" W/ 2#4 (L-240) (S-240).



REGISTERED PROFESSIONAL ENGINEER
 E.L. LARSEN
 LICENSE NO. 100000000
 STATE OF OREGON

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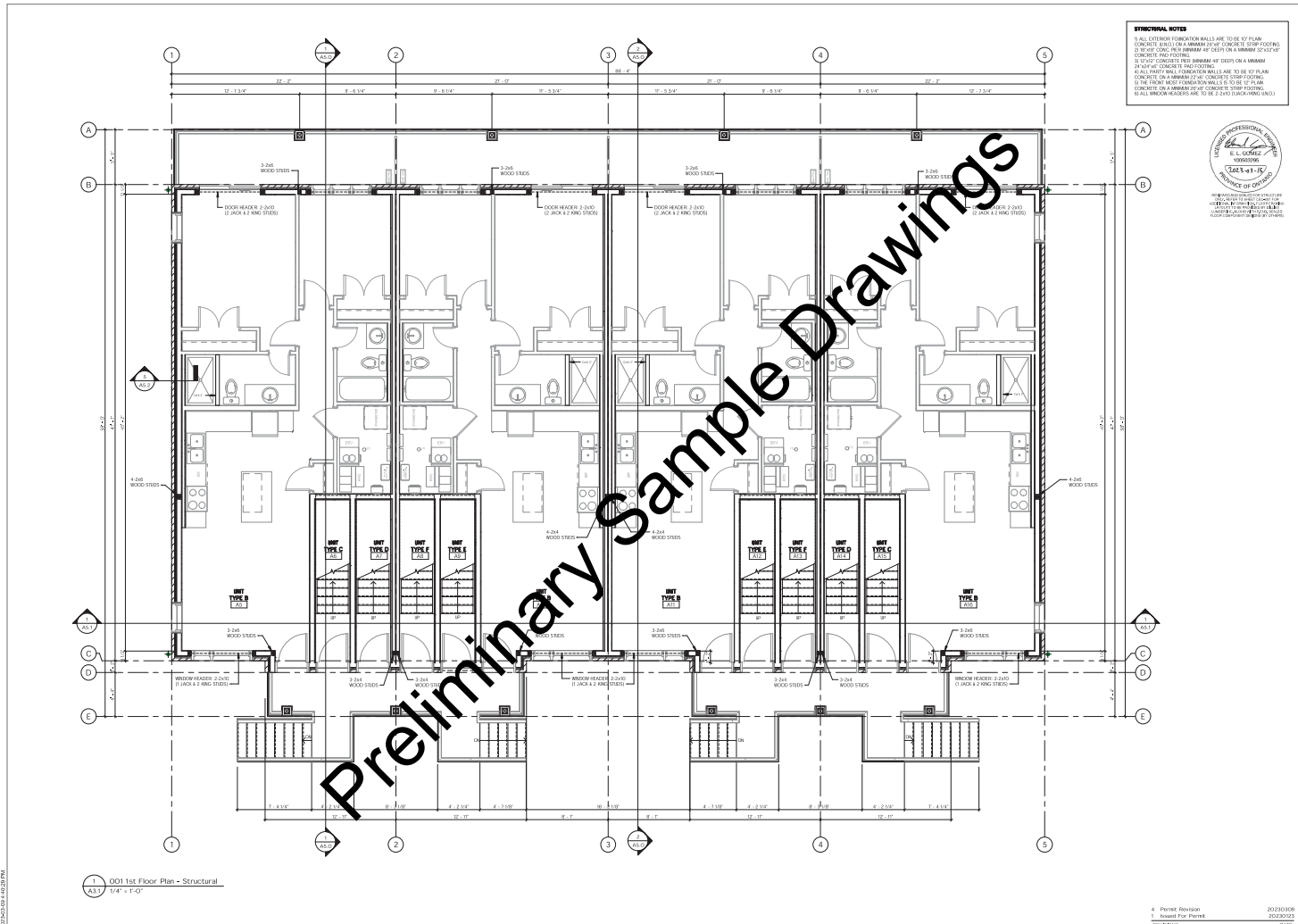
PROJECT: HURON CREEK - LA LOMA
 SHEET: A3.1

HURON CREEK - LA LOMA
 Units 25-40, Gravel Ridge Trail, Kitchener
 Structural - 1st Floor Plan

NO.	REVISION	DATE
1	Permit Revision	20230309
2	Issued For Permit	20230313

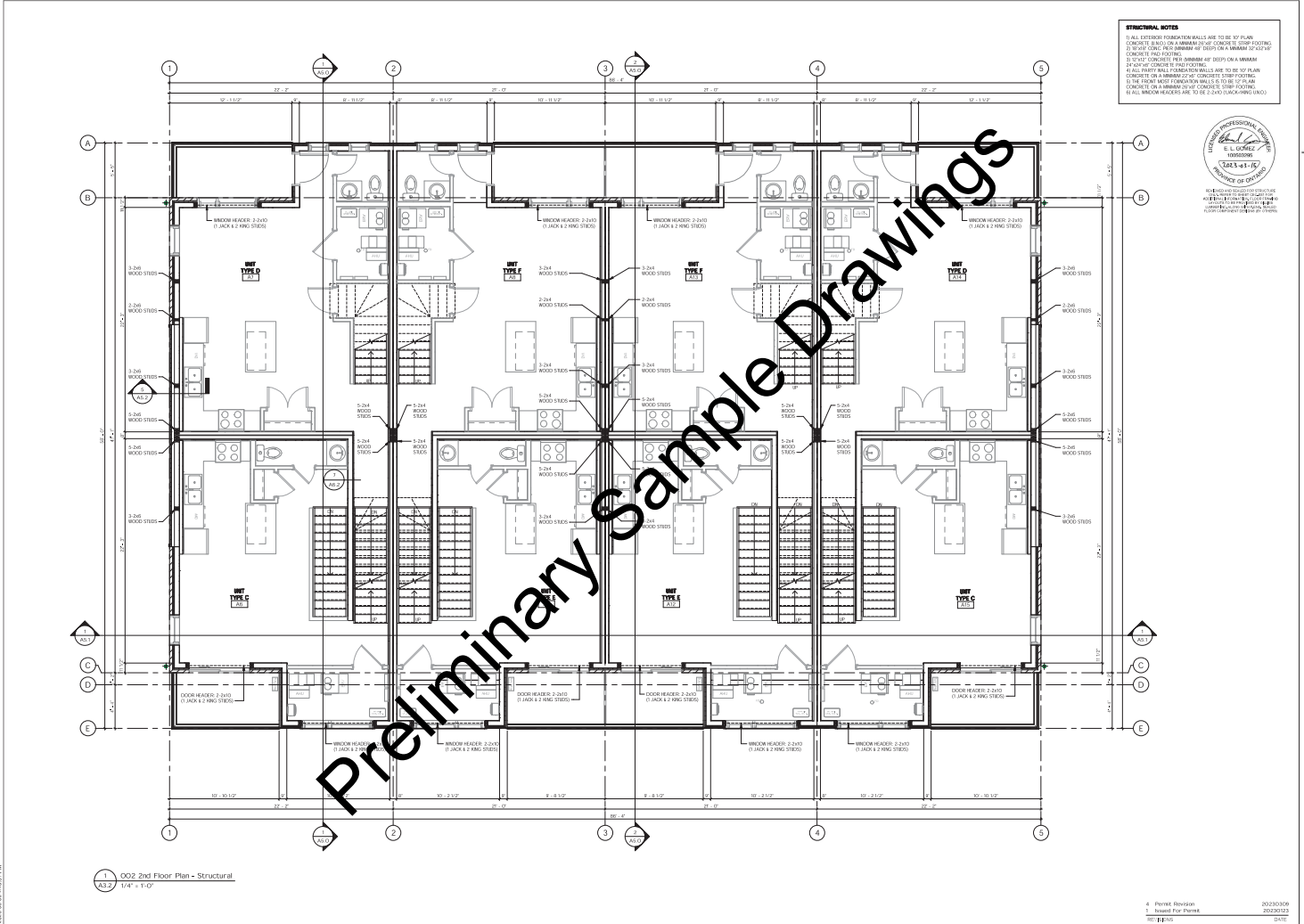
DATE: 02/29/23
 DRAWN BY: JLD
 CHECKED BY: JLD
 PROJECT: HURON CREEK - LA LOMA
 SHEET: A3.1

A3.1



1 001 1st Floor Plan - Structural
 A3.1
 1/4" = 1'-0"

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STRUCTURAL NOTES

1. ALL EXTERIOR FOUNDATION WALLS ARE TO BE 12" PUMP CONCRETE BLOCK ON A MINIMUM 12" W/C CONCRETE STRIP FOOTING TO BE CAST AND FINISHED AT THE SAME TIME AS THE CONCRETE SLAB FOOTING.

2. 3" W/C CONCRETE PERIMETER AT DEPTH ON A MINIMUM 4" W/C CONCRETE STRIP FOOTING.

3. ALL EXTERIOR WALLS SHALL BE 12" W/C PUMP CONCRETE ON A MINIMUM 12" W/C CONCRETE STRIP FOOTING TO BE CAST AND FINISHED AT THE SAME TIME AS THE CONCRETE SLAB FOOTING.

4. ALL INTERIOR FOUNDATION WALLS ARE TO BE 12" W/C CONCRETE ON A MINIMUM 12" W/C CONCRETE STRIP FOOTING TO BE CAST AND FINISHED AT THE SAME TIME AS THE CONCRETE SLAB FOOTING.



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PROFESSIONAL ASSOCIATION OF ARCHITECTS OF ONTARIO
E.L. GORR
100000000
STATE OF OREGON

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E.L. GORR
100000000
STATE OF OREGON

HURON CREEK - LA LOMA
Units 25-40, Gravel Ridge Trail, Kitchener

Structural - 2nd Floor Plan

PROJECT	FILE NO.	DATE
CLIENT	20200306	20200306
DESIGNER	20200306	20200306
CHECKER	20200306	20200306
APPROVER	20200306	20200306
DATE	20200306	20200306
SCALE	1/4" = 1'-0"	1/4" = 1'-0"

4 - Permit Revision
1 - Issued For Permit
= Final

20200306 4:05:27 PM

STRUCTURAL NOTES

- ALL DIMENSION INFORMATION WILL BE TO FACE UNLESS NOTED OTHERWISE.
- CONCRETE SHALL BE A MINIMUM 2800 PSI COMPRESSIVE STRENGTH CONCRETE (CYC) CAST ON A MINIMUM 4" DEPTH ON A MINIMUM 2" X 2" OF CONCRETE FLOOR FINISH.
- 3" OF 2" CONCRETE FLOOR FINISH AT DECK ON A MINIMUM 2" X 2" OF CONCRETE FLOOR FINISH.
- ALL FLOOR FINISH INFORMATION WILL BE TO FACE UNLESS NOTED OTHERWISE.
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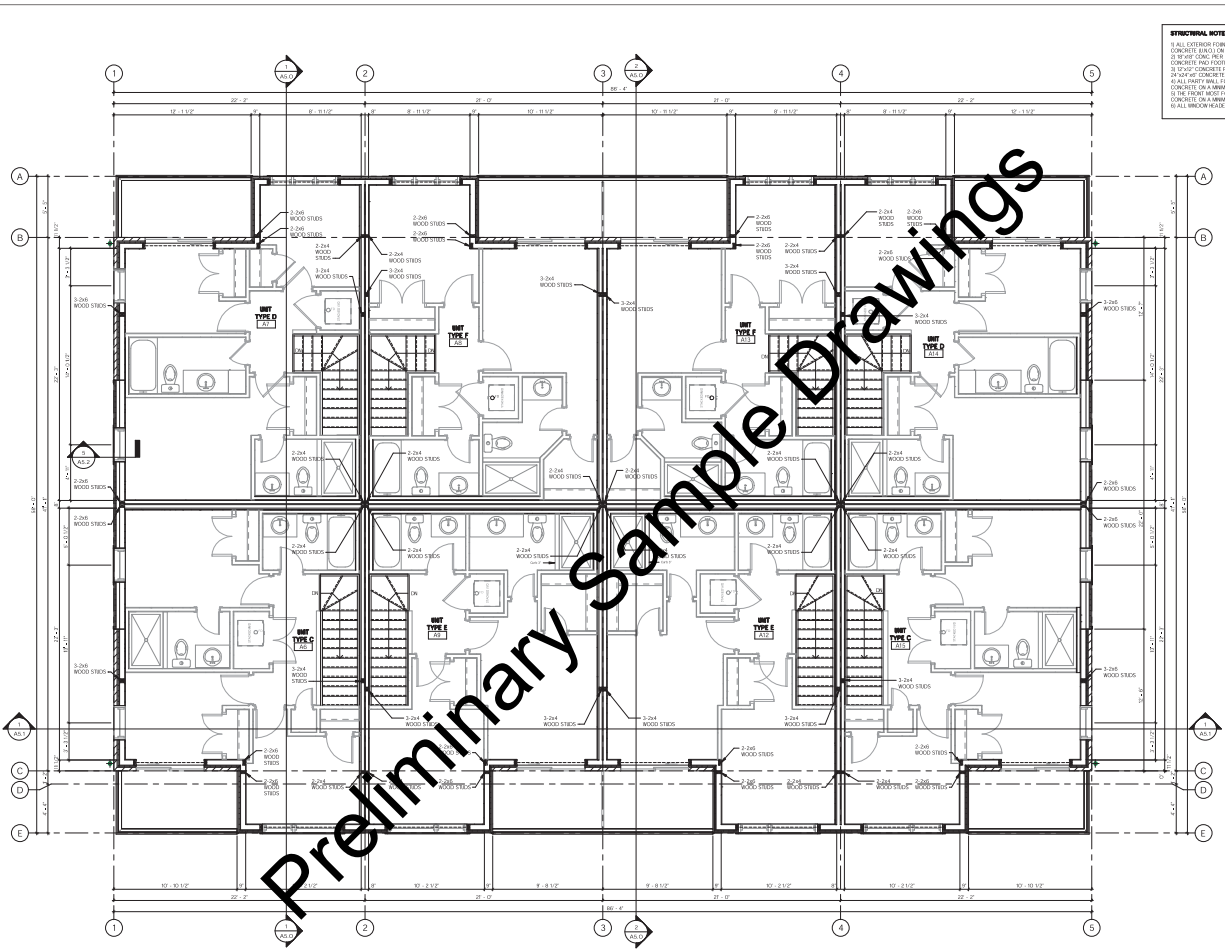
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HURON CREEK - LA LOMA
Units 25-40, Gravel Ridge Trail, Kitchener
Structural - 3rd Floor Plan

NO.	REVISION	DATE
1	Permit Revision	20230309
2	Issued For Permit	20230313
3	Final	20230313

PROJECT: HURON CREEK - LA LOMA
SHEET: 30303
DATE: 20230313
CHECKED BY: [Signature]
DESIGNED BY: [Signature]
DATE: 20230313

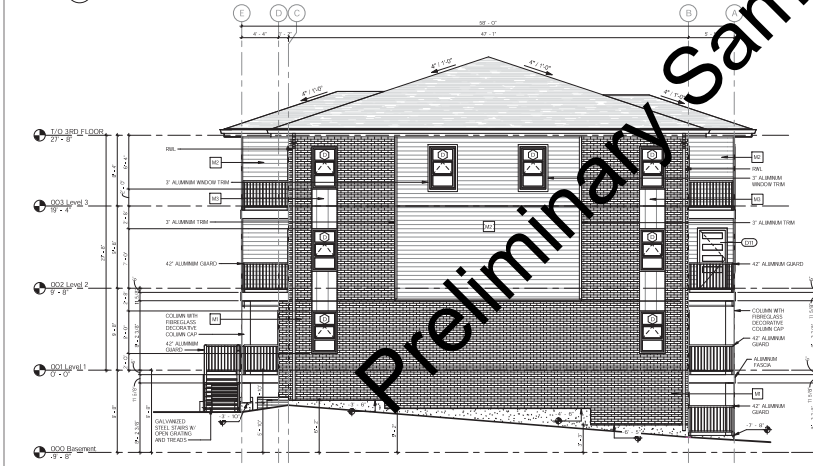


303 3rd Floor Plan - Structural
A3.3 1/4" = 1'-0"

4 - Permit Revision
1 - Issued For Permit
3 - Final



3 Building Elevation South
3/16" = 1'-0"



1 Building Elevation East
3/16" = 1'-0"



2 Perspective A

MATERIAL LEGEND

W	STONE VENEER - SANDTON CHESA WATTE BRICK - COLOR: SILVER STONE
M	METAL BEAMS - RUSTAL BEILING PROJECTS COLOR REVERSON COLOR: REVERWOOD
M	ALUMINUM SPOKE RUSTAL BEILING 1/2" V-GROVE COLOR: SMOOTH BLACK

OTHER ELEMENTS

ROOFING	BLACK
WIND DOORS	BLACK
DOOR FRAMES	BLACK
ALUMINUM WINDOW FRAMES	BLACK
ALUMINUM DOORITS	BLACK
FLASHINGS	TO MATCH ADJACENT MATERIAL
CAP FLASHINGS	BLACK
BALCONY ALUMINUM FRASC	BLACK
BALCONY GARDERS	BLACK PVC ALUMINUM
COLUMN ALUMINUM ROOF	BLACK
WALL CORNER TRIM	TO MATCH ADJACENT MATERIAL

NOTE: CONFIRM FINAL COLOR SELECTION WITH OWNER.

Preliminary Sample Drawings

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ASSOCIATION OF ARCHITECTS
 REGISTERED ARCHITECT
 BOBBY D. TURNER
 LICENSE NO. 10000

PROJECT: _____

HURON CREEK - LA LOMA
 Units 25-40, Gravel Ridge Trail, Kitchener
 Building Elevations

PROJECT	REVISED	DATE
DESIGNED BY	202009	
CHECKED BY	202009	
DATE	202009	
DATE	202009	
DATE	202009	
DATE	202009	

4 Permit Revision 20200209
 3 Issues For Clarification 20200209
 2 Issues For Permit 20200209
 1 Final 20200209

20200209 4.02.2019



Preliminary Sample Drawings

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ASSOCIATION OF ARCHITECTS OF ONTARIO
REGISTERED ARCHITECT

HURON CREEK - LA LOMA
Units 25-40, Gravel Ridge Trail, Kitchener
Building Elevations

PROJECT: _____
DATE: _____
SCALE: _____
SHEET: _____

4 Permet Revision
1 Issued For Permit
11/1/2020

20230306
20200703
11/1/2020

A4.1

Site Photographs- August 22, 2025



Figure 1- North Facing View of Subject Property from Compton Street



Figure 2- West Facing View of Subject Property from Wilson Street



Figure 3- West Side of Subject Property Abutting 51 & 41 Compton Street; Transit Route 1 on Compton Street



Figure 4- South Facing View of Adjacent Property (16 Compton Street)



Figure 5- East Facing View of Evangel Pentecostal Church from Subject Property



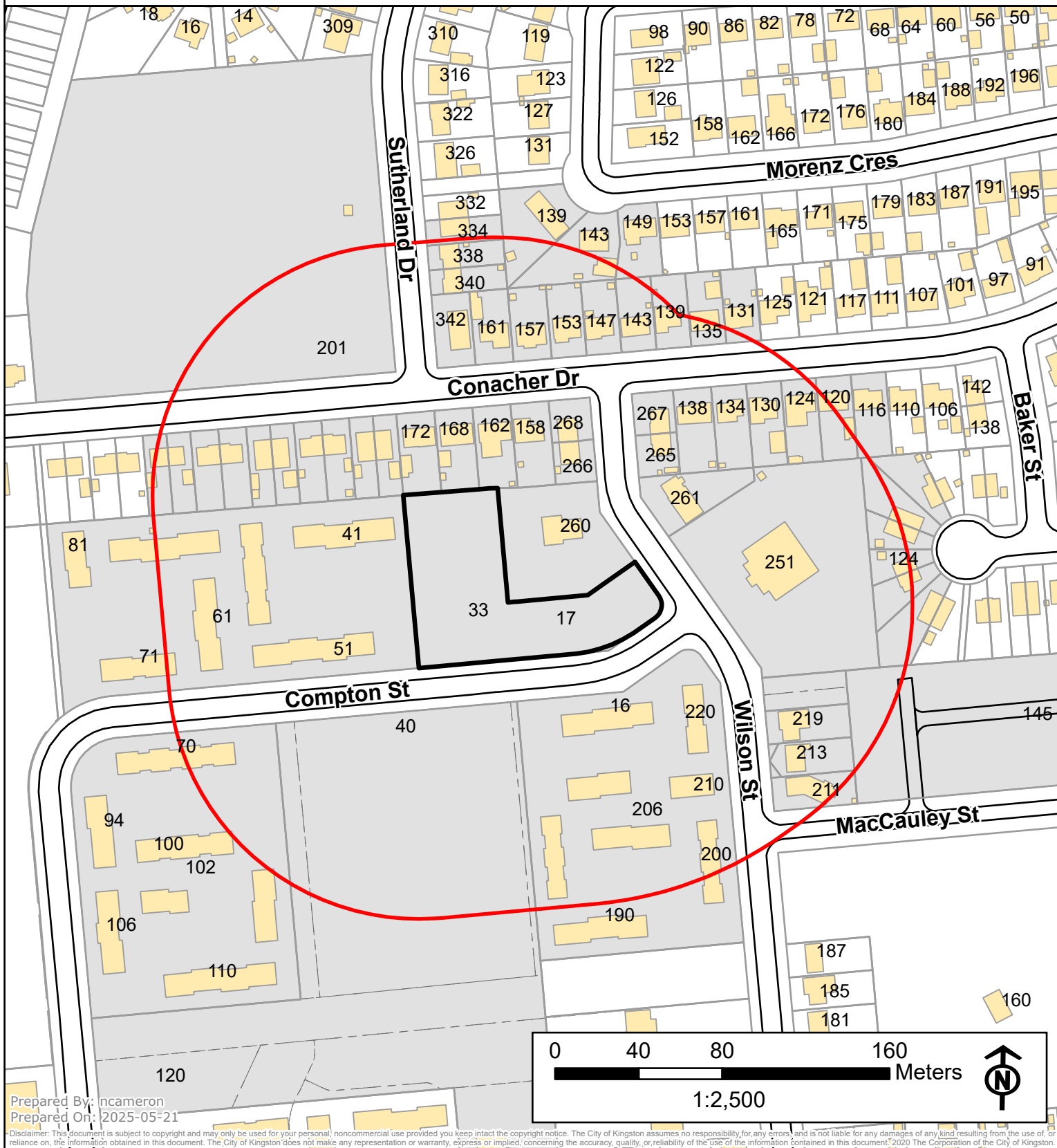
Figure 6- South Facing View of Headway Park to the West



**Delegated Authority
Public Notification Map**

Address: 33 Compton Street
File Number: D14-008-2025

- Subject Lands
- Property Boundaries
- Proposed Parcels
- 120m Public Notification Boundary
- 61 Properties in Receipt of Notice (MPAC)



Prepared By: incameron
Prepared On: 2025-05-21

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